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Email: [stsgc@aol.com](mailto:stsgc@aol.com)

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Smith, Thomas & Smith, Inc.

*General Contractors*

JOHN R. GUBISCH, III  
VICE PRESIDENT

4713 MAPLE AVE.  
BETHESDA, MD 20814



## HISTORIC PRESERVATION COMMISSION


Isiah Leggett  
County Executive

Leslie Miles  
Chairperson

Date: February 28, 2012

### MEMORANDUM

TO: Diane R. Schwartz Jones, Director  
Department of Permitting Services

FROM: Josh Silver, Senior Planner   
Historic Preservation Section  
Maryland-National Capital Park & Planning Commission

SUBJECT: Historic Area Work Permit #591960, partial demolition of garage and new garage construction

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The Montgomery County Historic Preservation Commission (HPC) has reviewed the attached application for a Historic Area Work Permit (HAWP). This application was **approved** at the February 22, 2012 meeting.

The HPC staff has reviewed and stamped the attached construction drawings.

THE BUILDING PERMIT FOR THIS PROJECT SHALL BE ISSUED CONDITIONAL UPON ADHERENCE TO THE ABOVE APPROVED HAWP CONDITIONS AND MAY REQUIRE APPROVAL BY DPS OR ANOTHER LOCAL OFFICE BEFORE WORK CAN BEGIN.

Applicant: W. Reid Thompson

Address: 14 West Kirke Street, Chevy Chase

This HAWP approval is subject to the general condition that the applicant will obtain all other applicable Montgomery County or local government agency permits. After the issuance of these permits, the applicant must contact this Historic Preservation Office if any changes to the approved plan are made. Once the work is complete the applicant will contact the staff person assigned to this application at 301-563-3400 or [joshua.silver@mncppe-mc.org](mailto:joshua.silver@mncppe-mc.org) to schedule a follow-up site visit.





HISTORIC PRESERVATION COMMISSION  
301/563-3400

# APPLICATION FOR HISTORIC AREA WORK PERMIT

Contact Email: stsgc@cd.com Contact Person: DAVID ALESSANDRINI  
 Daytime Phone No.: 301.656.0141  
 Tax Account No.: 00496691  
 Name of Property Owner: W. REID THOMPSON Daytime Phone No.: 301.986.1290  
 Address: 14 N. KIRKE STREET CHEVY CHASE MD 20815  
Street Number City State Zip Code  
 Contractor: SMITH, THOMAS & SMITH INC. Phone No.: 301.656.0141  
 Contractor Registration No.: 36291  
 Agent for Owner: DAVID ALESSANDRINI Daytime Phone No.: 301.656.0141

**LOCATION OF BUILDING/PREMISE**

House Number: 14 Street: W. KIRKE ST  
 Town/City: CHEVY CHASE Nearest Cross Street: MAGNOLIA PARKWAY  
 Lot: 14 Block: 33 Subdivision: CHEVY CHASE VILLAGE  
 Liber: \_\_\_\_\_ Folio: \_\_\_\_\_ Parcel: \_\_\_\_\_

**PART ONE: TYPE OF PERMITS ACTION AND USE**

**1A. CHECK ALL APPLICABLE:**

- Construct  Extend  Alter/Renovate
- Move  Install  Wreck/Raze
- Revision  Repair  Revocable.

**CHECK ALL APPLICABLE:**

- A/C  Stab  Room Addition  Porch  Deck  Shed
- Solar  Fireplace  Woodburning Stove  Single Family
- Fence/Wall (complete Section 4)  Other: CARRIAGE HOUSE

1B. Construction cost estimate: \$ 60,000  
 1C. If this is a revision of a previously approved active permit, see Permit # \_\_\_\_\_

**PART TWO: COMPLETE FOR NEW CONSTRUCTION AND EXTEND/ADDITIONS**

2A. Type of sewage disposal: 01  WSSC 02  Septic 03  Other: \_\_\_\_\_  
 2B. Type of water supply: 01  WSSC 02  Well 03  Other: \_\_\_\_\_

**PART THREE: COMPLETE ONLY FOR FENCE/RETAINING WALL**

3A. Height \_\_\_\_\_ feet \_\_\_\_\_ inches  
 3B. Indicate whether the fence or retaining wall is to be constructed on one of the following locations:  
 On party line/property line  Entirely on land of owner  On public right of way/ easement

*I hereby certify that I have the authority to make the foregoing application, that the application is correct, and that the construction will comply with plans approved by all agencies listed and I hereby acknowledge and accept this to be a condition for the issuance of this permit.*

