



HISTORIC PRESERVATION COMMISSION

Marc Elrich
County Executive

Robert Sutton
Chairman

Date: March 21, 2024

MEMORANDUM

TO: Rabbiah Sabbakhan
Department of Permitting Services

FROM: Chris Berger
Historic Preservation Section
Maryland-National Capital Park & Planning Commission

SUBJECT: Historic Area Work Permit # 1060792 - 16400 Layhill Road, Sandy Spring

The Montgomery County Historic Preservation Commission (HPC) has reviewed the attached application for a Historic Area Work Permit (HAWP). This application was **approved** at the March 20, 2024, HPC meeting.

The HPC staff has reviewed and stamped the attached construction drawings.

THE BUILDING PERMIT FOR THIS PROJECT SHALL BE ISSUED CONDITIONAL UPON ADHERENCE TO THE ABOVE APPROVED HAWP CONDITIONS AND MAY REQUIRE APPROVAL BY DPS OR ANOTHER LOCAL OFFICE BEFORE WORK CAN BEGIN.

Applicant: Jose Thommana
Address: 100 Edison Park Drive, 4th Floor, Gaithersburg

This HAWP approval is subject to the general condition that the applicant will obtain all other applicable Montgomery County or local government agency permits. After the issuance of these permits, the applicant must contact this Historic Preservation Office if any changes to the approved plan are made. Once work is complete the applicant will contact Chris Berger at 301-495-4571 or chris.berger@montgomeryplanning.org to schedule a follow-up site visit.





APPLICATION FOR HISTORIC AREA WORK PERMIT
HISTORIC PRESERVATION COMMISSION
301.563.3400

FOR STAFF ONLY:
HAWP# 1060792
DATE ASSIGNED

APPLICANT:

Name: E-mail:
Address: City: Zip:
Daytime Phone: Tax Account No.:

AGENT/CONTACT (if applicable):

Name: E-mail:
Address: City: Zip:
Daytime Phone: Contractor Registration No.:

LOCATION OF BUILDING/PREMISE: MIHP # of Historic Property

Is the Property Located within an Historic District? Yes/District Name

No/Incl

Is there a map of the easement, and documentation from the Easement...

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission
[Signature]

Are other Planning and/or Hearing Examiner Approvals /Revisions (Conditional Use, Variance, Record Plat, etc.?) If YES, include supplemental information.

Building Number: Street:

Town/City: Nearest Cross Street:

Lot: Block: Subdivision: Parcel:

TYPE OF WORK PROPOSED: See the checklist on Page 4 to verify that all supporting items for proposed work are submitted with this application. Incomplete Applications will not be accepted for review. Check all that apply:

- New Construction, Addition, Demolition, Grading/Excavation, Deck/Porch, Fence, Hardscape/Landscape, Roof, Shed/Garage/Accessory Structure, Solar, Tree removal/planting, Window/Door, Other:

I hereby certify that I have the authority to make the foregoing application, that the application is correct and accurate and that the construction will comply with plans reviewed and approved by all necessary agencies and hereby acknowledge and accept this to be a condition for the issuance of this permit.

Jose Thommana

Signature of owner or authorized agent

Date

HAWP APPLICATION: MAILING ADDRESSES FOR NOTIFYING
[Owner, Owner's Agent, Adjacent and Confronting Property Owners]

Owner's mailing address

Owner's Agent's mailing address

Adjacent and confronting Property Owners mailing addresses

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED

Montgomery County

Historic Preservation Commission



Robert H. Potter

Description of Property: Please describe the building and surrounding environment. Include information on significant structures, landscape features, or other significant features of the property:

Description of Work Proposed: Please give an overview of the work to be undertaken:

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By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED

Montgomery County

Historic Preservation Commission




Work Item 1: _____	
Description of Current Condition:	Proposed Work:

Work Item 2: _____	
Description of Current Condition:	Proposed Work:

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APPROVED
Montgomery County
Historic Preservation Commission



Work Item 3: _____	
Description of Current Condition:	Proposed Work:

**HISTORIC AREA WORK PERMIT
CHECKLIST OF
APPLICATION REQUIREMENTS**

	Required Attachments						
Proposed Work	I. Written Description	2. Site Plan	3. Plans/Elevations	4. Material Specifications	5. Photographs	6. Tree Survey	7. Property Owner Addresses
New Construction	*	*	*	*	*	*	*
Additions/Alterations	*	*	*	*	*	*	*
Demolition	*	*	*		*		*
Deck/Porch	*	*	*	*	*	*	*
Fence/Wall	*	*	*	*	*	*	*
Driveway/Parking Area	*	*		*	*	*	*
Grading/Excavation/Landscaping	*	*		*	*	*	*
Tree Removal	*	*		*	*	*	*
Siding/ Roof Changes	*	*	*	*	*		*
Window/Door Changes	*	*	*	*	*		*
Masonry Repair/Repoint	*	*	*	*	*		*
Signs	*	*	*	*	*		*

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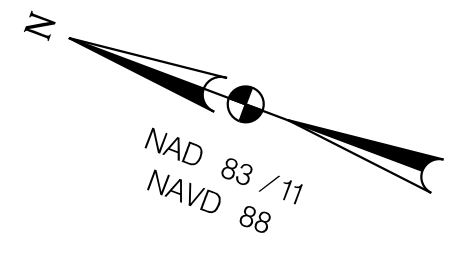


- RIGHT-OF-WAY LEGEND**
- PROPOSED FEE SIMPLE
 - PROPOSED TEMPORARY CONSTRUCTION EASEMENT
 - SHARED MAINTENANCE AREA
 - EXISTING RIGHT-OF-WAY/PROPERTY LINES
 - EXISTING EASEMENT

- PAVING LEGEND**
- CONCRETE DRIVEWAY
 - ASPHALT SHARED USE PATH
 - FLEXIBLE SHARED USE PATH
 - MILL AND OVERLAY
 - CONCRETE SIDEWALK
 - DETECTABLE WARNING SURFACE
 - FULL DEPTH ASPHALT
 - PAVEMENT /SIDEWALK REMOVAL

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Montgomery County
Historic Preservation Commission
Robert A. Patton



N 532750
E 1304500

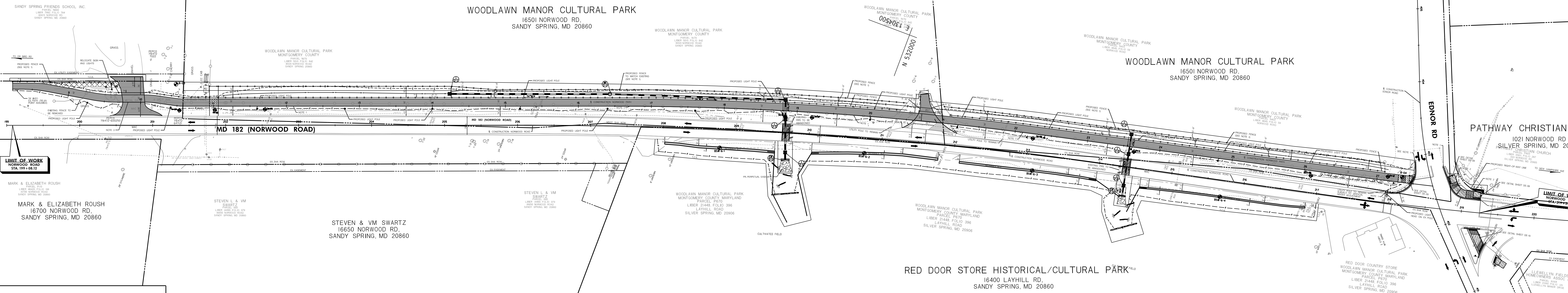
SANDY SPRINGS FRIENDS SCHOOL INC.
16923 NORWOOD RD.
SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK
16501 NORWOOD RD.
SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK
16501 NORWOOD RD.
SANDY SPRING, MD 20860

PATHWAY CHRISTIAN CHURCH
1021 NORWOOD RD
SILVER SPRING, MD 20905

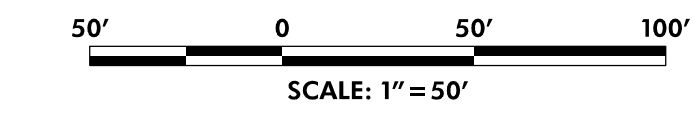
RED DOOR STORE HISTORICAL/CULTURAL PARK
16400 LAYHILL RD.
SANDY SPRING, MD 20860



LIMIT OF WORK
NORWOOD ROAD
STA. 1994+00.12

LIMIT OF WORK
NORWOOD ROAD
STA. 1994+00.12

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
DIVISION OF TRANSPORTATION ENGINEERING
HERITAGE TRIANGLE TRAIL PHASE 1
DR. BIRD / NORWOOD ROAD
SHARED USE PATH



NORWOOD SITE DISPLAY
SCALE: 1" = 50' DWG. | OF |

APPROVED	DATE	Chief, Transportation Planning and Design Section
DESIGNED: MEG	CONTRACT NO. MR2022024	
DRAWN: MEG	ROAD NO.	JOB NO.
CHECKED: SAB	DATE	
APPROVED:	FILE NO.	
Chief, Division of Transportation Engineering		

RK&K
P: 410.728.2900
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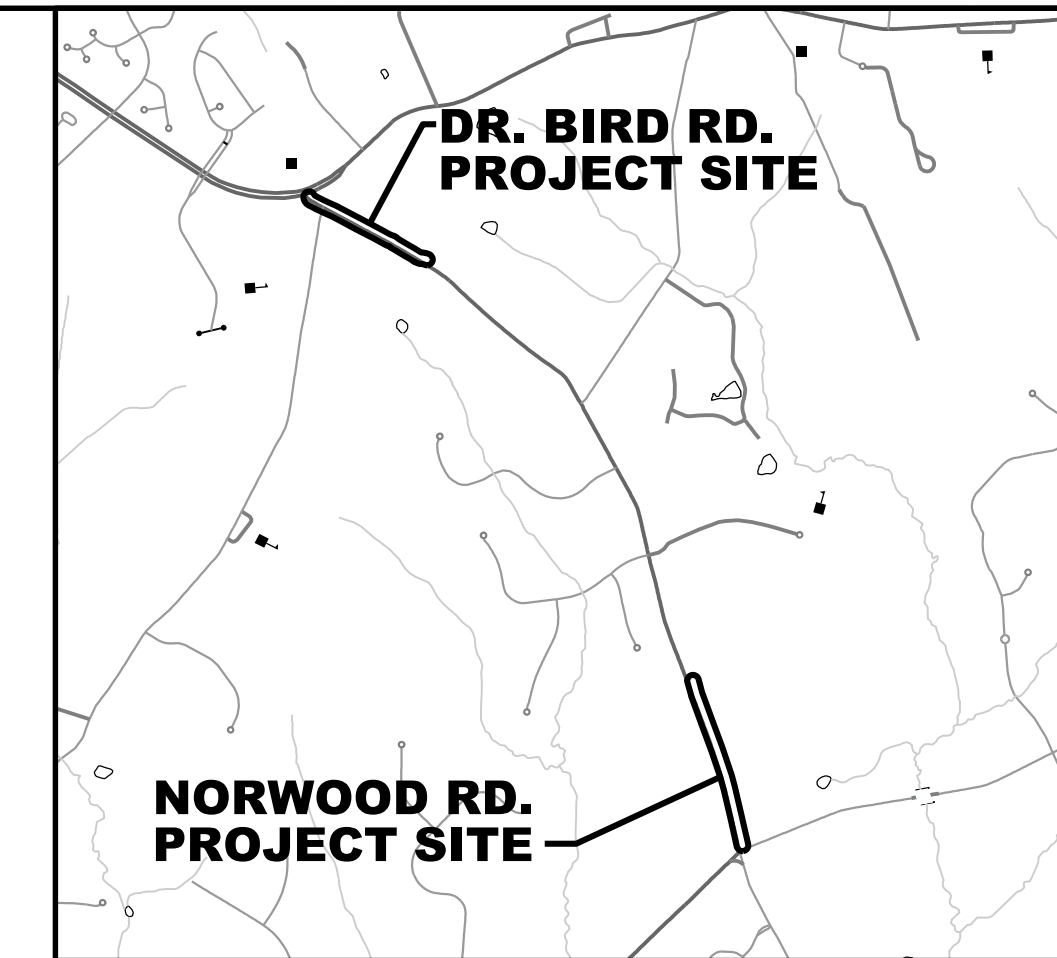
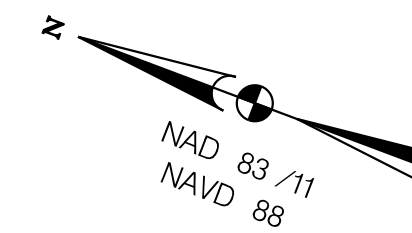
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Montgomery County
Historic Preservation Commission

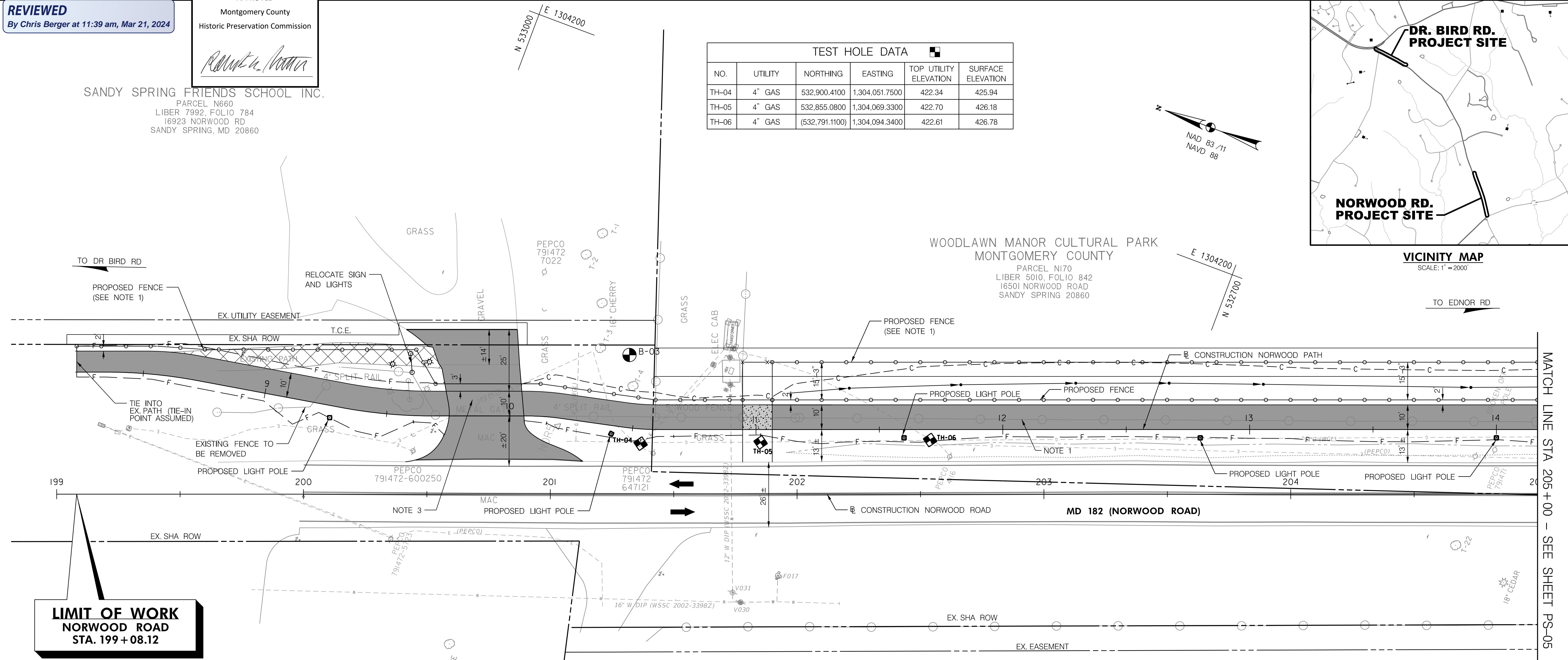
Robert A. ...

SANDY SPRING FRIENDS SCHOOL INC.
PARCEL N660
LIBER 7992, FOLIO 784
16923 NORWOOD RD
SANDY SPRING, MD 20860

TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-04	4" GAS	532,900.4100	1,304,051.7500	422.34	425.94
TH-05	4" GAS	532,855.0800	1,304,069.3300	422.70	426.18
TH-06	4" GAS	(532,791.1100)	1,304,094.3400	422.61	426.78



VICINITY MAP
SCALE: 1" = 2000'



**LIMIT OF WORK
NORWOOD ROAD
STA. 199+08.12**

MARK & ELIZABETH ROUSH
PARCEL P110
LIBER 48420, FOLIO 199
16700 NORWOOD ROAD
SANDY SPRING, MD 20860

STEVEN L & VM
SWARTZ
PARCEL 000
LIBER 14490, FOLIO 379
16650 NORWOOD ROAD
SANDY SPRING, MD 20860

ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
4175 SF	MD 182 - STA. 200+88, LT TO STA. 205+00, LT
WOOD RAIL FENCE	
322 LF	MD 182 - STA. 201+78, 38.4' LT TO STA. 205+00, 38.2' LT
321 LF	MD 182 - STA. 201+79, 53.4' LT TO STA. 205+00, 53.4' LT

REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)	
134 LF	MD 182 - STA. 200+95, 31.5' LT TO STA. 201+79, 60.3' LT
310 LF	MD 182 - STA. 201+90, 30.9' LT TO STA. 205+00, 29.3' LT

NOTES:

- REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
- FENCE AND GATE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS. PROVIDE A 10' MINIMUM PEDESTRIAN PATHWAY WITH 2% MAXIMUM CROSS-SLOPE ACROSS ENTRANCE.



OWNER / ADDRESS:
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
100 EDISON PARK DRIVE
GAITHERSBURG, MARYLAND

CONTACT:
REBECCA PARK
REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
240-777-7263

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY IMB

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
DIVISION OF TRANSPORTATION ENGINEERING
HERITAGE TRIANGLE TRAIL PHASE 1
DR. BIRD / NORWOOD ROAD
SHARED USE PATH
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

P: 410.728.2900
700 East Pratt Street, Suite 500 | Baltimore, MD 21202

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PLAN NO. MR2022024 DWG. PS-04

DRAWING NO. PS-04 OF 07 SHEET NO. 22 OF 101

PLOTTER: 3/14/2024
FILE: \\ad.rk.com\ys\Cloud\Projects\2020\2097_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0004_DrBird.dgn

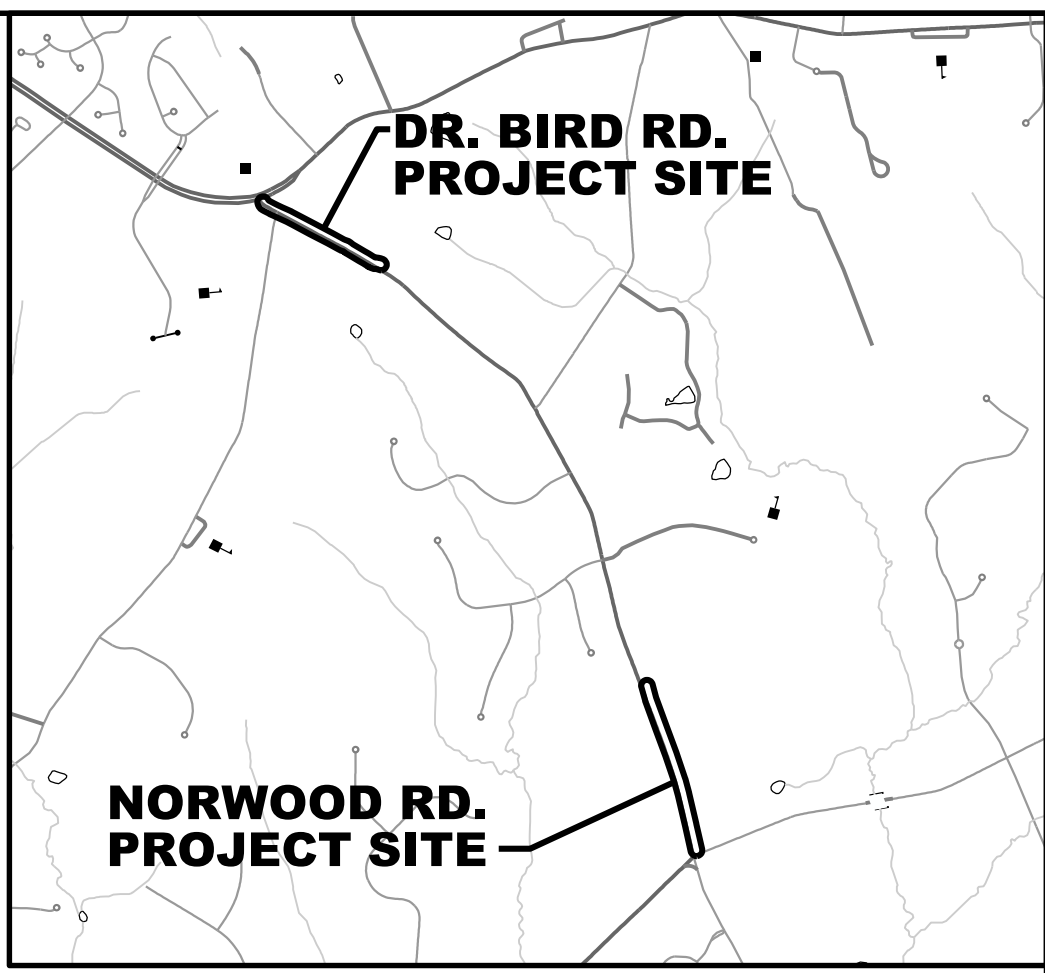
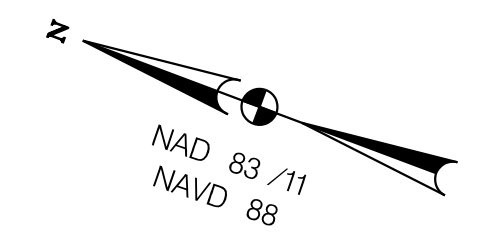
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ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
5022 SF	MD 182 - STA. 205+00, LT TO STA. 210+00, LT
WOOD RAIL FENCE	
322 LF	MD 182 - STA. 205+00, 38.2' LT TO STA. 210+00, 41.2' LT
321 LF	MD 182 - STA. 205+00, 53.4' LT TO STA. 210+00, 52.0' LT
REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)	
502 LF	MD 182 - STA. 205+00, 29.3' LT TO STA. 210+00, 28.8' LT

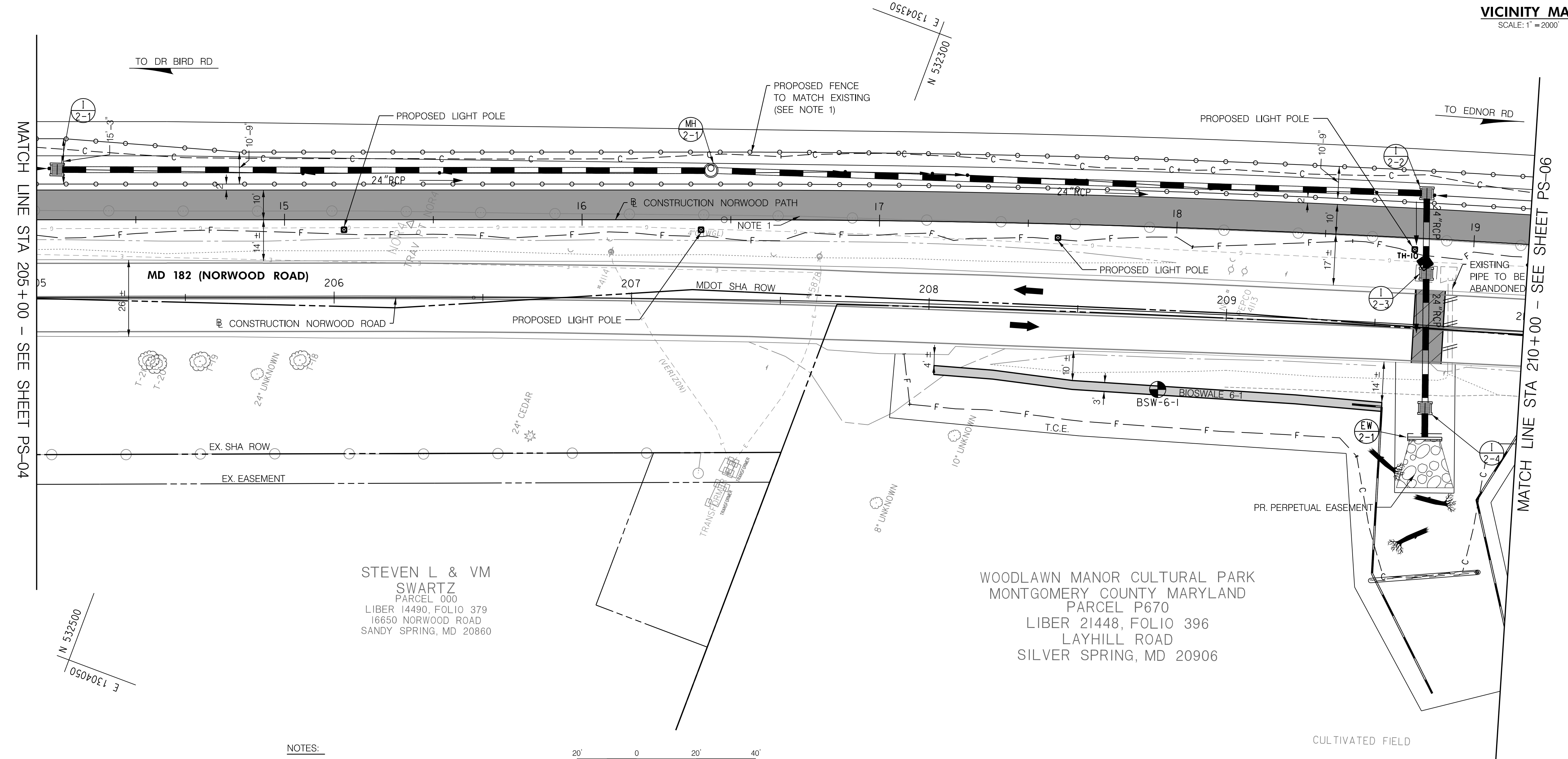
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APPROVED
Montgomery County
Historic Preservation Commission
Robert K. ...

TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-10	4" GAS	532,119.8023	1,304,335.2552	417.82	422.09



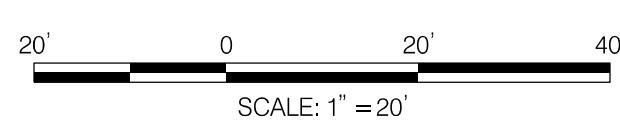
WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY
PARCEL N170
LIBER 5010, FOLIO 842
16501 NORWOOD ROAD
SANDY SPRING 20860



STEVEN L & VM SWARTZ
PARCEL 000
LIBER 14490, FOLIO 379
16650 NORWOOD ROAD
SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY MARYLAND
PARCEL P670
LIBER 21448, FOLIO 396
LAYHILL ROAD
SILVER SPRING, MD 20906

- NOTES:
- REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
 - FENCE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS.



LEGEND

	CONCRETE DRIVEWAY
	ASPHALT SHARED USE PATH
	FLEXIBLE SHARED USE PATH
	MILL AND OVERLAY
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	FULL DEPTH ASPHALT
	PAVEMENT /SIDEWALK REMOVAL

OWNER / ADDRESS:
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DEPARTMENT OF TRANSPORTATION
100 EDISON PARK DRIVE
GAITHERSBURG, MARYLAND

CONTACT:
REBECCA PARK
REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
240-777-7263

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY IMB

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
DIVISION OF TRANSPORTATION ENGINEERING
HERITAGE TRIANGLE TRAIL PHASE 1
DR. BIRD / NORWOOD ROAD
SHARED USE PATH
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. PS-05 OF 07 SHEET NO. 23 OF 101

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PLOTTED: 3/14/2024
 FILE: \\ad.rkk.com\ys\Cloud\Projects\2020\202097_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0005_DrBird.dgn

FINE MILLING ASPHALT PAVEMENT 1 INCH TO 2.5 INCH DEPTH

69 SY MD 182 - STA. 211+27, LT TO STA. 212+00, LT

ASPHALT SHARED USE PATH (STD. NO. MD 580.08)

1452 SF MD 182 - STA. 210+00, LT TO STA. 211+47, LT

3273 SF MD 182 - STA. 211+69, LT TO STA. 215+00, LT

WOOD RAIL FENCE

124 LF MD 182 - STA. 210+00, 41.2' LT TO STA. 211+21, 39.2' LT

132 LF MD 182 - STA. 210+00, 52.0' LT TO STA. 211+30, 51.9' LT

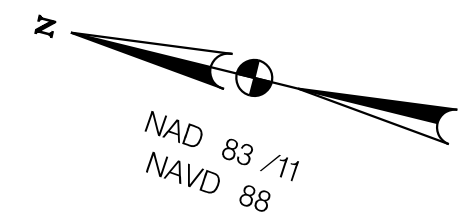
322 LF MD 182 - STA. 211+81, 51.7' LT TO STA. 215+00, 51.0' LT

312 LF MD 182 - STA. 211+90, 40.9' LT TO STA. 215+00, 40.3' LT

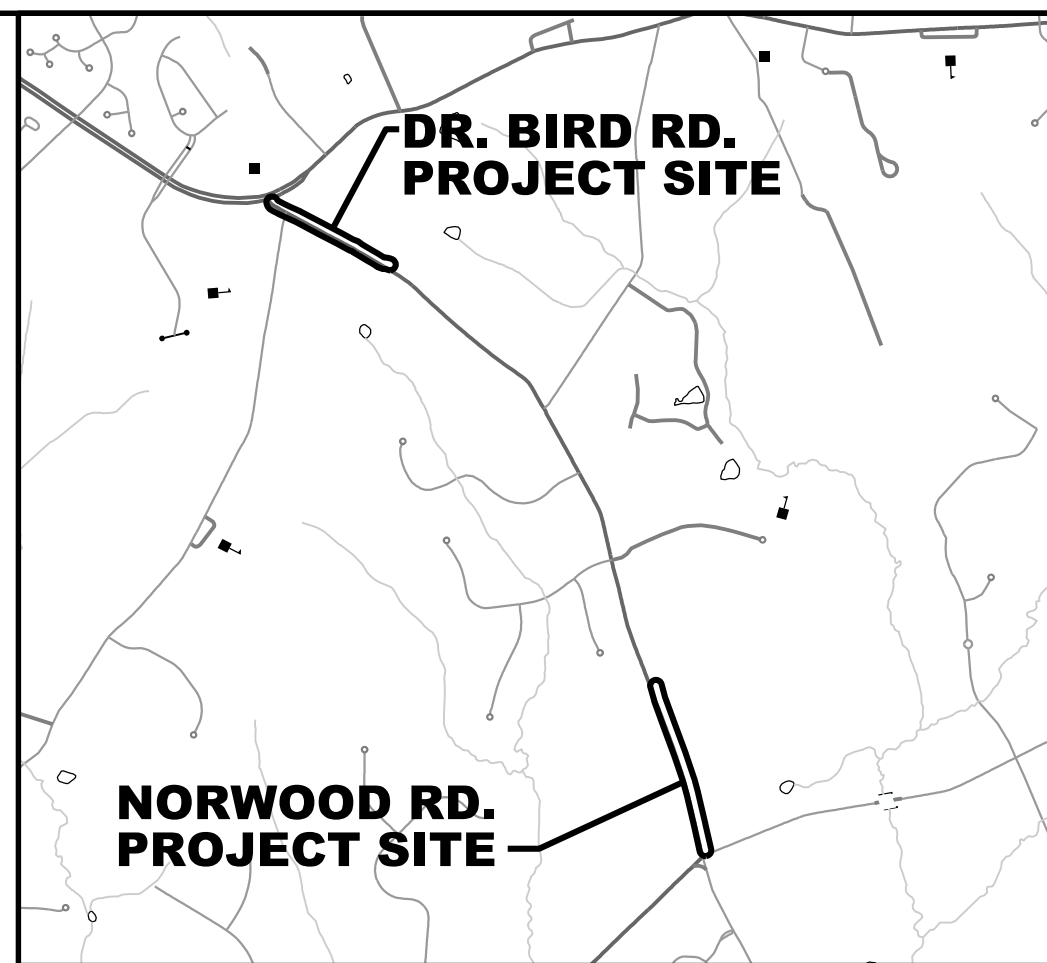
WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY
PARCEL N170
LIBER 5010, FOLIO 842
16501 NORWOOD ROAD
SANDY SPRING 20860

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

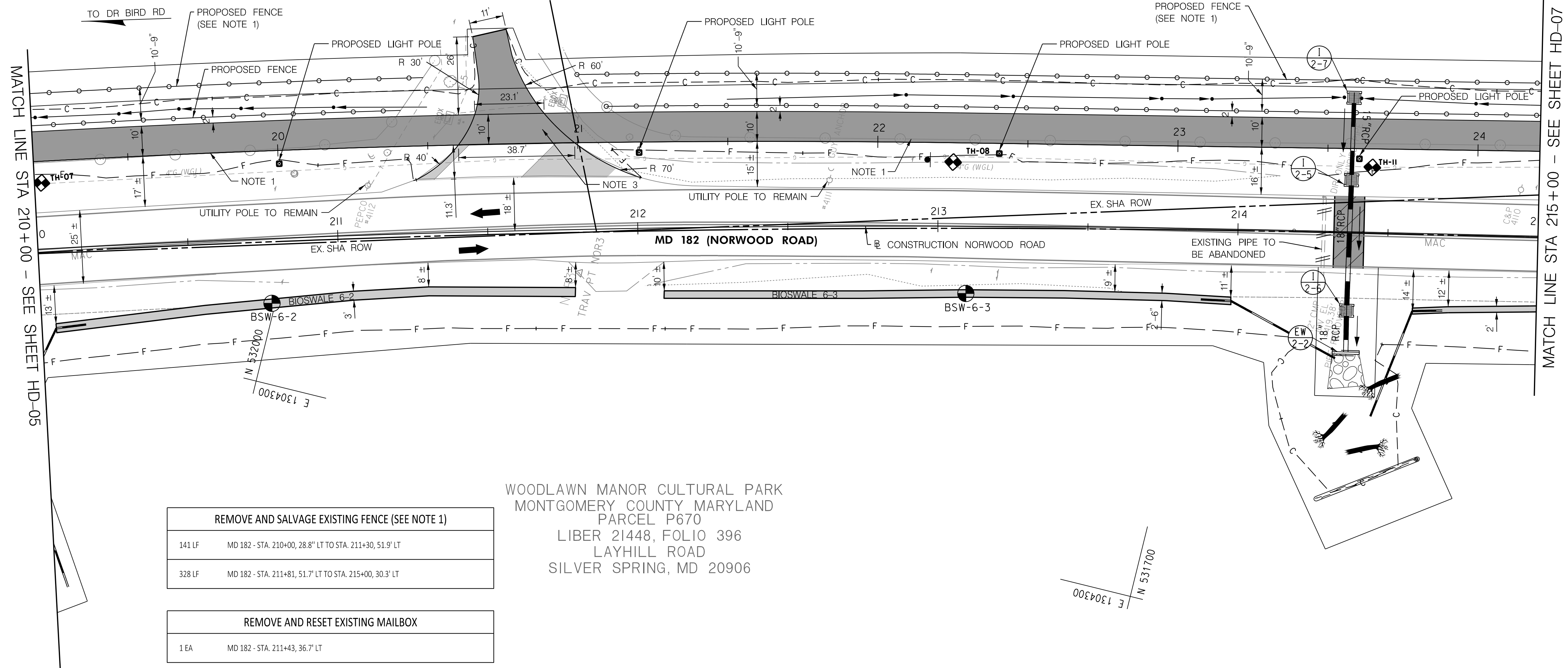
APPROVED
Montgomery County
Historic Preservation Commission
Robert A. ...



WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY
PARCEL N422
LIBER 4845, FOLIO 59
NORWOOD ROAD



TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-07	4" GAS	532,085.2900	1,304,347.0200	419.60	421.76
TH-08	4" GAS	531,791.9800	1,304,426.4900	419.22	424.74
TH-11	4" GAS	531,655.9400	1,304,458.2100	418.88	422.00



REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)

141 LF MD 182 - STA. 210+00, 28.8' LT TO STA. 211+30, 51.9' LT

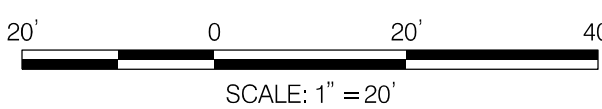
328 LF MD 182 - STA. 211+81, 51.7' LT TO STA. 215+00, 30.3' LT

REMOVE AND RESET EXISTING MAILBOX

1 EA MD 182 - STA. 211+43, 36.7' LT

WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY MARYLAND
PARCEL P670
LIBER 21448, FOLIO 396
LAYHILL ROAD
SILVER SPRING, MD 20906

- NOTES:
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 - PROVIDE A 10' MINIMUM PEDESTRIAN PATHWAY WITH 2% MAXIMUM CROSS-SLOPE ACROSS ENTRANCE.