[Signature] \_\_\_\_\_ 1/12/12 \_\_\_\_\_  
Signature of owner or authorized agent Date

Approved: [Signature] \_\_\_\_\_ For Chairperson, Historic Preservation Commission  
 Disapproved: \_\_\_\_\_ Signature: \_\_\_\_\_ Date: 2/28/12  
 Application/Permit No.: 591960 Date Filed: 2/24/12 Date Issued: \_\_\_\_\_

**THE FOLLOWING ITEMS MUST BE COMPLETED AND THE  
REQUIRED DOCUMENTS MUST ACCOMPANY THIS APPLICATION.**

**1. WRITTEN DESCRIPTION OF PROJECT**

- a. Description of existing structure(s) and environmental setting, including their historical features and significance:

THE EXISTING GARAGE STRUCTURE IS LOCATED AT  
THE RIGHT REAR OF THE LOT, BEHIND THE  
HOUSE. ACCESS IS GAINED BY THE EXISTING  
CONCRETE DRIVEWAY. THE GARAGE IS FINISHED  
IN 4" STEEL SIDING AND ASPHALT SHINGLES, AND  
IS KEEPING IN DESIGN/STYLE WITH THE MAIN HOUSE.

- b. General description of project and its effect on the historic resource(s), the environmental setting, and, where applicable, the historic district:

NEW CARRIAGE HOUSE TO MATCH THE LOOK OF THE  
EXISTING GARAGE, INCLUDING THE "USE OF THE  
EXISTING GARAGE DOORS AND FRONT TRIM.  
CARRIAGE HOUSE TO BE SHEATHED WITH 4" HARDIE  
PLANK SIDING.

**2. SITE PLAN**

Site and environmental setting, drawn to scale. You may use your plot. Your site plan must include:

- the scale, north arrow, and date;
- dimensions of all existing and proposed structures; and
- site features such as walkways, driveways, fences, ponds, streams, trash dumpsters, mechanical equipment, and landscaping.

**3. PLANS AND ELEVATIONS**

You must submit 2 copies of plans and elevations in a format no larger than 11" x 17". Plans on 8 1/2" x 11" paper are preferred.

- Schematic construction plans, with marked dimensions, indicating location, size and general type of walls, window and door openings, and other fixed features of both the existing resource(s) and the proposed work.
- Elevations (facades), with marked dimensions, clearly indicating proposed work in relation to existing construction and, when appropriate, context. All materials and fixtures proposed for the exterior must be noted on the elevations drawings. An existing and a proposed elevation drawing of each facade affected by the proposed work is required.

**4. MATERIALS SPECIFICATIONS**

General description of materials and manufactured items proposed for incorporation in the work of the project. This information may be included on your design drawings.

**5. PHOTOGRAPHS**

- Clearly labeled photographic prints of each facade of existing resource, including details of the affected portions. All labels should be placed on the front of photographs.
- Clearly label photographic prints of the resource as viewed from the public right-of-way and of the adjoining properties. All labels should be placed on the front of photographs.

**6. TREE SURVEY**

If you are proposing construction adjacent to or within the dripline of any tree 6" or larger in diameter (at approximately 4 feet above the ground), you must file an accurate tree survey identifying the size, location, and species of each tree of at least that dimension.

**7. ADDRESSES OF ADJACENT AND CONFRONTING PROPERTY OWNERS**

For ALL projects, provide an accurate list of adjacent and confronting property owners (not tenants), including names, addresses, and zip codes. This list should include the owners of all lots or parcels which adjoin the parcel in question, as well as the owner(s) of lot(s) or parcel(s) which lie directly across the street/highway from the parcel in question. You can obtain this information from the Department of Assessments and Taxation, 81 Monroe Street, Rockville, (301/279-1355).

PLEASE PRINT (IN BLUE OR BLACK INK) OR TYPE THIS INFORMATION ON THE FOLLOWING PAGE.  
PLEASE STAY WITHIN THE GUIDES OF THE TEMPLATE, AS THIS WILL BE PHOTOCOPIED DIRECTLY ONTO MAILING LABELS.