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MONTGOMERY COUNTY
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NO.	REVISION	DATE	BY

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY IMB

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PLOTTER: 3142024
 FILE: \\ad.rk.com\rs\Cloud\Projects\2020\2097_MCDOTTransp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0006_DrBird.dgn

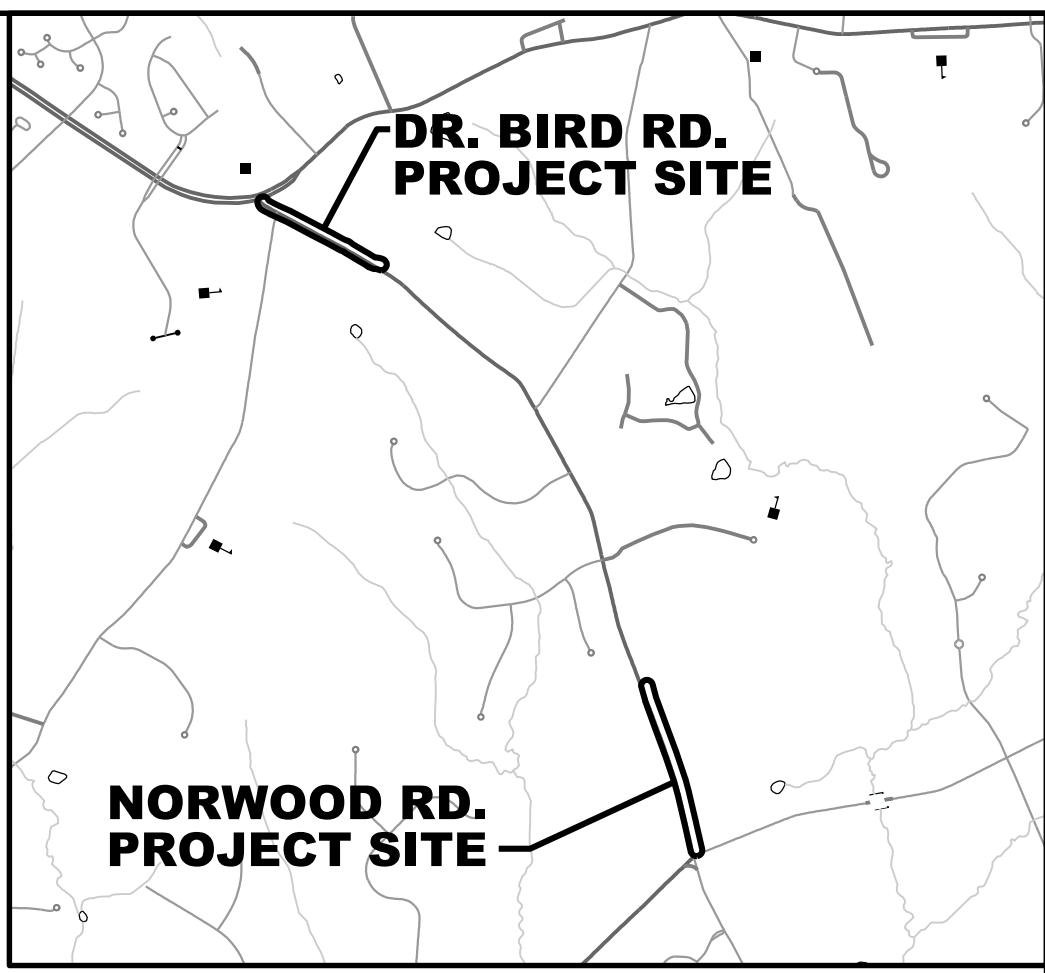
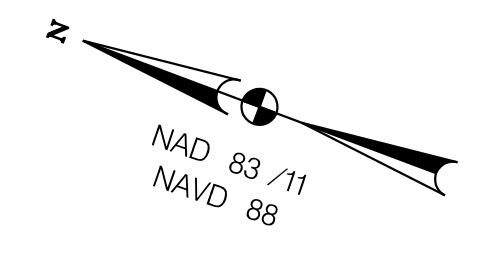
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FILE: \\ad.rk.com\rs\Cloud\Projects\2020\2097_MCDOTTransp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0006_DrBird.dgn

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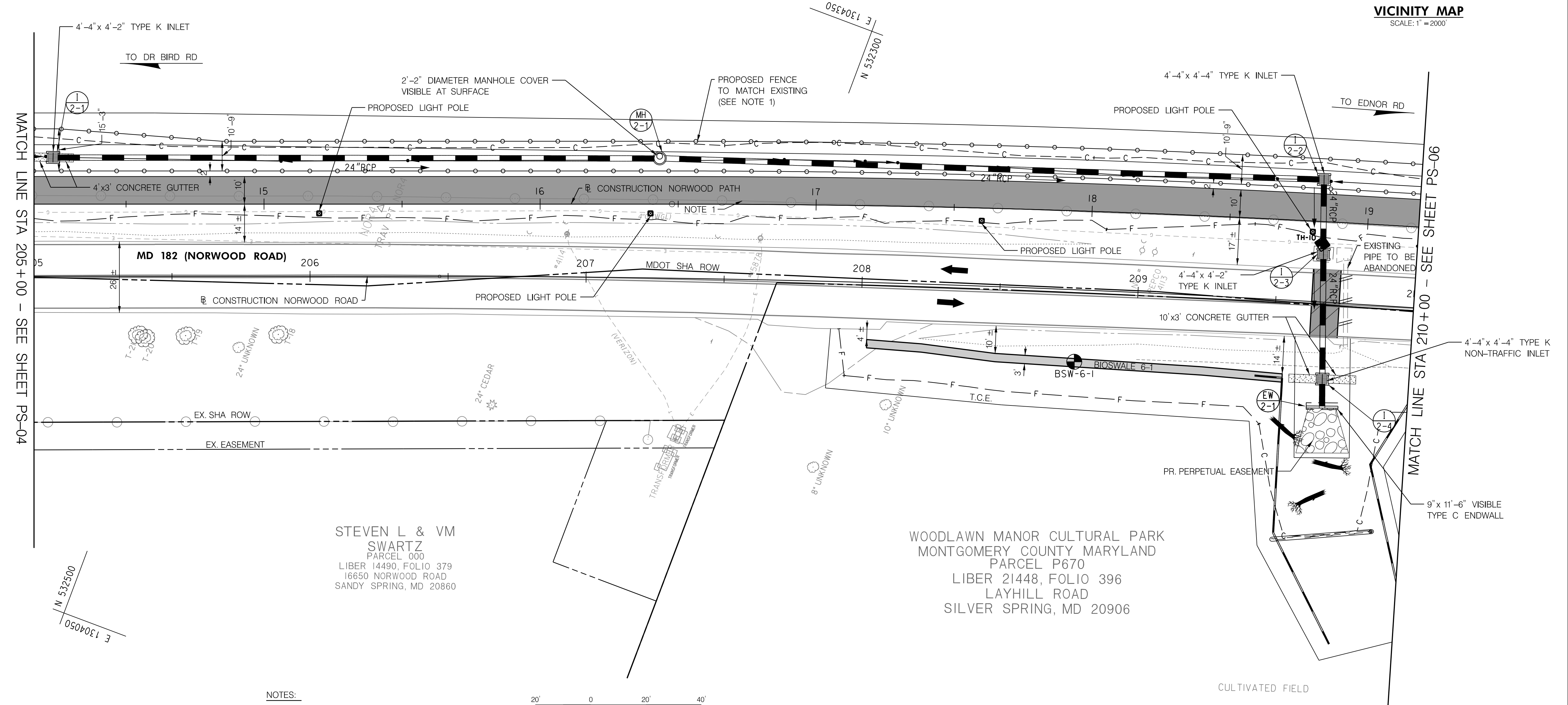
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Montgomery County
Historic Preservation Commission
Robert A. ...

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MONTGOMERY COUNTY
PARCEL N170
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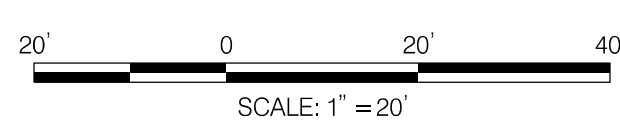
MATCH LINE STA 205+00 - SEE SHEET PS-04

MATCH LINE STA 210+00 - SEE SHEET PS-06

STEVEN L & VM SWARTZ
PARCEL 000
LIBER 14490, FOLIO 379
16650 NORWOOD ROAD
SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY MARYLAND
PARCEL P670
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SILVER SPRING, MD 20906

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	ASPHALT SHARED USE PATH
	FLEXIBLE SHARED USE PATH
	MILL AND OVERLAY
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	FULL DEPTH ASPHALT
	PAVEMENT /SIDEWALK REMOVAL

OWNER / ADDRESS:
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
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CONTACT:
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MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
240-777-7263

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

DESIGNED BY _____ DRAWN BY _____ CHECKED BY _____

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
DIVISION OF TRANSPORTATION ENGINEERING
HERITAGE TRIANGLE TRAIL PHASE 1
DR. BIRD / NORWOOD ROAD
SHARED USE PATH
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. _____ OF 07 SHEET NO. 23 OF 101

P: 410.728.2900
700 East Pratt Street, Suite 500 | Baltimore, MD 21202

Engineers | Construction Managers | Planners | Scientists
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PLOTTER: 3/14/2024
FILE: \\ad.rkk.com\ys\Cloud\Projects\2020\202097_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0005_DrBird.dgn

PLOTTER: 3/14/2024
FILE: \\ad.rkk.com\ys\Cloud\Projects\2020\202097_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0005_DrBird.dgn

ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
3081 SF	MD 182 - STA. 215+00, LT TO STA. 218+09, LT

5 INCH CONCRETE SIDEWALK	
232 SF	MD 182 - STA. 218+09, LT TO STA. 218+31, LT
648 SF	MD 182 - STA. 218+69, LT TO STA. 219+24, LT
352 SF	MD 182 - STA. 219+10, RT TO STA. 219+35, RT

MDOT SHA STANDARD TYPE A CURB 8 INCH X 16 INCH MINIMUM (STD. NO. MD 620.02)	
81 LF	MD 182 - STA. 218+69, LT TO STA. 219+24, LT
24 LF	MD 182 - STA. 219+23, 22.9 RT TO STA. 219+35, 43.4 RT

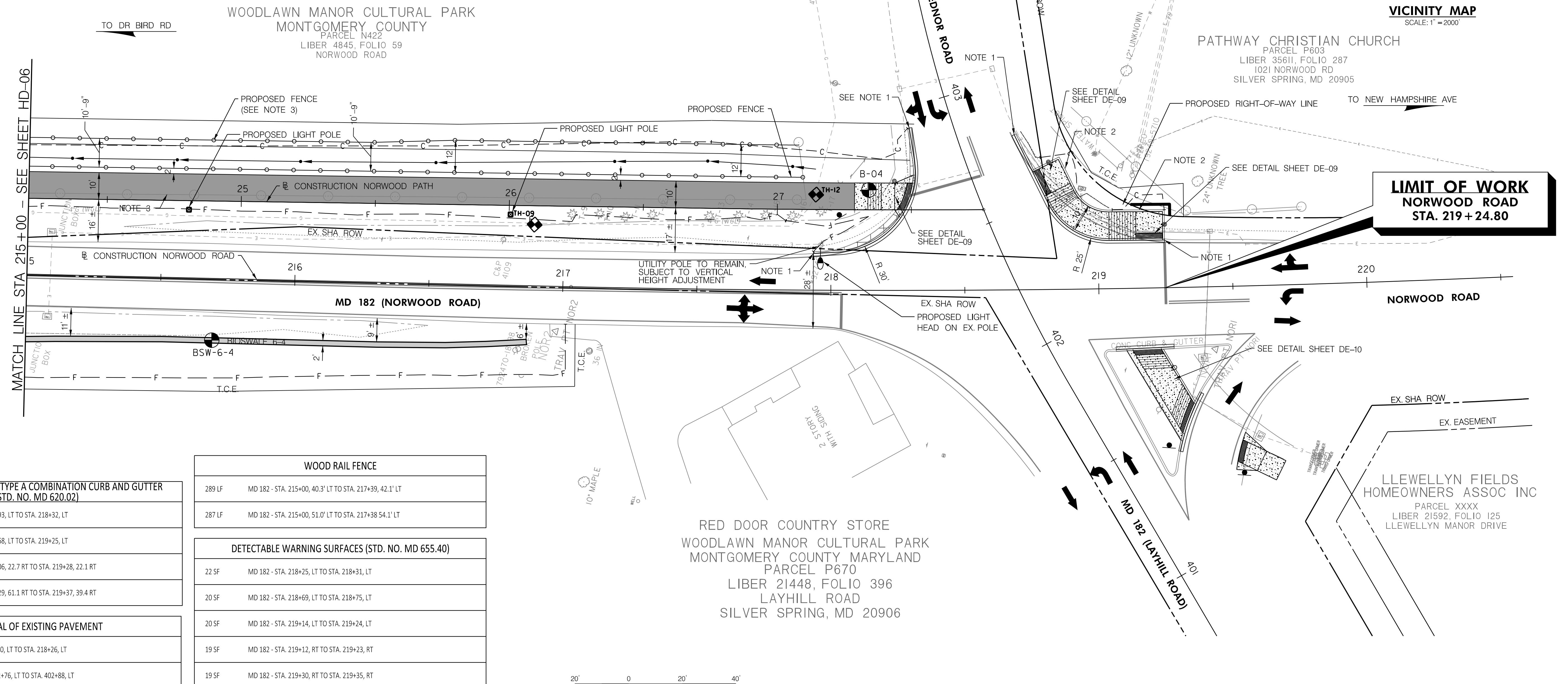
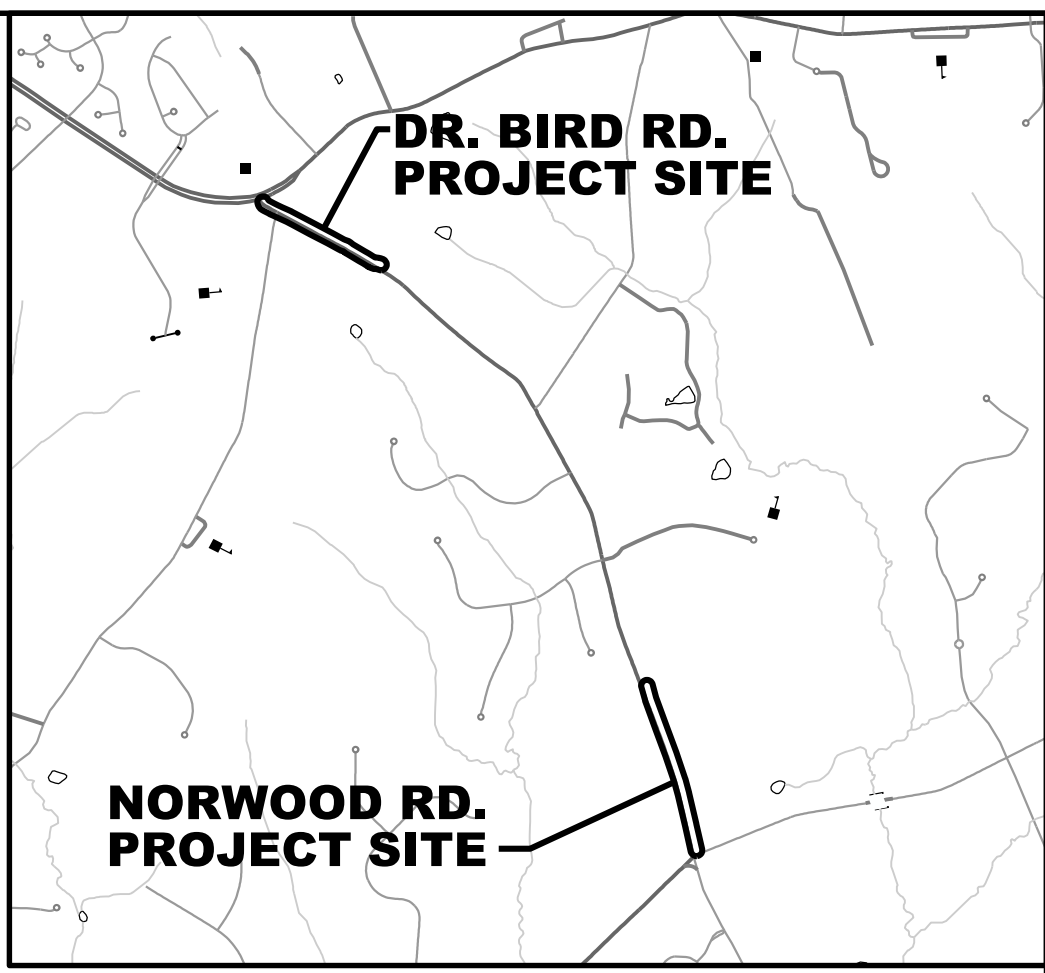
REMOVE AND SALVAGE EXISTING FENCE	
141 LF	MD 182 - STA. 215+00, 30.3' LT TO STA. 217+41, 54.1' LT

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission
Rebecca Park

TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-09	4" GAS	531,417.3700	1,304,512.4600	421.14	425.72
TH-12	4" GAS	531,318.0700	1,304,548.4800	425.54	427.79

WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY
PARCEL N422
LIBER 4845, FOLIO 59
NORWOOD ROAD

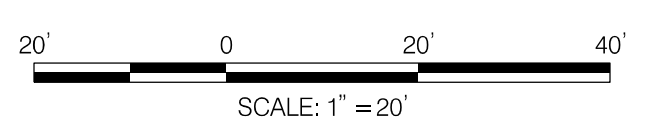


MDOT SHA STANDARD TYPE A COMBINATION CURB AND GUTTER (STD. NO. MD 620.02)	
72 LF	MD 182 - STA. 217+93, LT TO STA. 218+32, LT
78 LF	MD 182 - STA. 218+68, LT TO STA. 219+25, LT
22 LF	MD 182 - STA. 219+06, 22.7 RT TO STA. 219+28, 22.1 RT
24 LF	MD 182 - STA. 219+29, 61.1 RT TO STA. 219+37, 39.4 RT

REMOVAL OF EXISTING PAVEMENT	
3 CY	MD 182 - STA. 218+00, LT TO STA. 218+26, LT
1 CY	EDNOR RD - STA. 402+76, LT TO STA. 402+88, LT

WOOD RAIL FENCE	
289 LF	MD 182 - STA. 215+00, 40.3' LT TO STA. 217+39, 42.1' LT
287 LF	MD 182 - STA. 215+00, 51.0' LT TO STA. 217+38 54.1' LT

DETECTABLE WARNING SURFACES (STD. NO. MD 655.40)	
22 SF	MD 182 - STA. 218+25, LT TO STA. 218+31, LT
20 SF	MD 182 - STA. 218+69, LT TO STA. 218+75, LT
20 SF	MD 182 - STA. 219+14, LT TO STA. 219+24, LT
19 SF	MD 182 - STA. 219+12, RT TO STA. 219+23, RT
19 SF	MD 182 - STA. 219+30, RT TO STA. 219+35, RT



OWNER / ADDRESS:
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
100 EDISON PARK DRIVE
GAITHERSBURG, MARYLAND

CONTACT:
REBECCA PARK
REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
240-777-7263

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY TMB

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
DIVISION OF TRANSPORTATION ENGINEERING
HERITAGE TRIANGLE TRAIL PHASE 1
DR. BIRD / NORWOOD ROAD
SHARED USE PATH
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. PS-07 OF 07 SHEET NO. 25 OF 101

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- LEGEND
- CONCRETE DRIVEWAY
 - ASPHALT SHARED USE PATH
 - FLEXIBLE SHARED USE PATH
 - MILL AND OVERLAY
 - CONCRETE SIDEWALK
 - DETECTABLE WARNING SURFACE
 - FULL DEPTH ASPHALT
 - PAVEMENT / SIDEWALK REMOVAL
- NOTES:
- NOSE DOWN CURB OVER 2 FEET TO MATCH EXISTING EDGE OF ROAD.
 - ALL CONCRETE BACKING CURB SHALL BE PAID FOR AS MDOT SHA TYPE A CURB, ANY HEIGHT OR DEPTH, 12" MAXIMUM HEIGHT. REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
 - IF CONTRACTOR ENCOUNTERS LONGITUDINAL UNDERDRAIN THEY SHALL REPAIR AND REPLACE IN ACCORDANCE WITH STD. MD 387.11. FENCE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS.

PLOTTER: 3/14/2024
 FILE: \\ad.rk.com\rs\Cloud\Projects\2020\202097_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0007_DrBird.dgn

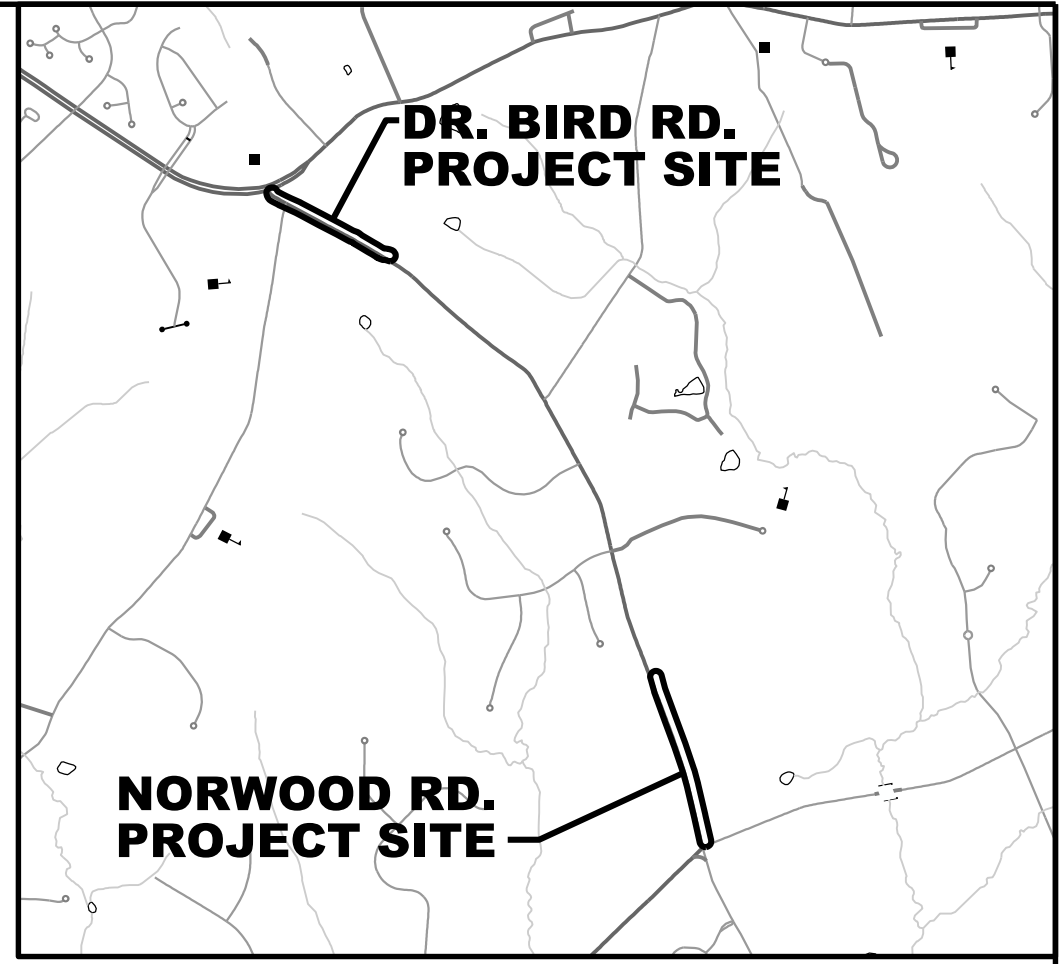
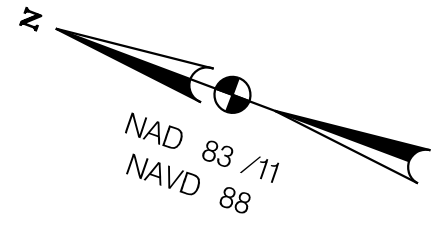
PLOTTED: 3/14/2024
FILE: \\ad.rk.com\rs\Cloud\Projects\2020\202097_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0007_DrBird.dgn

ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
5022 SF	MD 182 - STA. 205+00, LT TO STA. 210+00, LT
WOOD RAIL FENCE	
322 LF	MD 182 - STA. 205+00, 38.2' LT TO STA. 210+00, 41.2' LT
321 LF	MD 182 - STA. 205+00, 53.4' LT TO STA. 210+00, 52.0' LT
REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)	
502 LF	MD 182 - STA. 205+00, 29.3' LT TO STA. 210+00, 28.8' LT

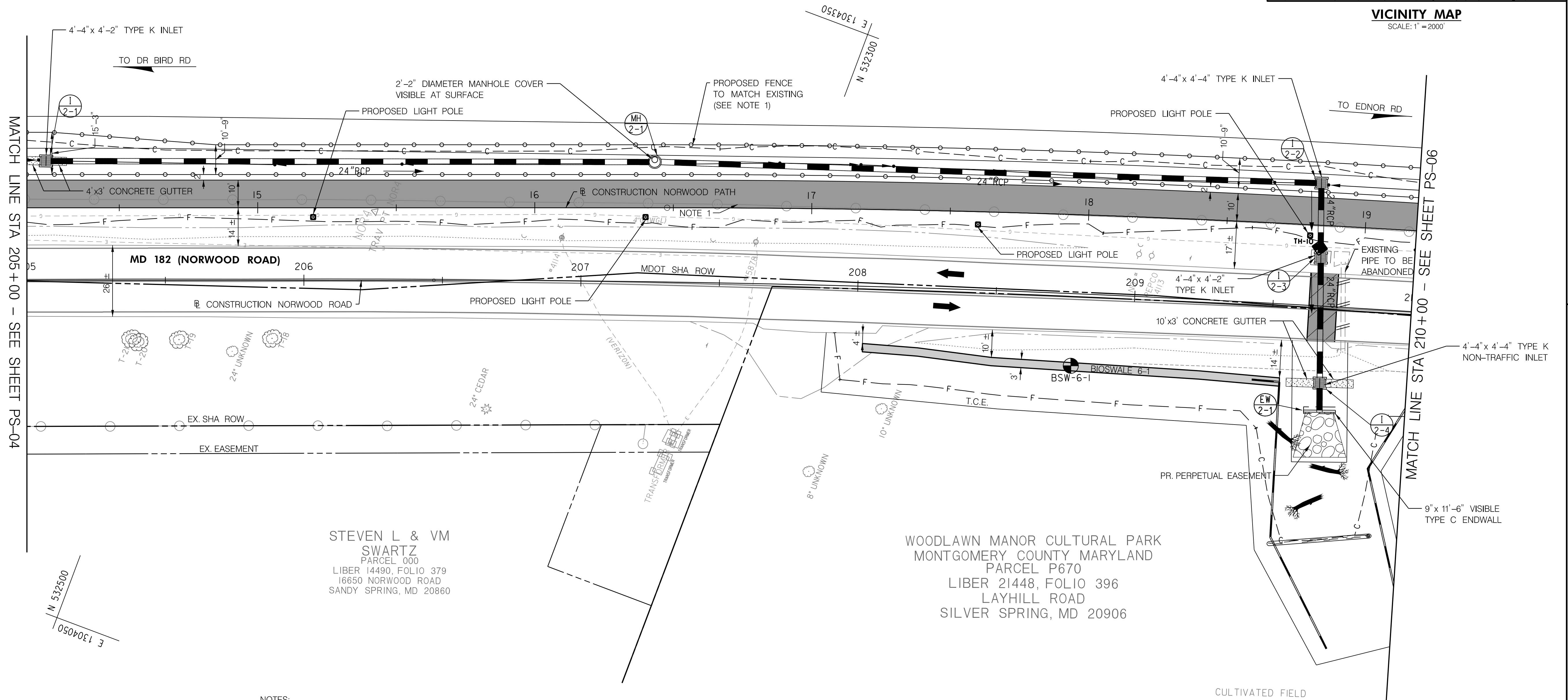
REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission
Robert A. ...

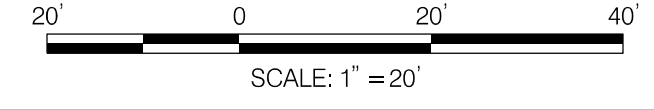
TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-10	4" GAS	532,119.8023	1,304,335.2552	417.82	422.09



WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY
PARCEL N170
LIBER 5010, FOLIO 842
16501 NORWOOD ROAD
SANDY SPRING 20860



- NOTES:**
- REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
 - FENCE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS.



LEGEND

	CONCRETE DRIVEWAY
	ASPHALT SHARED USE PATH
	FLEXIBLE SHARED USE PATH
	MILL AND OVERLAY
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	FULL DEPTH ASPHALT
	PAVEMENT / SIDEWALK REMOVAL

OWNER / ADDRESS:
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
100 EDISON PARK DRIVE
GAITHERSBURG, MARYLAND

CONTACT:
REBECCA PARK
REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
240-777-7263

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION ROCKVILLE, MARYLAND	
RECOMMENDED FOR APPROVAL	
Chief, Transportation Planning and Design Section	Date
APPROVED	
Chief, Division of Transportation Engineering	Date

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
DIVISION OF TRANSPORTATION ENGINEERING
HERITAGE TRIANGLE TRAIL PHASE 1
DR. BIRD / NORWOOD ROAD
SHARED USE PATH
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. OF 07 SHEET NO. 23 OF 101

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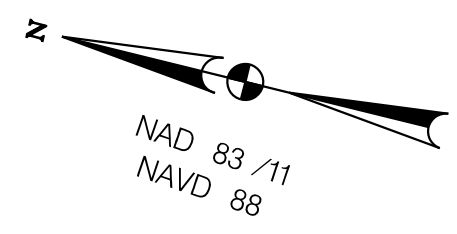
PLOTTED: 3/14/2024
FILE: \\ad.rkk.com\rs\Cloud\Projects\2020\202097_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0005_DrBird.dgn

FINE MILLING ASPHALT PAVEMENT 1 INCH TO 2.5 INCH DEPTH	
69 SY	MD 182 - STA. 211+27, LT TO STA. 212+00, LT
ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
1452 SF	MD 182 - STA. 210+00, LT TO STA. 211+47, LT
3273 SF	MD 182 - STA. 211+69, LT TO STA. 215+00, LT
WOOD RAIL FENCE	
124 LF	MD 182 - STA. 210+00, 41.2' LT TO STA. 211+21, 39.2' LT
132 LF	MD 182 - STA. 210+00, 52.0' LT TO STA. 211+30, 51.9' LT
322 LF	MD 182 - STA. 211+81, 51.7' LT TO STA. 215+00, 51.0' LT
312 LF	MD 182 - STA. 211+90, 40.9' LT TO STA. 215+00, 40.3' LT

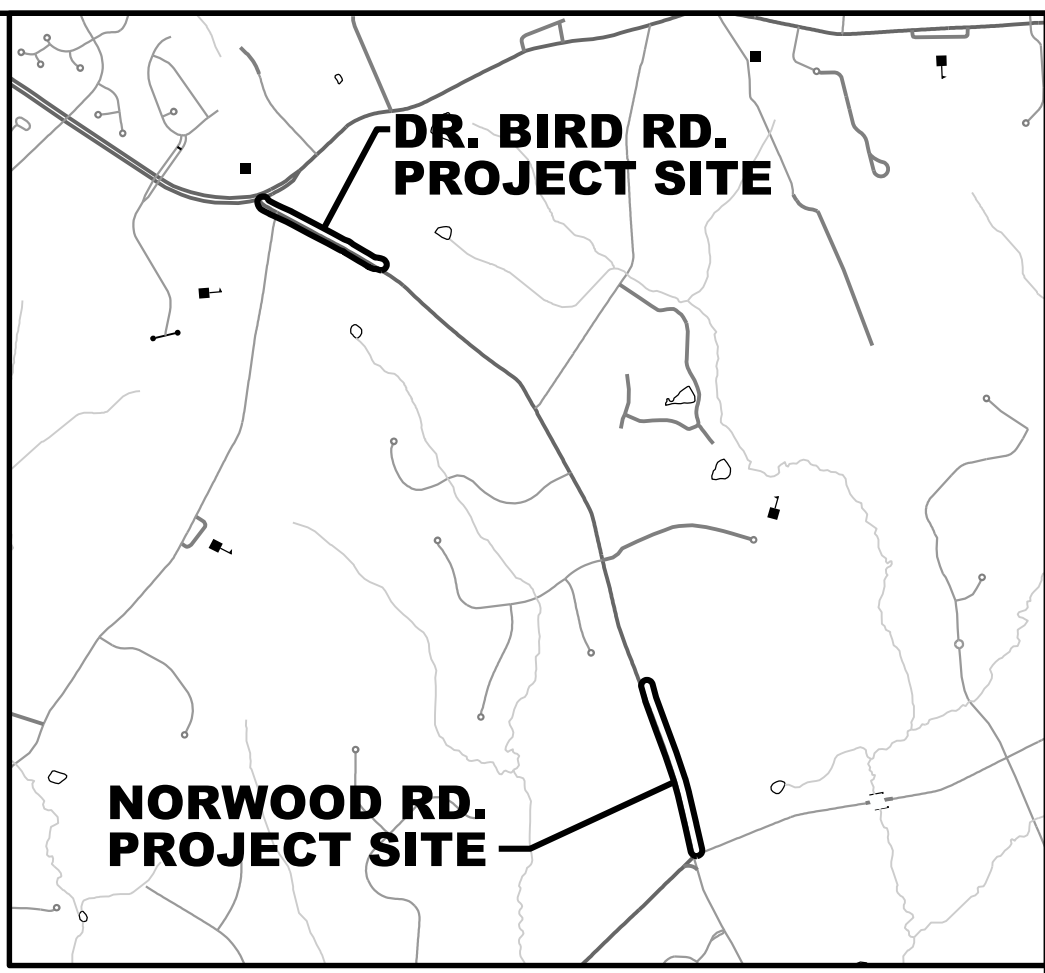
WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY
PARCEL N170
LIBER 5010, FOLIO 842
16501 NORWOOD ROAD
SANDY SPRING 20860

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

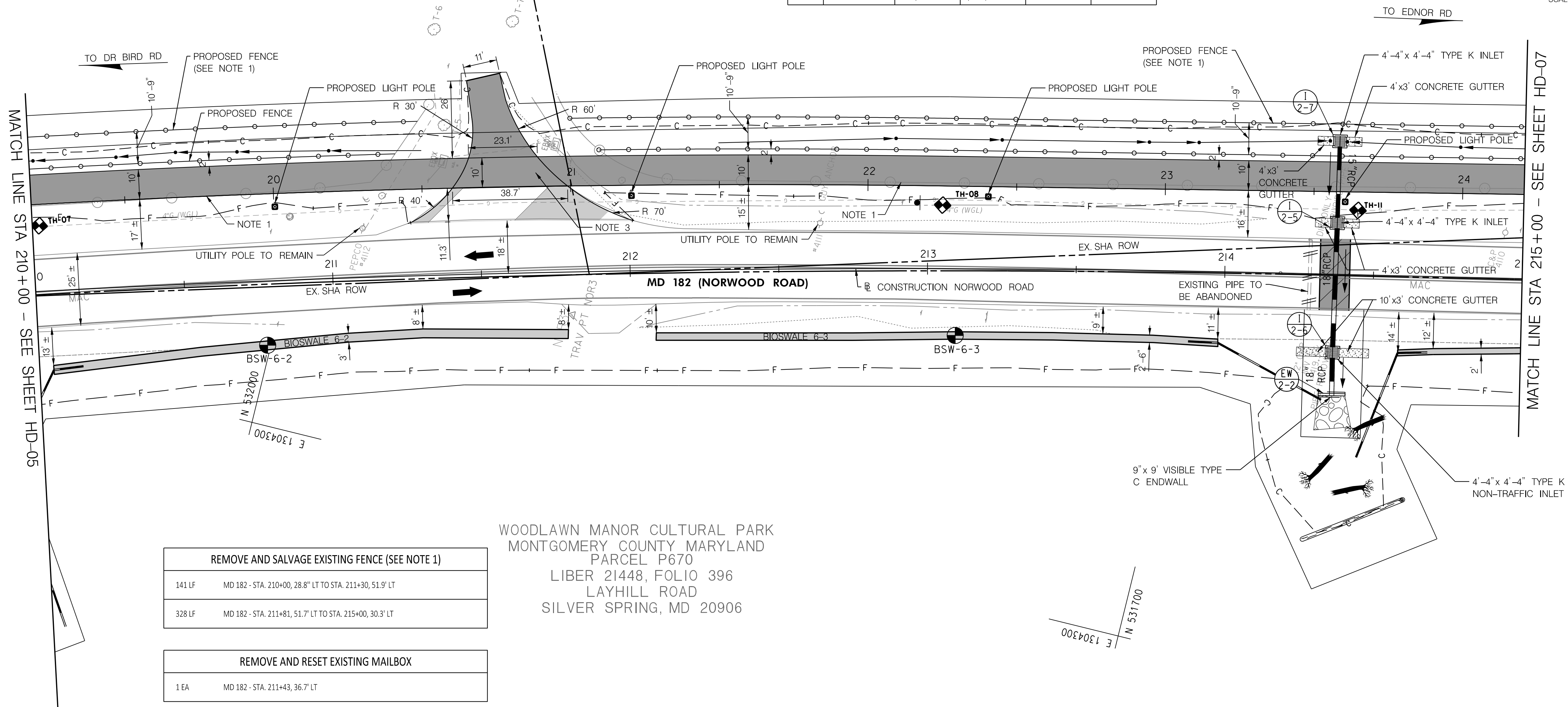
APPROVED
Montgomery County
Historic Preservation Commission
Robert H. ...



WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY
PARCEL N422
LIBER 4845, FOLIO 59
NORWOOD ROAD



TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-07	4" GAS	532,085.2900	1,304,347.0200	419.60	421.76
TH-08	4" GAS	531,791.9800	1,304,426.4900	419.22	424.74
TH-11	4" GAS	531,655.9400	1,304,458.2100	418.88	422.00

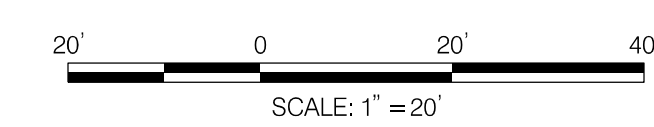


REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)	
141 LF	MD 182 - STA. 210+00, 28.8' LT TO STA. 211+30, 51.9' LT
328 LF	MD 182 - STA. 211+81, 51.7' LT TO STA. 215+00, 30.3' LT

REMOVE AND RESET EXISTING MAILBOX	
1 EA	MD 182 - STA. 211+43, 36.7' LT

WOODLAWN MANOR CULTURAL PARK
MONTGOMERY COUNTY MARYLAND
PARCEL P670
LIBER 21448, FOLIO 396
LAYHILL ROAD
SILVER SPRING, MD 20906

- NOTES:**
- REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
 - FENCE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS.
 - PROVIDE A 10' MINIMUM PEDESTRIAN PATHWAY WITH 2% MAXIMUM CROSS-SLOPE ACROSS ENTRANCE.