**MONTGOMERY COUNTY HISTORIC PRESERVATION COMMISSION**  
**STAFF REPORT**

<b>Address:</b>	14 West Kirke Street, Chevy Chase	<b>Meeting Date:</b>	2/22/2012
<b>Resource:</b>	Outstanding Resource (House) Chevy Chase Village Historic District	<b>Report Date:</b>	2/15/2012
<b>Applicant:</b>	W. Reid Thompson (David Alessandrini, Architect)	<b>Public Notice:</b>	2/8/2012
<b>Review:</b>	HAWP	<b>Tax Credit:</b>	None
<b>Case Number:</b>	35/13-12E	<b>Staff:</b>	Josh Silver
<b>PROPOSAL:</b>	Partial demolition of garage and new construction		

**STAFF RECOMMENDATION**

Staff recommends that the HPC **approve** this HAWP application.

**ARCHITECTURAL DESCRIPTION**

**SIGNIFICANCE:** Outstanding Resource within the Chevy Chase Village Historic District  
**STYLE:** Queen Anne  
**DATE:** 1892-1916

**PROPOSAL**

The applicant is proposing to permanently remove the roof, rear and both side walls from an existing garage in the rear yard of the property and construct a new garage in the same location with same dimensions and height. The proposal includes systematically disassembling the front wall (doors/trim/pilasters) for reuse on the proposed new garage and demolition of the existing concrete floor in order to pour new code compliant concrete footers and a floor slab.

The proposed material treatments for the new garage consist of painted fiber cement siding, asphalt shingles, painted wooden, casement or double-hung, 6/6 SDL windows, a wooden SDL multi-light side entry door, and reuse of the existing carriage doors, trim and pilasters on the front elevation.

**APPLICABLE GUIDELINES**

When reviewing alterations within the Chevy Chase Village Historic District several documents are to be utilized as guidelines to assist the Commission in developing their decision. These documents include the *Chevy Chase Village Historic District Master Plan – Expansion*, approved and adopted in August 1997, *Montgomery County Code Chapter 24A (Chapter 24A)* and the *Secretary of the Interior's Standards for Rehabilitation (Standards)*. The pertinent information in these documents is outlined as follows:

*Chevy Chase Village Historic District Master Plan*

The *Guidelines* break down specific projects into three levels of review - Lenient, Moderate and Strict Scrutiny.

**“Lenient Scrutiny”** means that the emphasis of the review should be on issues of general massing and scale, and compatibility with the surrounding streetscape, and should allow for a very liberal interpretation of preservation rules. Most changes should be permitted unless there are major problems with massing, scale or compatibility.

**“Moderate Scrutiny”** involves a higher standard of review than “lenient scrutiny.” Besides issues of massing, scale and compatibility, preserving the integrity of the resource is taken into account. Alterations should be designed so that the altered structure still contributes to the district. Use of compatible new materials, rather than the original building materials, should be permitted. Planned changes should be compatible with the structure’s existing design, but should not be required to replicate its architectural style.

**“Strict Scrutiny”** means that the planned changes should be reviewed to insure that the integrity of the significant exterior architectural or landscaping features and details is not compromised. However, strict scrutiny should not be “strict in theory but fatal in fact” i.e. it does not mean that there can be no changes but simply that the proposed changes should be reviewed with extra care.

The *Guidelines* state three basic policies that should be adhered to, including:

Preserving the integrity of the contributing structures in the district. Alterations to contributing structures should be designed in such a way that the altered structure still contributes to the district.

Design review emphasis should be restricted to changes that will be visible from the front or side public right-of-way, or that would be visible in the absence of vegetation or landscaping.

Alterations to the portion of a property that are not visible from the public right-of-way should be subject to very lenient review. Most changes to rear of the properties should be approved as a matter of course.

The *Guidelines* that pertain to this project are as follows:

***Garages and accessory buildings-*** which are detached from the main house should be subject to lenient scrutiny but should be compatible with the main building.

***Montgomery County Code; Chapter 24A***

- (a) The commission shall instruct the director to deny a permit if it finds, based on the evidence and information presented to or before the commission that the alteration for which the permit is sought would be inappropriate, inconsistent with or detrimental to the preservation, enhancement or ultimate protection of the historic site or historic resource within an historic district, and to the purposes of this chapter.
- (b) The commission shall instruct the director to issue a permit, or issue a permit subject to such conditions as are found to be necessary to insure conformity with the purposes and requirements of this chapter, if it finds that:

- (1) The proposal will not substantially alter the exterior features of an historic site or historic resource within an historic district; or
- (2) The proposal is compatible in character and nature with the historical, archeological, architectural or cultural features of the historic site or the historic district in which an historic resource is located and would not be detrimental thereto or to the achievement of the purposes of this chapter; or
- (3) The proposal would enhance or aid in the protection, preservation and public or private utilization of the historic site or historic resource located within an historic district in a manner compatible with the historical, archeological, architectural or cultural value of the historic site or historic district in which an historic resource is located; or
- (4) The proposal is necessary in order that unsafe conditions or health hazards be remedied; or
- (5) The proposal is necessary in order that the owner of the subject property not be deprived of reasonable use of the property or suffer undue hardship; or
- (6) In balancing the interests of the public in preserving the historic site or historic resource located within an historic district, with the interests of the public from the use and benefit of the alternative proposal, the general public welfare is better served by granting the permit.
  - (c) It is not the intent of this chapter to limit new construction, alteration or repairs to any 1 period or architectural style.
  - (d) In the case of an application for work on an historic resource located within an historic district, the commission shall be lenient in its judgment of plans for structures of little historical or design significance or for plans involving new construction, unless such plans would seriously impair the historic or architectural value of surrounding historic resources or would impair the character of the historic district. (Ord. No. 9-4, § 1; Ord. No. 11-59.)

***Secretary of the Interior's Standards for Rehabilitation:***

- #9 New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.

**STAFF DISCUSSION**

Staff supports the proposed partial demolition of the existing garage and construction of a new garage with the same dimensions and in the same location.

Staff performed a limited visual inspection of the garage interior and exterior and documented the following: (1) multiple cracks in the existing floor slab and no evidence of structural footers; (2) non-historic aluminum siding nailed directly to the wall framing; (3) unconventional side and rear wall framing; and (4) replacement roofing.

The existing garage is documented on the 1927 Sanborn Fire Insurance Map. The primary structure was constructed c1892-1916; as such it is likely that the garage is of an earlier construction date and contributes to the historic district.

However, staff supports the proposed partial demolition given the garage displays evidence of having been altered and that the proposed garage is a compatible reproduction of the existing garage the applicant is proposing to partially demolish.

The Chevy Chase Village Guidelines state **garages and accessory structures** detached from the main house should be subject to **lenient scrutiny** but should be compatible with main building (house).

**“Lenient Scrutiny”** means that the emphasis of the review should be on issues of general massing and scale, and compatibility with the surrounding streetscape, and should allow for a very liberal interpretation of preservation rules. Most changes should be permitted unless there are major problems with massing, scale or compatibility.

The massing, scale and dimensions of the proposed garage are identical to the existing garage and therefore a subordinate relationship of the garage to the house will be maintained while still allowing for the garage to contribute to the historic district. The proposed reuse of the existing carriage doors on the front elevation of the new garage will preserve the character of the garage, as such the proposed work will have negligible impact on the streetscape of the historic district.

Staff finds that in applying *Lenient Scrutiny*, the applicant’s proposal to partially demolish the existing garage and construct a new garage as outlined in the proposal section is consistent with the *Guidelines*.

### **STAFF RECOMMENDATION**

Staff recommends that the Commission **approve the HAWP application** as being consistent with Chapter 24A-8(b) (1) & (2);

1. The proposal will not substantially alter the exterior features of an historic site or historic resource within an historic district; or
2. The proposal is compatible in character and nature with the historical, archeological, architectural or cultural features of the historic site or the historic district in which an historic resource is located and would not be detrimental thereto or to the achievement of the purposes of this chapter; or

and with the *Secretary of the Interior’s Standards for Rehabilitation*;

and with the general condition that the applicant shall present the **3 permit sets of drawings, if applicable, to Historic Preservation Commission (HPC) staff for review and stamping** prior to submission for the Montgomery County Department of Permitting Services (DPS) building permits;

and with the general condition that the applicant shall notify the Historic Preservation Staff if they propose to make **any alterations** to the approved plans. Once the work is completed the applicant will contact the staff person assigned to this application at 301.563.3400 or [joshua.silver@mncppc-mc.org](mailto:joshua.silver@mncppc-mc.org) to schedule a follow-up site visit.





HISTORIC PRESERVATION COMMISSION
301/563-3400

APPLICATION FOR HISTORIC AREA WORK PERMIT

Contact Email: sfsgc@aol.com Contact Person: DAVID ALESSANDRINI
Tax Account No.: 00456651 Daytime Phone No.: 301-656-0141
Name of Property Owner: W. REID THOMPSON Daytime Phone No.: 301-986-1290
Address: 14 N. KIRKE STREET CHEVY CHASE MD 20815
Contractor: SMITH, THOMAS & SMITH INC. Phone No.: 301-656-0141
Contractor Registration No.: 36291
Agent for Owner: DAVID ALESSANDRINI Daytime Phone No.: 301-656-0141

LOCATION OF BUILDING/PREMISE

House Number: 14 Street: W. KIRKE ST
Town/City: CHEVY CHASE Nearest Cross Street: MAGNOLIA PARKWAY
Lot: 14 Block: 33 Subdivision: CHEVY CHASE VILLAGE
Liber: Folio: Parcel:

PART ONE: TYPE OF PERMIT ACTION AND USE

1A. CHECK ALL APPLICABLE:

- Construct, Extend, Alter/Renovate, Move, Install, Wreck/Raze, Revision, Repair, Revocable.