OWNER / ADDRESS:
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
100 EDISON PARK DRIVE
GAITHERSBURG, MARYLAND

CONTACT:
REBECCA PARK
REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
240-777-7263

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY TMB

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
DIVISION OF TRANSPORTATION ENGINEERING
HERITAGE TRIANGLE TRAIL PHASE 1
DR. BIRD / NORWOOD ROAD
SHARED USE PATH
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. PS-06 OF 07 SHEET NO. 24 OF 101

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PLOTTER: 3142024
FILE: \\ad.rk.com\rs\Cloud\Projects\2020\2097_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0006_DrBird.dgn



Photo 1: Woodlawn property and proposed SUP location, looking southeast from intersection of Norwood Road and Excalibur Way (RK&K, March 2021)



REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission
Robert A. [Signature]

g south property
(RK&K,



Photo 3: Proposed SUP location, looking southeast from just north of the Woodlawn driveway (RK&K, March 2021)



Photo 4: Existing culvert at north end of driveway (RK&K, March 2021)

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission





Photo 5: Woodlawn driveway and northern sign, looking north from the driveway (RK&K, March 2021)



Photo 6: Woodlawn driveway and southern sign, looking south from the driveway (RK&K, March 2021)

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission





Photo 7: Existing culvert south of driveway, looking southwest to the Red Door Store Property (RK&K, March 2021)



REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission
Robert A. [Signature]

Photo 8: Proposed driveway location, looking north to the Red Door Store Property (RK&K, March 2021)



Photo 9: View of existing SUP south of Ednor Road, looking southeast from the Red Door Store property (RK&K, March 2021)

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED

Montgomery County

Historic Preservation Commission

Handwritten signature of Robert A. Patton



Photo 1: Existing SUP along Norwood Road, looking southeast from north of Hennessy Terrace, similar to what will be installed along the Woodlawn property (Google Street View, November 2022)



Photo 2: Existing SUP along Norwood Road, looking north from just north of Hennessy Terrace at Norwood, similar to what will be installed along Woodlawn (Google Street View, November 2022)



Photo 3: Existing SUP along Norwood Road, looking north from Hennessy Terrace, similar to what will be installed along Woodlawn (Google Street View, November 2022)

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission

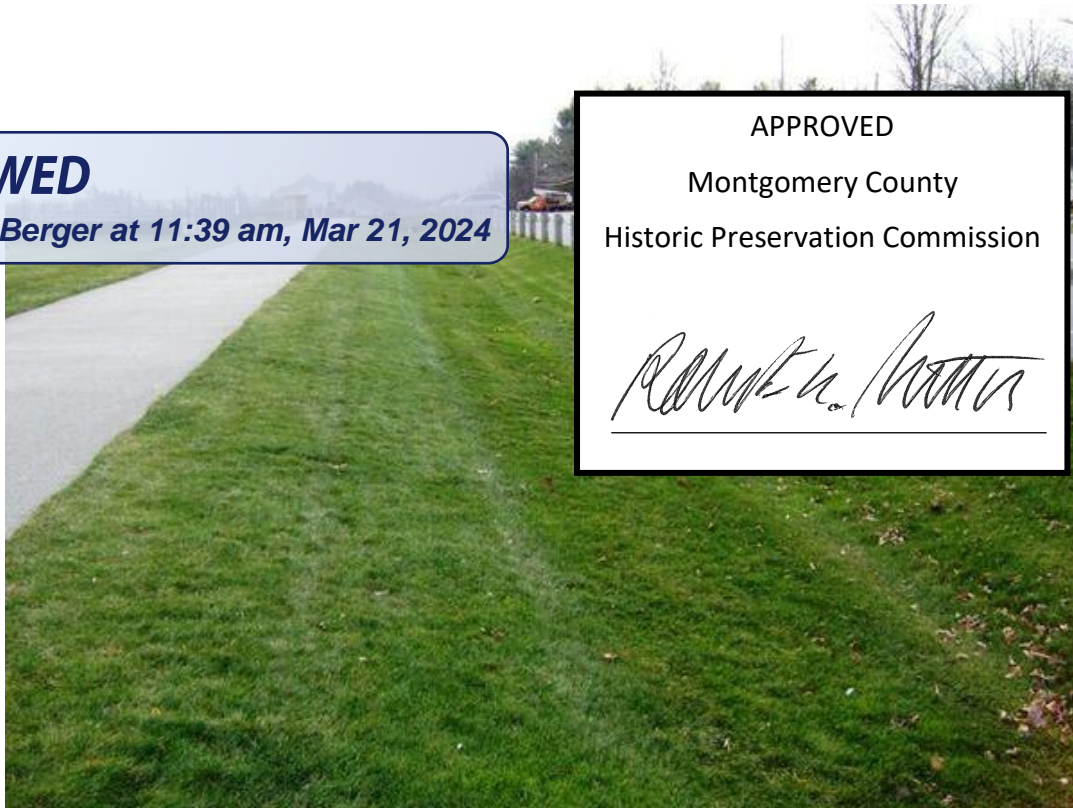


Photo 4: Image of bioswale similar to what is proposed on the Red Door Store property (RKK Photo)



Photo 5: Image of bioswale similar to what is proposed on the Red Door Store property (RK&K photo)

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED

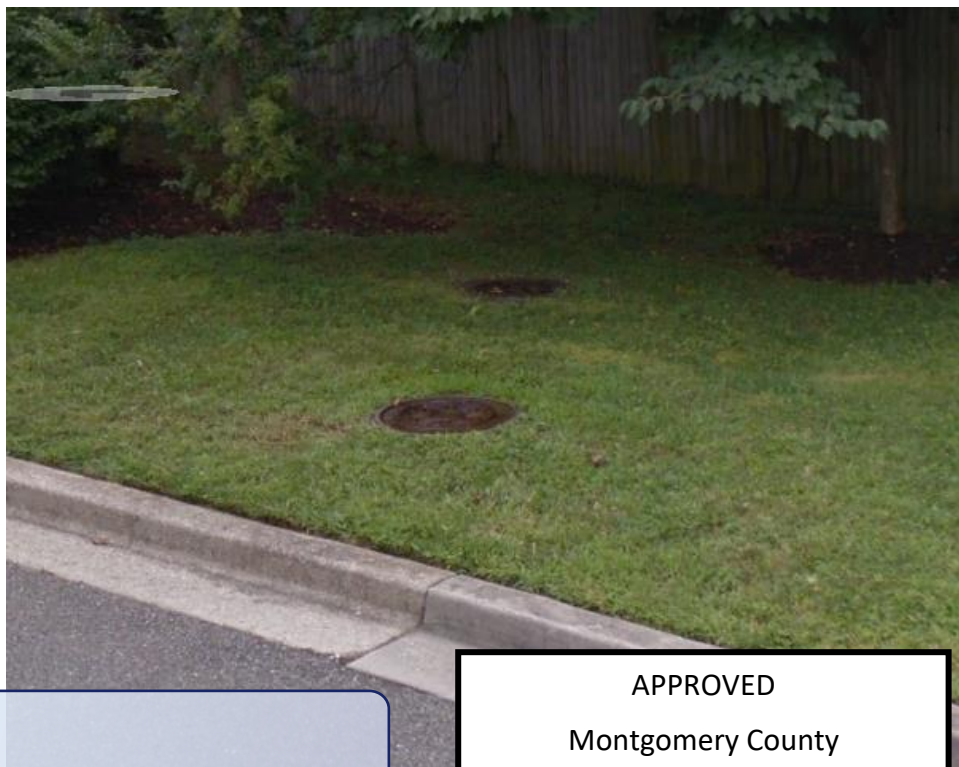
Montgomery County

Historic Preservation Commission

A handwritten signature in black ink, appearing to read "Robert H. Porter". The signature is written in a cursive style and is positioned above a horizontal line.



Photo 1: Example of a K inlet with concrete gutter. Note that the gutter is not used for all K inlets and where it is used, it may be used on both sides.



REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

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Montgomery County
Historic Preservation Commission




Photo 3: Example manhole cover.



Photo 4: Example of type C endwall with outfall protection.

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED

Montgomery County

Historic Preservation Commission

Handwritten signature of Robert H. [Name]

MONTGOMERY COUNTY, MARYLAND
DEPARTMENT OF TRANSPORTATION
TRAFFIC ENGINEERING AND OPERATIONS

JUNE 2016

RESIDENTIAL, DIRECT BURIAL FIBERGLASS POLE
ROUND, TAPERED, POST-TOP
GRAY OR BLACK

1) DESCRIPTION

The residential, round, tapered, direct burial fiberglass pole shall be made of a fiberglass reinforced composite (fiberglass filament and color pigmented resin), with a polyurethane and UV inhibitor coating, with a natural finish. This fiberglass pole is intended for use on residential roadways, walkways, and tunnels throughout Montgomery County. Any manufacturer, distributor or vendor who submits a bid shall agree to comply with these specifications and the attached drawings.

REVIEWED

DESIGN CRITERIA
By Chris Berger at 11:39 am, Mar 21, 2024

2.1 AASHTO Standards

The residential, round, tapered, direct burial requirements of the American Association of Highway Officials (AASHTO) Standard, "Specifications for Highway Signs, Luminaires and Traffic Signs."

APPROVED
Montgomery County
Historic Preservation Commission



2.2 Wind Load

The residential, round, tapered, direct burial fiberglass pole shall be designed to resist (at yield strength of the material without permanent deflection or destruction) test loads equivalent to the calculated wind loads developed by the velocity pressures of an 80 MPH wind with a 30% gust factor. A minimum safety factor of 1.82 on the yield strength shall be maintained.

2.3 Effective Projected Area (EPA)

The residential, round, tapered, direct burial fiberglass pole shall be designed

using the following assumptions:

- a) The streetlight luminaire shall be mounted at a height of 12 feet above the level of the surrounding ground (EPA of 3 Sq. Ft. +/-).
- b) One (24" x 36") traffic sign may be mounted with the sign's bottom edge 7 feet above the ground. (EPA of 6 Sq. Ft. +/-).

3) MATERIALS

- a) The residential, round, tapered, direct burial fiberglass pole shall be constructed by a winding filament process with color pigmented polyester resin impregnated into the filaments. The filament winding shall be continuously applied with uniform tension.
- b) The resin used will be color pigmented and shall be ultraviolet resistant. A highly weather resistant pigmented polyurethane coating shall be applied to the pole at a minimum thickness of 1.5 mils.

4) FINISH

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024



5) TENONS

The residential, round, tapered, direct burial fiberglass pole shall be bonded, hot-dipped galvanized steel or aluminum

6) HAND-HOLES

The residential, round, tapered, direct burial fiberglass pole shall have one 2 ½ inch x 5 inch hand-hole, with a non-metallic cover secured with a vandal-resistant, stainless steel screws.

7) POLE

7.1 Shaft

The residential, round, tapered, direct burial fiberglass pole shaft shall have a bottom pole diameter of 5.5 inches (+/- 0.1 inches), and a top pole diameter of 2.9 inches (+/- 0.1 inches)

7.2 Length

The residential, round, tapered, direct burial fiberglass pole shall have a nominal minimum luminaire mounting height of 12 feet and a maximum of 14 feet above the surrounding ground. The shaft shall be embedded a minimum of 3 feet in the ground.

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED

Montgomery County

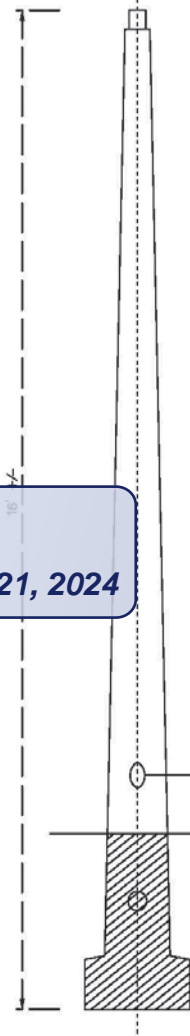
Historic Preservation Commission



Robert H. Potter

SPECIFICATIONS FOR STREETLIGHT HARDWARE

SPECIFICATIONS FOR STREETLIGHT HARDWARE



REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED

Montgomery County

Historic Preservation Commission

Robert H. Potter

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
TRAFFIC ENGINEERING AND OPERATIONS

JUNE 2016

RESIDENTIAL, COLONIAL POST-TOP,
LED OPTICS, TYPE III DISTRIBUTION, STYLE LUMINAIRE

1) PURPOSE

The purpose of these specifications is to prescribe the minimum requirements for the design, manufacture, fabrication, finishing and delivery of colonial post-top, LED optics, type III distribution, style luminaire. This luminaire is intended for use on or with the black fiberglass pole. These colonial post-tops, LED optics, type III distribution, style luminaires are intended for use along residential roadways, walkways, and tunnels throughout Montgomery County. Any manufacturer, distributor or vendor who submits a bid shall agree to comply with these specifications and attached drawings.

2) DESCRIPTION

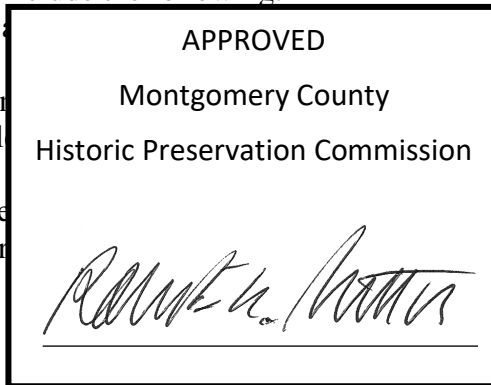
The residential, colonial post-top, LED optics, type III distribution, style luminaire is made of a cast aluminum alloy housing.

Each streetlight luminaire shall include the following:

- a) Cast aluminum housing
- b) 120 volt LED Driver;
- c) 10KV Surge Suppression Device and NEMA standard photoelectric control receptacle,
- f) Acrylic or Polycarbonate lens
- h) All necessary hardware as specified.

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024



3) DESIGN CRITERIA

3.1) AASHTO Standards

The luminaire shall meet the requirements of American Association of State Highway and Transportation Officials (AASHTO) Standard, "Specification for Structural supports for Highway Signs, Luminaires and Traffic Signals," latest edition.

3.2) Shape and Minimum Size

- a) The luminaire shall be of a trapezoidal shape. The minimum size for the luminaire shall 40.0 inches (sum of the length plus height), when viewed from the side.
- b) The luminaire shall be suitable to accommodate 120 volt LED Driver, 10KV Surge Suppression Device and NEMA standard photoelectric control receptacle and NEMA multi-volt standard photocell.

3.3) Effective Projected Area (EPA)

The luminaire shall have a maximum estimated allowable EPA for the luminaire of $0.7 \pm$ square feet.

3.4 Finish

The luminaire shall have a black polyester powder coat finish. During the finishing process, all critical openings shall be plugged to prevent contamination of the threads or reduction of other critical openings.

4) MATERIALS

4.1 Housing

The luminaire shall consist of a water tight housing fabricated from die-cast aluminum with a gasketed die-cast aluminum canopy. The canopy shall be hinged on one side and secured on the opposite side with a captive stainless steel screw. All castings used to fabricate the luminaire housing shall be clean and smooth with details defined and true to pattern. The housing shall be suitable to accommodate 120 volt LED Driver, 10KV Surge Suppression Device and NEMA standard photoelectric control receptacle and NEMA multi-volt standard photocell.

4.2 Driver & Surge Protection

The driver shall be mounted to facilitate easy removal for maintenance operations. The driver shall be equipped with a 10KV Surge Protection and suppression system. All electrical connections shall be polarized and of plug-in design. The driver shall be wired to receive 120 volt AC current. The driver shall reliably start and operate the lamp in ambient temperatures down to minus 30 degrees. The terminal block shall be capable of accepting up to a #6 AWG wire.

4.3 LED Color Temperature (CCT) and Rendering Index (CRI)

The Correlated Color Temperature (CCT) shall be a nominal Kelvin Temperature of $3500K \pm 200K$ with a minimum Color Rendering Index (CRI) of 70.

4.4 Photoelectric Cell

The photocell receptacle shall be mounted for easy access and maintenance. The photocell shall be of the NEMA twist-lock type.

4.5 Side refractor panels

The luminaire shall be equipped with acrylic or polycarbonate resin refractor panels, with spring loaded retainer clips to hold refractor panels.

4.6 Slip Fitter

The slip fitter shall have a nominal inside diameter of 3.375 inches +/- 0.25 and shall be secured to the lamp post tenon with three or four evenly spaced set screws. The slip fitter shall accommodate a tenon 3.0 inches long.

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

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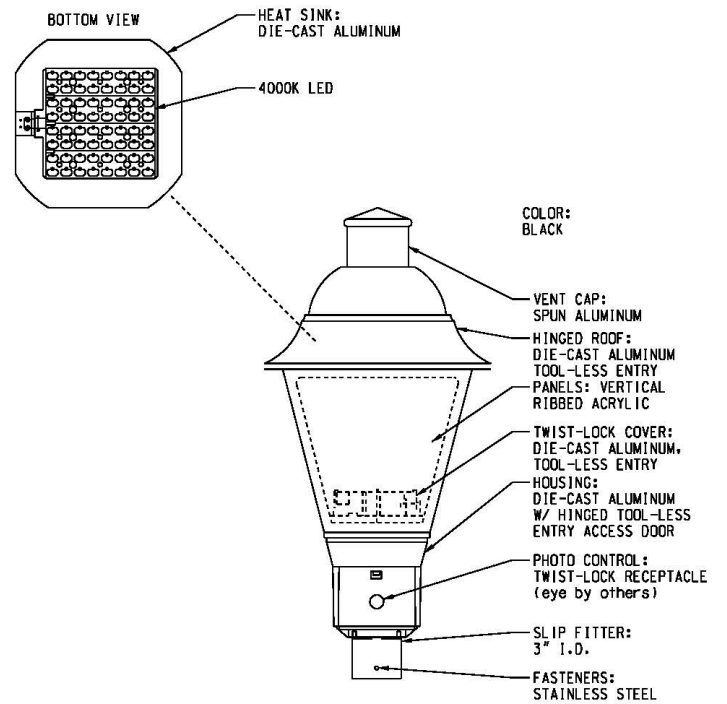
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SPECIFICATIONS FOR STREETLIGHT HARDWARE

IFB # 1063092

SPECIFICATIONS FOR STREETLIGHT HARDWARE



REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

APPROVED

Montgomery County

Historic Preservation Commission


November 15, 2022

Mr. Christopher Conklin, PE, Director
Montgomery County Department of Transportation
Executive Office Building
101 Monroe Street, 10th Floor
Rockville, Maryland 20850

Dear Mr. Conklin:

At its regularly scheduled meeting on November 3, 2022, the Montgomery County Planning Board voted to provide the following comments for Mandatory Referral No. 20220024 by a vote of 4:0 (Commissioner Piñero was absent), for the design of sidepath improvements on Doctor Bird Road and Norwood Road between Olney-Sandy Spring Road (MD 108) and Layhill Road/Ednor Road:

1. Construction plans must be submitted to the Montgomery Parks Department for review as part of the Park Construction Permit process to ensure that all work is performed in accordance with M-NCPPC standard details, specifications, and policies. No work on parkland may occur until an approved Park Construction Permit is issued for the project.
2. The final Memorandum of Understanding regarding access and maintenance must be agreed to and finalized between MCDOT and M-NCPPC before the issuance of a Park Construction Permit. MCDOT will be responsible for maintaining the asphalt sidepath facility and the proposed lighting.
3. MCDOT shall continue to coordinate with M-NCPPC on the design of the path and other elements including lighting, user safety elements, stormwater culvert and outfalls, stormwater bioswales, security cameras, double fence design including a mow strip, and natural resource protection and mitigation.
4. Mitigation for impacts to Park trees (with a 6" diameter at breast high or greater) damaged or removed, shall either be (1) replacement planting on parkland at a rate of one inch to one inch diameter or (2) a monetary per inch caliper basis at the rate of \$100/diameter inch, to be paid to Montgomery Parks prior to completion of construction.
5. Where the sidepath crosses driveways, maintain the sidepath's elevation across the driveway. Where possible, the sidepath should be pu
6. Given the open section cross section propo existing utility poles close to the road's ed perspective should be given to relocating

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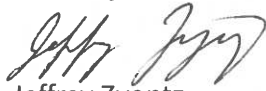
REVIEWED

By Chris Berger at 11:42 am, Mar 21, 2024

Mr. Christopher Conklin, PE, Director
November 15, 2022
Page Two

The Planning Board appreciates the opportunity to review this project and continue coordinating with your staff on this and future projects. If you have questions, please contact Stephen Aldrich at (301) 495-4528 or Stephen.Aldrich@MontgomeryPlanning.org.

Sincerely,



Jeffrey Zyontz

Chair

cc: Stephen Aldrich, Planner IV, M-NCPPC
Jason Sartori, Chief, M-NCPPC
Dan Sheridan, MCDOT

Enclosure:

M-NCPPC Staff Mandatory Referral Staff Report

JZ:SA:aj

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By Chris Berger at 11:42 am, Mar 21, 2024

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HERITAGE TRIANGLE TRAIL – PHASE 1
DOCTOR BIRD ROAD/NORWOOD ROAD SIDEPATH
MANDATORY REFERRAL NO. 2022024

Description

Montgomery County Department of Transportation (MCDOT) is proposing to contract a sidepath for pedestrian and bicycle use along two sections of MD 182 – 1,400 feet along Doctor Bird Road starting at its intersection with Olney Sandy Spring Road (MD 108) and ending at the Norwood Road intersection with Layhill Road. The project is located in the Olney/Sandy Spring area and will be integrated into the network at the project limits.

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

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Montgomery County
Historic Preservation Commission



No. MR2022024

Completed: 10-06-2022

MCPB

Item No. 9

November 3, 2022

Montgomery County

Planning Board

2425 Reddie Drive,

Floor 14

Wheaton, MD 20902

Planning Staff

SA	Stephen Aldrich, PE, Planner IV, Countywide Planning & Policy Division, Stephen.Aldrich@MontgomeryPlanning.org , 301-495-4528
DS	Douglas Stephens, Principal Natural Resources Specialist, Park Planning & Stewardship, Douglas.Stephens@MontgomeryPlanning.org , 301-650-4367
JS	Jason Sartori, Chief, Countywide Planning & Policy Division, Jason.Sartori@MontgomeryPlanning.org , 301-495-2172

LOCATION

Doctor Bird Road starting at Olney Sandy Spring Road (MD 108) and running south a distance of 1,400 feet, and Norwood Road from Layhill Road/Ednor Road intersection running north a distance of 1,700 feet (adjacent to Woodlawn Manor Cultural Park)

REVIEWED

MAST **By Chris Berger at 11:39 am, Mar 21, 2024**

1998 Sandy Spring Ashton Master Plan, 2018 Bicycle Master Plan, 2018 Master Plan of Highways and Transitways

APPLICANT

Montgomery County Department of Transportation

ACCEPTANCE DATE

August 22, 2022

REVIEW BASIS

20-302 of the Land Use Article (Mandatory Referral)

Summary:

- Montgomery County Department of Transportation to construct sidepath improvements along a 1,400 foot section of Doctor Bird Road starting at Olney-Sandy Spring Road (MD 108) heading to the south and a 1,700 foot-long section of Norwood Road starting at the intersection of Norwood Road with Layhill Road/Ednor Road intersection heading north. For construction of the sidepath on Doctor Bird Road, the project was reviewed by the Montgomery County Historic Preservation Commission. Staff Reference: MR2022024

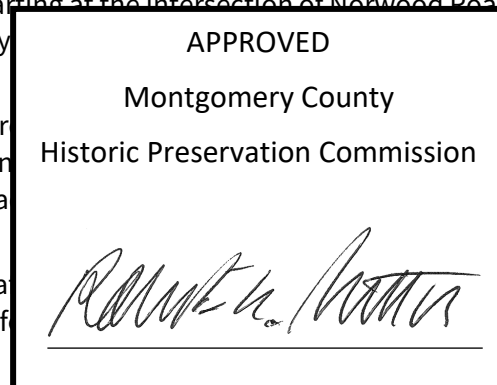


TABLE OF CONTENTS

MANDATORY REFERRAL REVIEW BASIS 3

SECTION 1: RECOMMENDATIONS SUMMARY 3

SECTION 2: PROJECT DESCRIPTION..... 4

PROPOSED DESIGN PROJECT 4

PEDESTRIAN AND BICYCLE SAFETY IMPACT 4

TYPICAL SECTIONS 8

STORMWATER MANAGEMENT AND DRAINAGE..... 9

PHASING SCHEDULE..... 10

PUBLIC LAND 10

FUNDING..... 11

LIGHTING 11

MEMORANDUM OF UNDERSTANDING..... 12

IMPACTS TO MONTGOMERY PARKS PROPERTIES 13

PUBLIC OUTREACH 13

SECTION 3: 35% DESIGN PLANS FINDINGS AND ANALYSIS 13

DESIGN ELEMENTS - TRANSPORTATION 14

ENVIRONMENTAL REVIEW..... 14

PARKLAND IMPACTS 16

HISTORIC PRESERVATION..... 22

MASTER PLAN CONFORMANCE..... 23

SECTION 4: COMMUNITY OUTREACH 23

SECTION 5: CONCLUSION 23

ATTACHMENTS 23

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By Chris Berger at 11:39 am, Mar 21, 2024

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MANDATORY REFERRAL REVIEW BASIS

This proposal for the construction of pedestrian and bicycle improvements is required to undergo the Mandatory Referral review process under the Montgomery County Planning Department's Uniform Standards for Mandatory Referral Review. State law requires all federal, state, and local governments and public utilities to submit proposed projects for a Mandatory Referral review by the Commission. The law requires the Planning Board to review and approve the proposed location, character, grade and extent of any road, park, public way, or ground, public (including federal) building or structure, or public utility (whether publicly or privately owned) prior to the project being located, constructed, or authorized. This review is typically performed in context with the relevant master plans, guidelines, and policies in effect.

Planning Staff acknowledges that the implementation of master plan transportation recommendations is a challenge faced by the Applicant in developing design plans to convert desired master plan recommendations into engineering design drawings. The design process up to 30/35 percent design typically brings clarity with considerably more detail than considered during a master plan, and issues such as environmental impacts, historic impacts, and construction costs may introduce new factors that need to be weighed in developing a final design solution. It is the intention of the Board that the Mandatory Referral process aids in this process to develop an optimal or at least an improved design solution.

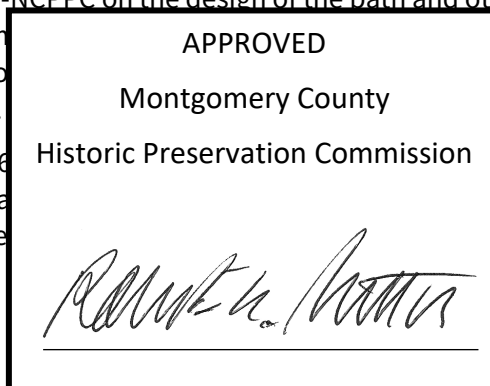
SECTION 1: RECOMMENDATIONS SUMMARY

Planning Staff recommends approval of the Heritage Triangle Trail – Phase 1 Sidepath 35 percent design plans (MR2022024) with the following comments.

1. Construction plans must be submitted to the Parks Department for review as part of the Park Construction Permit process to ensure that all work is performed in accordance with M-NCPPC standard details, specifications, and policies. No work on parkland may occur until an approved Park Construction Permit is issued for the project.
2. The final MOU regarding access and maintenance must be agreed to and finalized between MCDOT and M-NCPPC before the issuance of a Park Construction Permit. MCDOT will be responsible for maintaining the asphalt sidepath facility and the proposed lighting.
3. MCDOT shall continue to coordinate with M-NCPPC on the design of the path and other elements including lighting, user safety elements, stormwater bioswales, security cameras, do natural resource protection and mitigation.
4. Mitigation for impacts to Park trees (with a e either be (1) replacement planting on parkland a monetary per inch caliper basis at the rate Parks prior to completion of construction.

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By Chris Berger at 11:39 am, Mar 21, 2024



5. Where the sidepath crosses driveways, maintain the sidepath’s elevation across the driveway. Where possible, the sidepath should be pulled away from the roadway at driveways.
6. Given the open section cross section proposed along Norwood Road and the proximity of existing utility poles close to the road’s edge on the east side, consideration from a Vision Zero perspective should be given to relocating or undergrounding these utilities in the future.

SECTION 2: PROJECT DESCRIPTION

PROPOSED DESIGN PROJECT

The Montgomery County Department of Transportation (MCDOT) recently completed the 35% design phase of the Heritage Triangle Trail - Phase 1 Doctor Bird / Norwood Road Sidepath Project. The project provides a well-connected pedestrian and bicycle network sidepath along Doctor Bird Road (MD 182) and Norwood Road (MD 182). The project limits, shown below in Figure 1, complete two missing links in the bicycle network and provide a pedestrian and bicycling space where none exists today by adding the following:

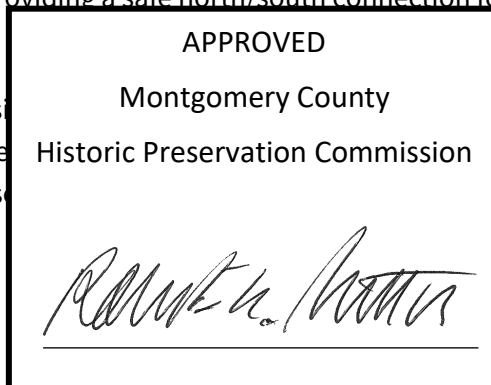
- Approximately 1,400 feet of new shared-use path along MD 182 starting at MD 108 (Olney Sandy Spring Road) and traveling south towards Norwood Road.
- Approximately 1,700 feet of new shared-use path along MD 182 starting at the driveway of The Adventure Park at Sandy Spring and traveling south to Ednor Road.
- Lighting, stormwater management facilities, and any required signal/crosswalk improvements at the intersections of MD 108 / MD 182 and Ednor Road / MD 182.

Completing the missing links in the bicycle network will provide a connection between several existing Maryland Heritage Sites along the MD 182 corridor, providing access to residential areas, transit, commercial districts, schools, places of worship, parks, and recreational areas.

PEDESTRIAN AND BICYCLE SAFETY IMPACT

This project is anticipated to significantly improve pedestrian and bicycle safety in the project area. In addition to improvements along Doctor Bird Road and Norwood Road, this project will complete two missing links in the larger bicycle network, providing a safe north/south connection for bicyclists and pedestrians in the Sandy Spring area.

In addition, the grading associated with the sidepath where it crosses the road will meet all safety requirements. This will allow motorists to safely pass bicyclists and pedestrians, improving safety for all parties.



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By Chris Berger at 11:39 am, Mar 21, 2024

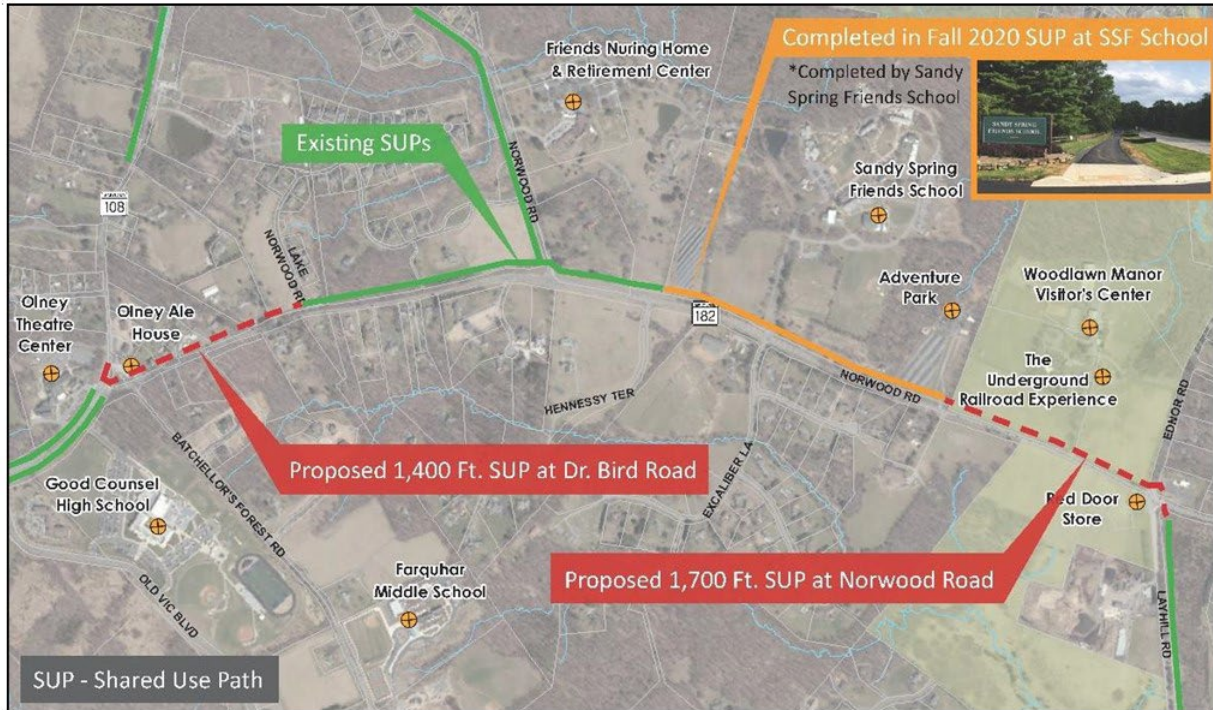


Figure 1 – Vicinity map

Doctor Bird Road - The existing roadway (see Figure 2) has the intersection of MD 182 / MD 108, resulting in bicyclists sharing narrow lanes with vehicles. The proposed sidepath (see Figure 3) will provide a dedicated facility to accommodate bicycle users who are not comfortable riding in a shared lane on a narrow roadway. It will also accommodate pedestrians through an area that currently does not have any separated pedestrian space. The path will connect to existing sidepaths at both termination points, improving the overall transportation network.

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By Chris Berger at 11:39 am, Mar 21, 2024

Norwood Road - The existing roadway (see Figure 4) contains a narrow shoulder, which results in bicyclists sharing narrow lanes with vehicles. The proposed sidepath (see Figure 5) will provide an off-road separated bicycle facility to accommodate bicycle users who are not comfortable riding in a shared lane on a narrow roadway. It will also accommodate pedestrians through an area that currently does not have any separated pedestrian space. The path will connect to existing sidepaths at both termination points, improving the overall transportation network.

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Robert H. [Signature]



Figure 2 – Doctor Bird Road

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By Chris Berger at 11:39 am, Mar 21, 2024

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Figure 3 – Doctor Bird Road: Proposed



Figure 4 – Norwood Road: Existing



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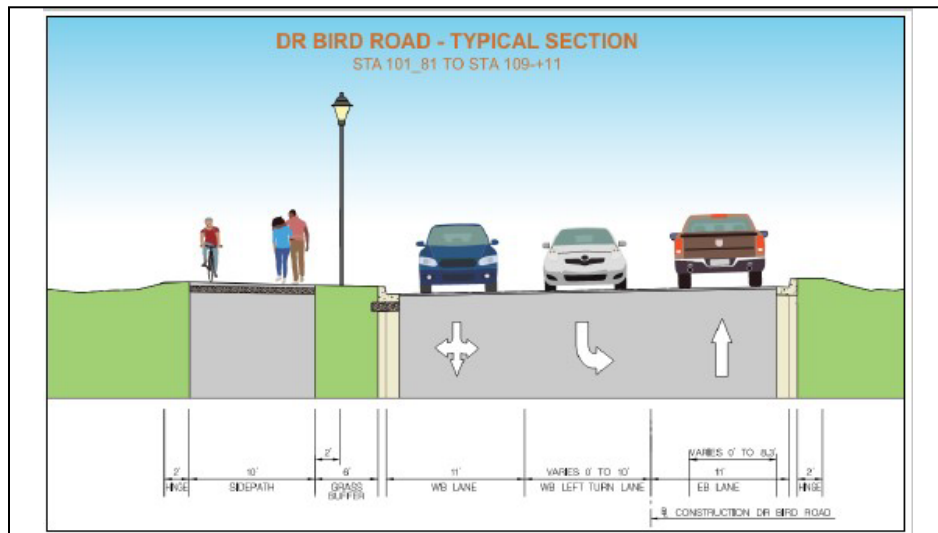
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Figure 5 – Norw

TYPICAL SECTIONS

Doctor Bird Road – Doctor Bird Road (MD 182) is classified as a two-lane Major Highway in the Master Plan of Highway and Transitways (MPOHT). The existing typical section consists of one eastbound through lane (11' to 12') and one westbound through lane (11'). There is a westbound left-turn lane approaching the intersection of Doctor Bird Road with Olney Sandy Spring Road (MD 108). The immediate area has a rural character.

Under the proposed condition, the existing roadway alignment will be shifted south, near the MD 182 / MD 108 intersection, to provide space for a buffer separated sidepath on the adjacent westbound lane. The proposed typical section will have a lane configuration identical to existing conditions with one eastbound 11-foot eastbound through lane, one 11-foot westbound through lane, a six-foot grass buffer, and an additional 10-foot sidepath adjacent to the westbound lane. Details are provided below:



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By Chris Berger at 11:39 am, Mar 21, 2024

Figure 6 – Doctor Bird Road Proposed Typical Sections

Norwood Road – Norwood Road (MD 182) is classified as a two-lane Major Highway in the MPOHT. The existing typical section consists of one northbound lane (11' to 13') and one southbound lane (10' to 14') without shoulders or safety grading. The immediate area has a rural character.

Under the proposed condition, the existing roadway will not be impacted. The proposed typical section will add a variable width buffered grass shoulder and variable width bio-swale adjacent to the southbound lane as well as a variable width buffered grass shoulder, 10-foot sidepath with buffered grass shoulders on each side, and a variable width drainage ditch adjacent to the northbound lane.

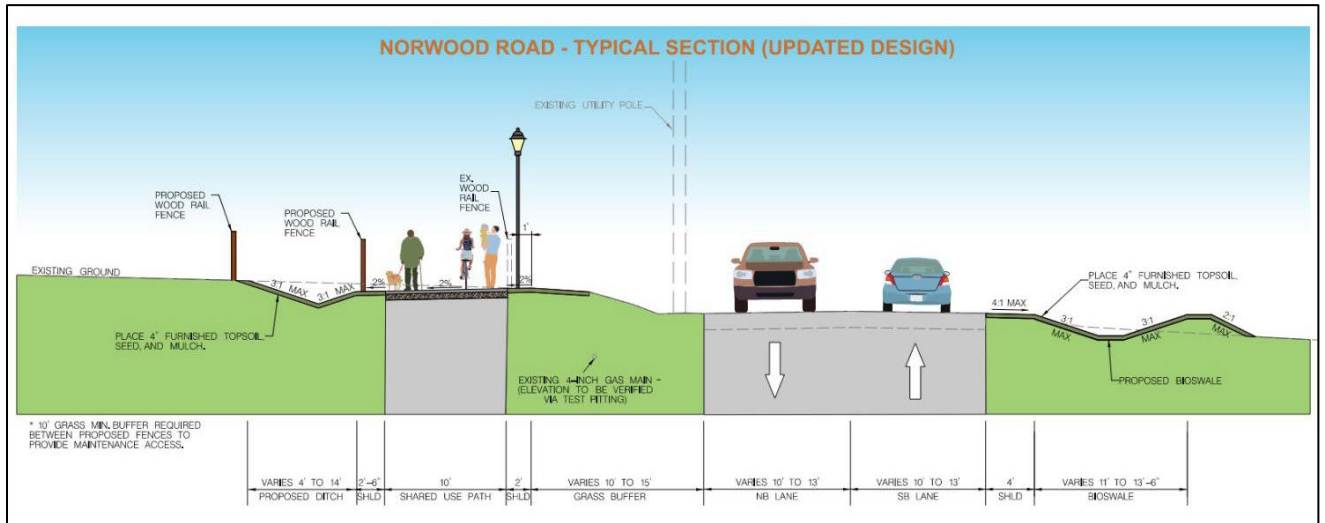


Figure 7 – Norwood Road Proposed Typical Section

STORMWATER MANAGEMENT AND DRAINAGE

The Applicant has provided full stormwater management treatment for this project; however, due to surrounding residential properties, utilities, constrained right-of-way (ROW), and the existence of high-value park properties adjacent to portions of the proposed path, locations to provide stormwater management are limited. Through previous discussions with M-NCPPC staff, it was decided that stormwater management treatment would be provided on the southbound side of Norwood Road. This limits the ability to treat stormwater on the northbound side of the road. These facilities have been maximized to the extent possible within the project area.

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By Chris Berger at 11:39 am, Mar 21, 2024

The project provides environmental site design (ESD) for the project area through the provision of one (1) Micro-bioretention facility and four (4) Bioswale facilities. The one micro-bioretention facility will be built within the project area while the bioswales will be built on parks property adjacent to the project area.

Management details. Though all available space within the project area is utilized to provide as much stormwater management treatment as possible, these 5 facilities still do not treat the full stormwater

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management requirement. Therefore, MCDOT will be seeking a waiver for the treatment that cannot be provided.

The proposed work will not impact any stream buffers, floodplains, or wetlands. The project site drains to the Watershed of the Anacostia River (02140205) to the South and the Watershed of Rocky Gorge Dam (02-13-11-07) to the North.

Drainage design within the project area is limited to replacing existing roadside drainage amenities in-kind along Doctor Bird Road. Along Norwood Road, a drainage ditch is proposed and designed between the newly proposed shared-use path and the roadway. This ditch provides conveyance to existing cross-culverts, which will be replaced and upgraded under this task to meet current criteria. The outfalls of these cross culverts are being upgraded with a riprap design to provide a stable outfall for crossflows and the proposed bioswales, which outfall into these areas.

PHASING SCHEDULE

This project represents Phase 1 of the Heritage Triangle Trail, a phased project that will connect four heritage tourism destinations—the Olney Theatre Center, Sandy Spring Museum, Underground Railroad Experience Trail Hike, and Woodlawn Manor Cultural Park—via a sidepath for bicyclists and pedestrians. Approximately one mile of trail exists along the eastern portion of Doctor Bird Road/Norwood Roads. A segment along the Sandy Spring Friends School’s Norwood Road frontage was recently completed in 2020. The current phase

is testing trail while establishing a new connection to the existing trail. An overview map is provided in Figure 8.

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By Chris Berger at 11:39 am, Mar 21, 2024

Following construction of Phase 1, the team is planning to complete Phase 2 of the Heritage Triangle Trail. The goal would be to initiate design work on the remaining segments in 2024.



PUBLIC LAND

The proposed sidepath will be constructed largely within the existing state and county right-of-way; however, some parts of the path along Doctor Bird Road will be installed on private residential property. Additionally, portions of the path along Norwood Road will be installed on Montgomery Parks property. The project team has been coordinating with both groups of stakeholders regarding the project design and will prepare full right-of-way/ easement plats as part of the final design process. Temporary construction easements will be required on both the private residential properties and Montgomery Parks property.

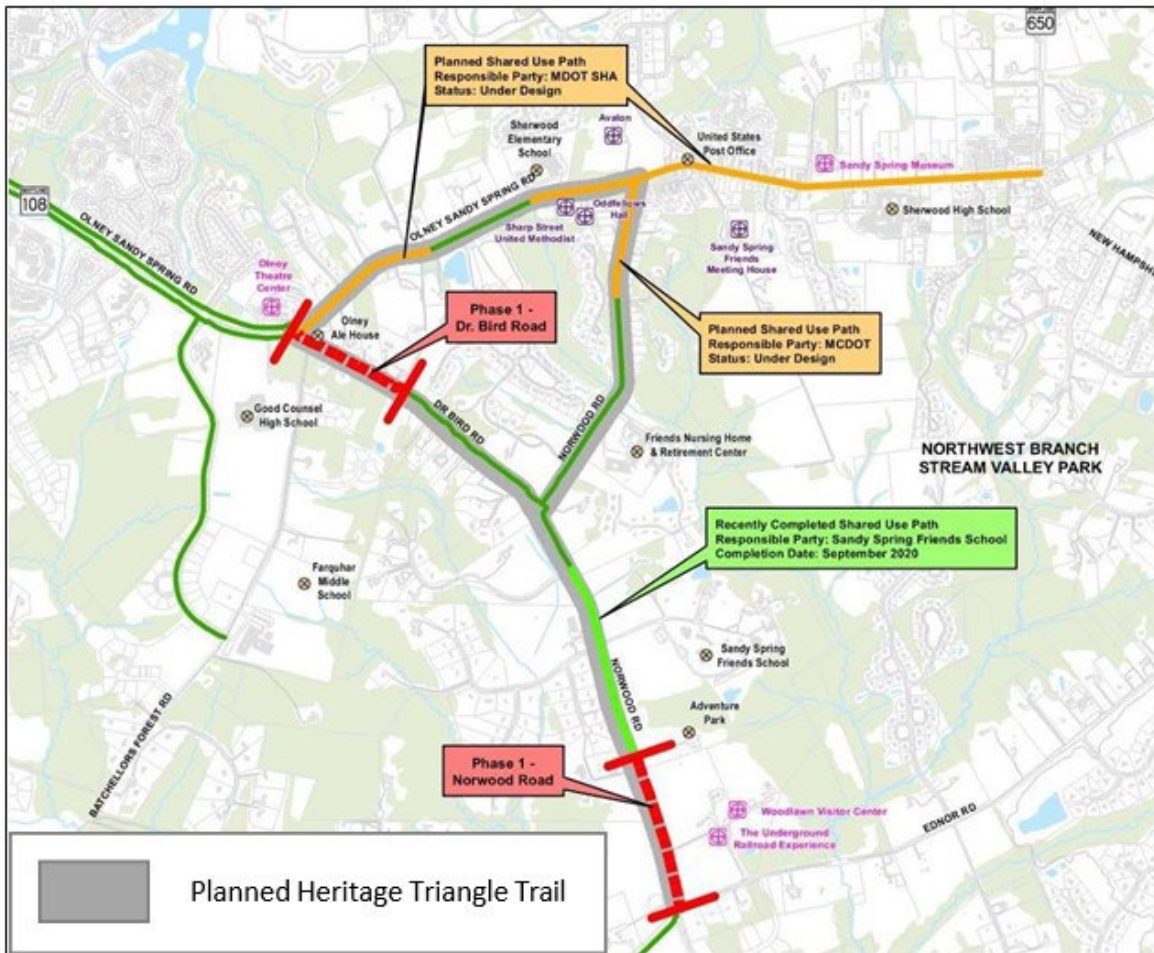


Figure 8 – Heritage Triangle Trail Overview Map

FUNDING

The design and construction of the project is currently funded by MCDOT with a contribution of \$100,000 from the Maryland Heritage Areas Authority Grant Program.

LIGHTING

The initial design plans for this project included pe... installed sidepath. During the project's stakehold... the installation of lighting along the Norwood Ro... and sev... the lighting concerns identified. Following severa... would be acceptable along Norwood Road under

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 By Chris Berger at 11:39 am, Mar 21, 2024

- Lighting Fixtures – MCDOT agreed to select a colonial post lighting fixture that met Parks’ requirements for light transmission (dark sky compliant, shielding, low wattage LED bulbs)
- Lighting Maintenance – MCDOT agreed to maintain the lighting fixtures
- Lighting Continuity – MCDOT agreed to coordinate the future lighting of currently un-lit portions of the surrounding sidepaths that are a part of the proposed Heritage Triangle Trail system (Sandy Spring Friends School Path and the sidepath directly north of this newly constructed path)
- Trail Counters – MCDOT agreed to install bicycle/pedestrian trail counters along the sidepath

MCDOT reviewed several proposed fixtures with Parks and it was agreed that the MCDOT standard colonial post fixture would be acceptable. Regarding the lighting continuity issue, MCDOT and Parks agreed to the following conditions per an email from Montgomery Parks in Spring 2022:

“The installation of lighting on parkland along Norwood Road is contingent upon lighting the adjacent path sections, North Path and Sandy Spring Friends School, concurrently. If lighting the adjacent path sections (North Path and Sandy Spring Friends School) is not to be included in HTT Phase 1, then conduit only will be approved for the parkland section at this time, with delayed approval for light fixtures when funds are procured for the adjacent path lighting to be installed concurrently. Parks’ goal is to provide lighting uniformity along the route that doesn’t lead trail users to unsafe dark zone.”

MCDOT AGREES TO THESE CONDITIONS AND CONFIRMED THAT THE LIGHTING FOR THE CURRENTLY UN-LIT AREAS WILL BE INSTALLED UNDER A SEPARATE CONTRACT SUBJECT TO THE APPROPRIATION OF FUNDS. ALL FUTURE LIGHTING PLANNED IS SUBJECT TO APPROPRIATION. MEMORANDUM OF UNDERSTANDING

In addition to the lighting coordination described in the previous section, MCDOT and Montgomery Parks conducted extensive coordination regarding the proposed project design and the division of maintenance responsibilities for the new project facilities. Because a significant portion of the planned sidepath, drainage facilities, and stormwater management facilities fall on parks property, the team initiated preliminary development on how the assets would be maintained following the completion of construction. Below is an outline of the preliminary responsibilities as discussed by the team:


- **Sidepath Facility – MCDOT:** MCDOT will be responsible for maintaining the asphalt sidepath facility. This will include repairs to the sidepath facility. Maintenance required.

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By Chris Berger at 11:39 am, Mar 21, 2024

- **Drainage Facility – Parks:** Montgomery Parks will be responsible for installing and maintaining drainage facilities installed on Parks property. This will include any other associated maintenance for the drainage facilities and any other associated maintenance for the drainage facilities. Parks is responsible for structural maintenance (including fences/railings located along the path).

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- **Stormwater Management Facility – Parks:** Montgomery Parks will maintain the stormwater management facilities on Parks property. This includes managing the vegetation in the bioswale, removing trash/debris, and inspecting and maintaining outfalls to ensure they are not blocked. The Department of Environmental Permitting (DEP) will be responsible for structural maintenance per the existing agreement between Parks and DEP (Chapter 19, Article II).
- **Path Lighting – MCDOT:** Currently MCDOT and Montgomery Parks are coordinating regarding project lighting. MCDOT will maintain the lighting if standard Montgomery County light posts and fixtures are installed. MCDOT and the project team are currently researching and investigating ways to amend the contract and maintain lighting if timers are included.
- **Security System – Parks:** Montgomery Parks to own and maintain security cameras installed along the sidepath.

All parties reviewed and acknowledged that this outline should be considered a starting point for the detailed MOU that will be developed to document the final maintenance responsibilities. MCDOT will continue to coordinate with Montgomery Parks and all associated stakeholders to develop, review, and finalize a maintenance MOU for this project.

IMPACTS TO MONTGOMERY PARKS PROPERTIES

This project will have an impact on the Woodlawn Manor Park and MCDOT has been coordinating closely with M-NCPPC, Montgomery County Parks, and the Park Police to incorporate these stakeholders’ concerns into its preliminary design. During early coordination with Parks, MCDOT redesigned the preliminary stormwater management system to shift the facilities to the west side of Norwood Road and significantly limit the property impacts to Woodlawn Manor Park. Other than the impacts to Woodlawn Manor Park, there are no additional impacts to M-NCPPC property.

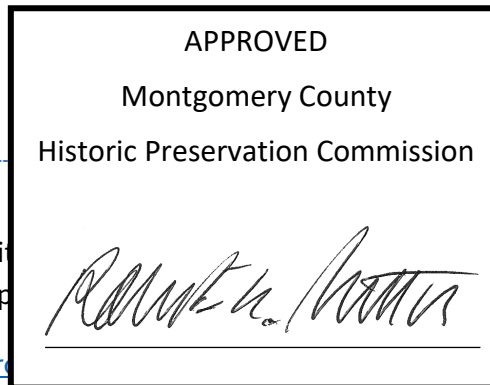
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By Chris Berger at 11:39 am, Mar 21, 2024

PUBLIC OUTREACH

A newsletter was mailed to residents within the vicinity and a public meeting was held on February 25, 2021. A link to the project information is available at the following URL:

<https://www.montgomerycountymd.gov/dot-dte/pr>



al public

The project team is continuing to coordinate with all project stakeholders as the design is finalized.

SECTION 3: 35% DESIGN PLANS FINDINGS AND ANALYSIS

The primary purpose of the proposed facility is to improve connectivity and safety for both pedestrians and bicyclists along Doctor Bird and Norwood Roads. Providing a ten-foot-wide sidepath facility parallel to Doctor Bird and Norwood Road in two distinct segments will fill in missing sidepath segments and improve the safety for users along the corridor and reduce the potential for crashes by minimizing conflict points with vehicles. The facility will improve the level of comfort for users of all ages and abilities by providing a dedicated facility with horizontal separation from the roadway in the form of a proposed grass buffer with a minimum width of five feet (primarily six feet wide or better) between the roadway and the proposed facility. The proposed facility will connect to existing shared use facilities on Olney Sandy Spring Road (MD 108) to the north and Layhill Road to the south.

DESIGN ELEMENTS - TRANSPORTATION

ROADWAY DESIGN

Doctor Bird Road and Norwood are considered to be Country Connectors with a 40-mph target speed under the Planning Board-approved Complete Streets Design Guide (CSDG). The road currently has a 35-mph posted speed along its length.

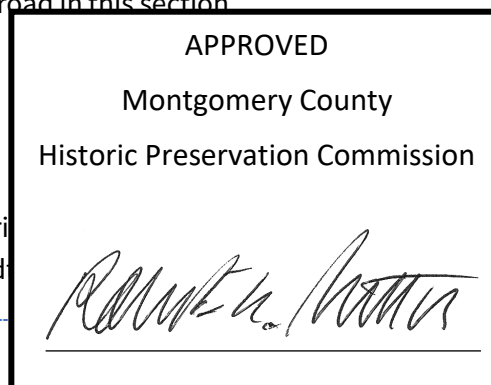
SHOULDER DESIGN

Doctor Bird Road and Norwood Road do not currently have paved shoulders, and short portions of the corridor are curbed (typically near new intersections and recently approved driveways). Along Norwood Road, the design will be adding a four-foot grass shoulder on the west side between the roadway and a drainage swale. If a shoulder is provided on a Country Connector, six feet is the minimum shoulder width identified in the CSDG. There is some concern with the east side of Norwood Road and the proximity of utility poles very close to the road edge. This is more of a Vision Zero concern along this corridor than the lack of shoulders. The project will not significantly change shoulder conditions along the east side of the road in this section.

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

The buffers proposed in this design are primarily... This is consistent with the minimum buffer width...



project limits.

ENVIRONMENTAL REVIEW

ENVIRONMENTAL GUIDELINES

The project area does not contain any environmental buffers, streams, other sensitive features. The project area does contain 0.07 acres of forest. The project is within the Northwest Branch watershed

and partially within the Hawlings River Watershed, both of which have a Use Class IV¹ (recreational trout waters) designation.

The project proposes 0.07 acres of forest removal and has no impacts on a stream valley buffer, wetlands, or the 100-Year-Floodplain. The environmental impacts are necessary and unavoidable to achieve the design standards of creating the new pathway, and those impacts have been minimized to the greatest extent possible.

FOREST CONSERVATION

The project is exempt from submission of a forest conservation plan. A forest conservation exemption (#42021057E) was granted under the provisions of Section 22A-5(e) as “a state or county highway project.” The exemption was confirmed on February 27, 2015.

While the project is exempt, the Applicant is still required under section 22A-9 of the County Code to:

- a) Minimize forest cutting, clearing, and loss of specimen trees to the extent possible while balancing other design, construction, and environmental standards. The constructing agency must make a reasonable effort to minimize the cutting or clearing of trees and other woody plants.
- b) If the forest to be cut or cleared for a county highway project equals or exceeds 20,000 square feet, the constructing agency must reforest a suitable area at the rate of one acre of reforestation for each acre of forest cleared.

The Applicant has minimized the limits of disturbance, minimizing impacts to large and specimen trees. However, the project still results in the loss of specimen or champion trees. Mitigation measures include highlighting the size and character of the tree. The Applicant has provided a plan to highlight forest tree mitigation.

Forest loss/mitigation

Even with minimizing the limits of disturbance and altering some design aspects there are some necessary and unavoidable impacts to forest. The project proposes to remove 0.07 acres (~3,050 square feet) of forest which is below the forest clearing threshold of .46 acres (20,000 square feet) allowed under the exemption. Therefore, the Applicant is not required to provide reforestation.

¹ Environmental Guidelines: Guidelines for Environmental Management of Development in Montgomery County, 2021, page 58.

REVIEWED
By Chris Berger at 11:39 am, Mar 21, 2024

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Tree Save

The Applicant has submitted a tree save plan in conjunction with the Mandatory Referral process to show how the impacts have been minimized and what stress reduction methods are being used to further minimize impacts on trees. Three specimen trees are being removed by this project.

PARKLAND IMPACTS

The proposed sidepath project along Norwood Road (MD182) will impact approximately 64,590 SF square feet of parkland at the frontage of Woodlawn Manor Cultural Park on the east side of Norwood Road. In addition, there are approximately 22,180 SF of proposed parkland impacts, at the Red Door Store Cultural Park on the west side of Norwood Road to accommodate stormwater management swales (Figure 9). There are no parkland impacts associated with the approximately 1,400-foot-long section along Doctor Bird Road starting at MD 108 (Olney Sandy Spring Road) and traveling south towards Norwood Road.

Woodlawn Manor Cultural Park is an 80-acre park dedicated to the preservation and interpretation of cultural resources. Woodlawn Manor is part of the National Park Service’s National Underground Railroad Network to Freedom and is an active educational site serving thousands of children and families each year. The two main functions of Woodlawn Manor include the museum and Park Police operations:

- 1) The museum located in the \$3.5 million renovated barn project, historic house, and Underground Railroad Experience Trail functions to tell the story of the Underground Railroad, Quakers, enslaved and Free African Americans of Sandy Spring and Montgomery County.
- 2) Park Police Special Operations have their headquarters at the park, where they prepare for emergency operations as well as stable and pasture their horses in barns and within electrified fencing.

The park is located in the Northwest Branch watershed and in the North Olney Manor Management Region.

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MCDOT Heritage Triangle Trail-Phase 1 at Woodlawn Manor Cultural Park

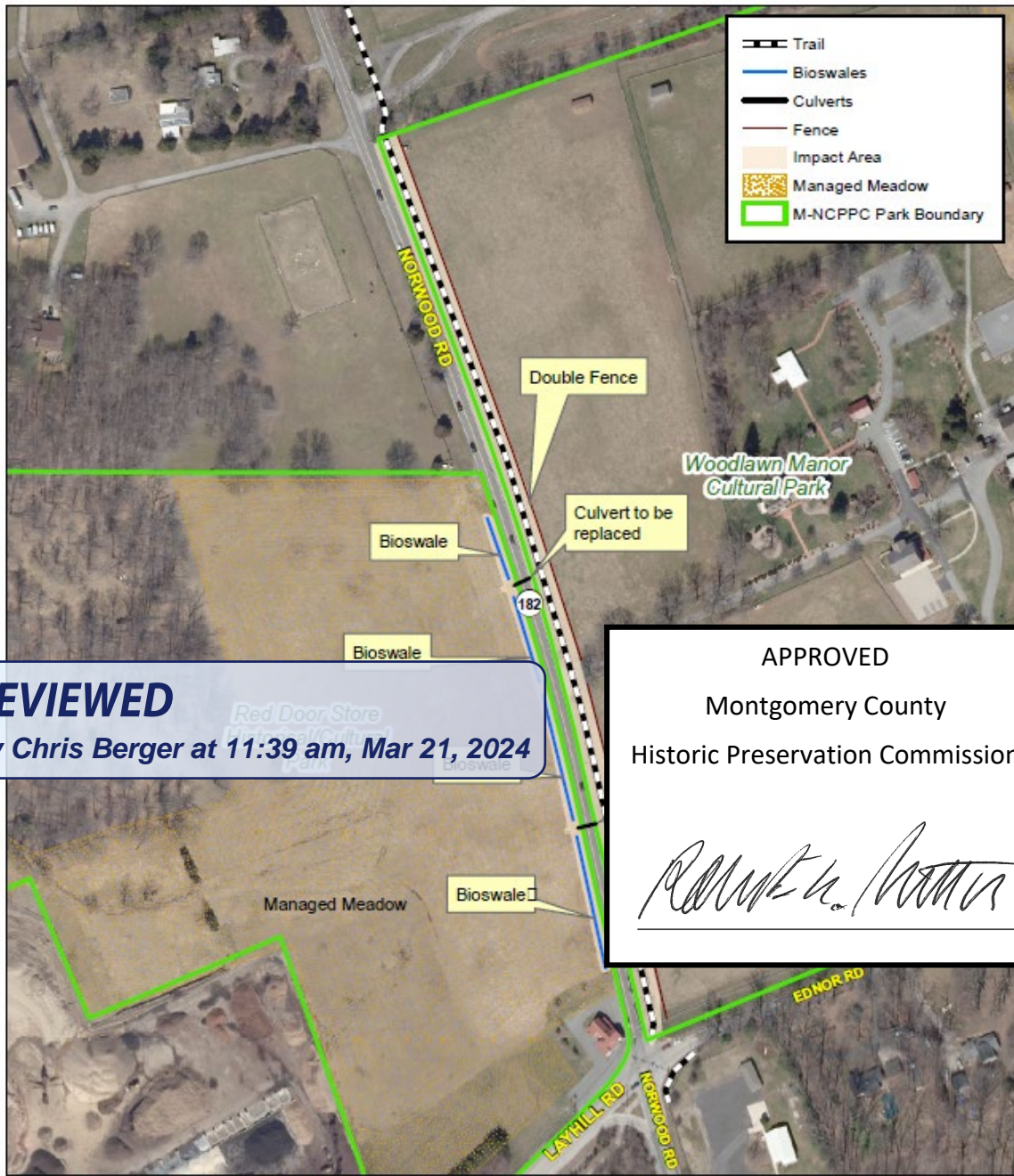


Figure 9 – Project Impacts to Parkland

Montgomery Parks supports the project’s goal to improve pedestrian and cyclist connectivity to these important cultural resources and is working with MCDOT to reduce the impacts to parkland where feasible and provide appropriate mitigation for unavoidable impacts. The 10-foot-wide sidepath connector proposed on the east side of Norwood Road along Woodlawn Manor Cultural Park, includes a variable 10- to 15-foot-wide grass buffer between the path and the roadway (see Figure 7). The pasture will be reduced by 22 to 30 feet along the approximately 1,600-foot length on park property.

LIGHTING

Montgomery Parks supports pedestrian lighting (as described in Section 2) for the Heritage Triangle Trail as long as all sections of the path are lit to prevent a discontinuity of lighting along the trail. MCDOT will be responsible for the maintenance of the lighting posts and fixtures. Since there is currently no existing path lighting on the sections adjacent to park property Parks is requiring that conduit only be installed until lighting for the adjacent trail sections is funded. Additional coordination on lighting and minimizing impacts from lighting in this section will be required and included in the Memorandum of Understanding (MOU) and Park Construction Permit.

CULTURAL RESOURCES

Due to the historical context of this site, MCDOT has been in coordination with the Maryland Historical Trust, the Maryland State Historic Preservation Office (MHT) and the Montgomery County Historic Preservation Commission (HPC). An archaeological investigation was performed along the length of the proposed trail. The investigation identified a very low-density field site. Further investigations will be required and that the project will have a significant impact on the site. MCDOT is continuing to coordinate with MHT and the Montgomery County Historic Preservation Commission (HP) staff and is pursuing Historic Area Work Permits for Woodlawn Manor. The MHT advised that “MCDOT should exercise caution during construction to ensure that project construction activities, equipment, and materials do not impact Woodlawn Manor property outside the Area of Potential Effects (APE).” Additionally, any archeological findings uncovered during construction shall be shared with Parks Cultural Resources Staff.

NATURAL RESOURCES

The proposed trail runs across the road frontage and the Woodlawn site and will impact landscape plantings including trees and shrubs. Nine red cedars (not specimen trees) on parkland at the intersection of Ednor Road and Norwood Road will be removed as part of the project (see Figure 10). At the entrance to Woodlawn Manor two large shrubs and one bur oak (not a specimen tree) will need to be removed to improve sightlines (see Figure 11). Parks and MCDOT will work collaboratively to fulfill Parks tree mitigation requirements through either the planting of new trees, mitigation fee payment, or other natural resource mitigation. Parks Urban Forester will provide guidance during the

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Historic Preservation Commission


Park Construction Permit review to avoid and minimize impacts to the critical root zones to the large oaks lining the entrance to the park.

On the west side of Norwood Road, is the Red Door Store Cultural Park, an approximately 80-acre park which houses the Red Door Store, a distinctive historical example of a mid-19th century rural crossroads store. In 2018, architectural, engineering and market studies were undertaken to better understand the level of effort required to rehabilitate and activate the structure. The Red Door Store Cultural Park contains an approximately 58-acre meadow management site. Meadow areas at the site are primarily composed of old field non-native species, with some native grasses and wildflowers



Figure 10 - Red Cedars to be removed adjacent to Woodlawn Manor Cultural Park

present. Active management currently includes annual mowing to keep the site open and dominated by herbaceous species. It is currently identified as an important area for open habitat species within the Montgomery Parks system. The meadow and adjacent forested area slope down to Batchellor's Run, a restored tributary of Northwest Branch.

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By Chris Berger at 11:39 am, Mar 21, 2024

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Figure 11 – Entrance to Woodlawn Manor Cultural Park

STORMWATER MANAGEMENT

Four bioswales are proposed along the west side of Norwood Road within the Red Door Store Cultural Park to provide required stormwater management for the project. Parks has committed to maintaining the stormwater facilities built by MCDOT on parkland per the agreement with the Montgomery County Department of Environmental Protection. Two SHA culverts will be replaced under Norwood Rd which currently do not function causing flooding on the roadway. Both culverts outfall into the meadow area on the west side of Norwood Rd. Parks will require MCDOT to construct stable and environmentally sensitive outfall structures that avoid erosion and channelization in the meadow.

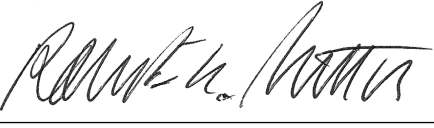
SAFETY AND SECURITY

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By Chris Berger at 11:39 am, Mar 21, 2024

Montgomery Parks looks forward to the improvement of the park, however the new path does present some safety concerns. The proposed path view noted concerns with the vehicular traffic utilizing the path. Additional safety measures should be taken to improve the safety of the path. To reduce thru traffic, a marked crossing and detected crossing are needed. Other appropriate measures identified during the project review. Woodlawn will need to be relocated and replaced.

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Woodlawn
 Figure 12
 Parks Staff have
 additional
 signage to
 cyclists, and
 the entrance to

Along the length of the east side of the path, at the request of Park Police, a double fence separated by a 10-foot width is proposed to separate the equestrian pasture from the path for the safety of both the

horses and path users. The fence must match the existing fence in design and materials. In addition, due to increased access and activity that the path will provide around the Park Police headquarters and equestrian facilities MCDOT will also provide four security cameras along the frontage of the Woodlawn Property. Finally, MCDOT will provide a trail counter along this section of the path to Parks' specification to allow for user data tracking and analysis.

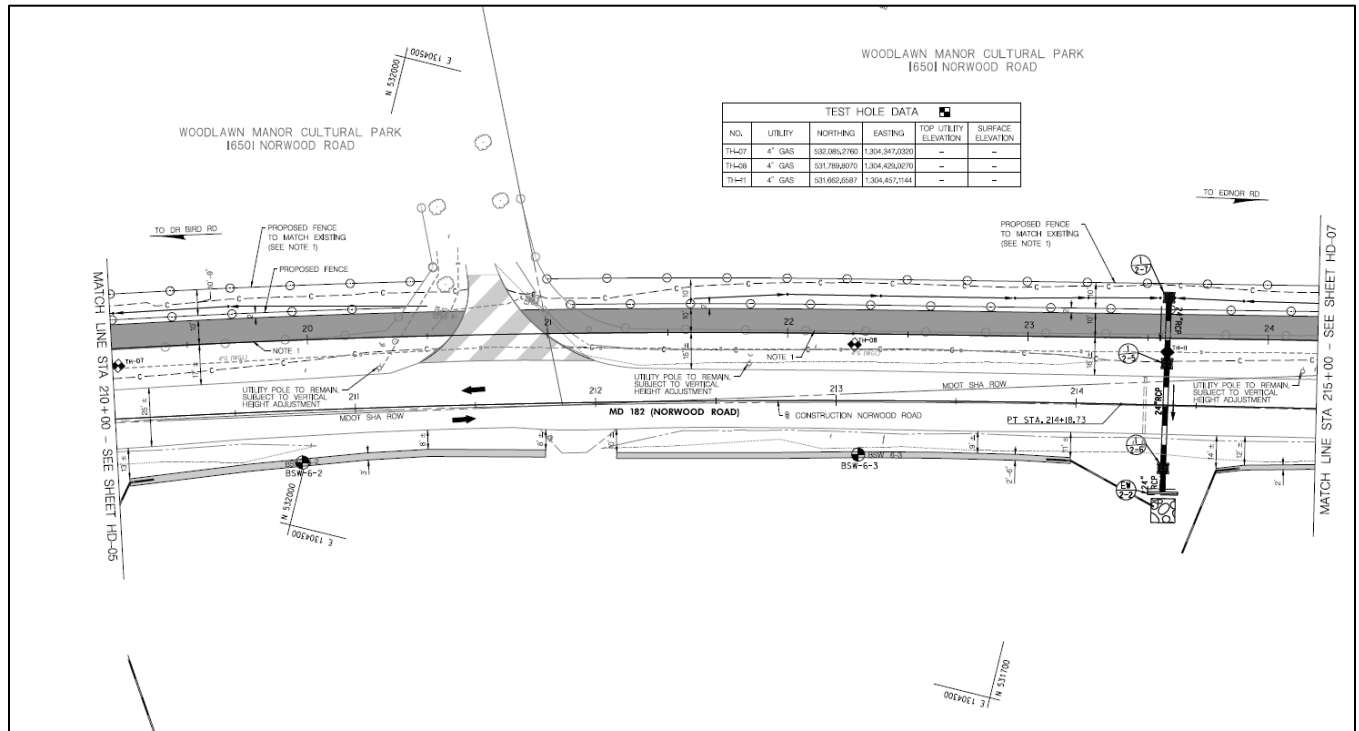


Figure 12 – Plan view of the proposed Sidepath at the Entrance of Woodlawn Manor Cultural Park

PARK CONSTRUCTION PERMIT AND MOU

Parks and MCDOT have agreed that Parks will maintain ownership over the land and no easements will be granted. A Memorandum of Understanding (MOU) will outline the long-term maintenance agreements for each agency.

MCDOT will be required to obtain a Park Construction Permit prior to commencement of any construction activity. Construction Permit review must include existing site topography, trees (with size and species) larger than 6" diameter at breast height of the proposed limit of disturbance on park property. Mitigation for trees (DBH or greater) damaged or removed, shall either be (1) replacement of one inch to one inch diameter or (2) a monetary per inch calculation, to be paid to Montgomery Parks prior to completion of construction determined by an M-NCPPC forester prior to construction based on the Final Design. During Park

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[Signature]

Construction Permit Review, Parks Staff will work with MCDOT to minimize impacts to parkland to the greatest extent possible and avoid all critical resources identified.

HISTORIC PRESERVATION

MCDOT's consultant completed detailed cultural resources analysis and submitted a Phase 1 Cultural Resources Report to the Maryland Historic Trust (MHT) in June 2021. MHT responded in August 2021 and determined that no additional Phase 2 archaeological investigations will be required and that the project will have no adverse effect on historic properties. The Applicant is continuing to coordinate with MHT and Montgomery Planning's Historic Preservation (HP) staff as the design progresses forward. Per coordination with HP staff, the Applicant has initiated the submission of draft Historic Area Work Permits (HAWP) via the online portal for the Red Door Store and Woodlawn properties.

The Doctor Bird/Norwood Road sidepath project is adjacent to several sites on the Master Plan for Historic Preservation including Woodlawn (28/14), the Holland Store and House/Red Door Store (23-119), and the Doctor Bird House (23/121). The project limits of disturbance will not encroach on the Doctor Bird House master plan site; but impacts both the Woodlawn and the Holland Store and House/Red Door Store sites, so a HAWP is required before the work begins.

Historic Preservation Staff supports the project and is not concerned about the potential impacts the work will have on the designated sites. The sidepath, lighting, and relocated fencing adjacent to Woodlawn will not significantly impact the historic character of the site's environmental setting. Additionally, HP Staff finds the re-grading necessary for the bioswale and culverts installed on the Holland Store and House/Red Door Store site will only minimally impact the visual character of the site.

A Phase 1 archaeological survey was completed under the MHT's oversight. MHT determined that no further archaeology in the project area was warranted.

Historic Preservation Staff encourages the Applicant to apply for a HAWP around the 70% design phase or at their earliest convenience.

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MASTER PLAN CONFORMANCE

The Heritage Triangle Trail – Phase 1 Sidepath is subject to three master plans: the 1998 Ashton Sandy Spring Master Plan, the 2018 Bicycle Master Plan and the 2018 Master Plan of Highways and Transitways.

The 2018 Bicycle Master Plan includes the following recommendation for MacArthur Boulevard:

BIKEWAY	FROM	TO	FACILITY TYPE	BIKEWAY TYPE
Doctor Bird Road (MD 182)	Olney Sandy Spring Road (MD 108)	Norwood Road	Separated Bikeway	Sidepath
Norwood Road (MD 182)	Doctor Bird Road (MD 182)	Layhill Road/Ednor Road	Separated Bikeway	Sidepath

The 2018 Master Plan of Highways and Transitways and the 1998 Ashton Sandy Spring Master Plan classifies Doctor Bird Road/Norwood Road (MD 182) as a two-lane major highway with a center turn lane. Per the Complete Streets Design Guide, these roads would be classified as Major Roadways.

REVIEWED

By Chris Berger at 11:39 am, Mar 21, 2024

The project is in conformance with the three plans it is subject to: the 1998 Ashton Sandy Spring Master Plan, the 2018 Bicycle Master Plan and the 2018 Master Plan of Highways and Transitways.



SECTION 4: COMMUNITY ENGAGEMENT

A newsletter was mailed to residents within the vicinity of the project in February 2021. A virtual public meeting was held on February 25, 2021. A link to the project website is below:

<https://www.montgomerycountymd.gov/dot-dte/projects/heritageTTph1/index.html>

The project team is continuing to coordinate with all project stakeholders as the design is finalized.

SECTION 5: CONCLUSION

Based on information provided by the Applicant, Planning Staff recommends approval of the Mandatory Referral with comments listed on pages 3 and 4 of this report to be transmitted to the Montgomery County Department of Transportation. The Applicant is requested to reply in writing to the Planning Board, addressing all comments.

ATTACHMENTS

Attachment A: 35 Percent Design Plans