CHECK ALL APPLICABLE:

- A/C, Stab, Room Addition, Porch, Deck, Shed, Solar, Fireplace, Woodburning Stove, Single Family, Fence/Wall, Other: CARRIAGE HOUSE

1B. Construction cost estimate: \$ 60,000

1C. If this is a revision of a previously approved active permit, see Permit #

PART TWO: COMPLETE FOR NEW CONSTRUCTION AND EXTEND/ADDITIONS

2A. Type of sewage disposal: 01 [X] WSSC 02 [ ] Septic 03 [ ] Other:
2B. Type of water supply: 01 [X] WSSC 02 [ ] Well 03 [ ] Other:

PART THREE: COMPLETE ONLY FOR FENCE/RETAINING WALL

3A. Height feet inches

3B. Indicate whether the fence or retaining wall is to be constructed on one of the following locations:

- On party line/property line, Entirely on land of owner, On public right of way/easement

I hereby certify that I have the authority to make the foregoing application, that the application is correct, and that the construction will comply with plans approved by all agencies listed and I hereby acknowledge and accept this to be a condition for the issuance of this permit.

Signature of owner or authorized agent

1/12/12 Date

Approved: For Chairperson, Historic Preservation Commission
Disapproved: Signature: Date:
Application/Permit No.: Data Filed: Date Issued:

**THE FOLLOWING ITEMS MUST BE COMPLETED AND THE  
REQUIRED DOCUMENTS MUST ACCOMPANY THIS APPLICATION.**

**1. WRITTEN DESCRIPTION OF PROJECT**

- a. Description of existing structure(s) and environmental setting, including their historical features and significance:

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THE RIGHT REAR OF THE LOT, BEHIND THE  
HOUSE. ACCESS IS GAINED BY THE EXISTING  
CONCRETE DRIVEWAY. THE GARAGE IS FINISHED  
IN 4" STEEL SIDING AND ASPHALT SHINGLES, AND  
IS KEEPING IN DESIGN/STYLE WITH THE MAIN HOUSE.

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EXISTING GARAGE, INCLUDING THE USE OF THE  
EXISTING GARAGE DOORS AND FRONT TRIM.  
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# Smith, Thomas & Smith, Inc.

GENERAL CONTRACTORS

MD. LICENSE #453  
D.C. LICENSE #17  
VA. LICENSE #035673

4713 MAPLE AVENUE, BETHESDA, MD 20814  
TELEPHONE: 301-656-0141  
EMAIL: STSGC@AOL.COM  
FAX: 301-656-6705  
www.smiththomasandsmith.com

February 1, 2012

## HISTORIC PRESERVATION COMMISSION SUBMITTAL

Project: Remodeling of existing Carriage House at #14 W. Kirke Street, Chevy Chase, MD (Accessory Building, rear, corner of property)

History: Mr. Reid Thompson, #14 W. Kirke Street, commissioned Smith, Thomas & Smith, Inc. (STS) to remodel the existing rear yard Carriage House from its current, open, garage-like interior to a Guest Suite, including a Bathroom, Bedroom and small Kitchen. The interior space appeared adequately sized to accommodate the improvements. Initially, it was assumed that the existing structure could remain, and with interior partitioning, and plumbing and electrical connections to the Main House, the project was feasible.

Mr. and Mrs. Thompson and STS agreed to save / preserve the front façade entirely, including the six (6) Carriage House doors, pilasters and trim.

New windows would be added to the side and rear walls, wood double hung and casements, similar to windows in the Main House.

The existing metal siding, 4" to-the-weather, would be replaced with Hardie-Plank siding, 4" to-the-weather, matching the "look" of the existing siding.

Several Floor Plan designs were submitted and finally a Plan was decided upon, after which cost estimating began.

Determining the Project construction requirements resulted in the following findings:

### Existing Structural Conditions:

- The existing Carriage House structure had no footing, but sits on a poured concrete slab, not reinforced, which is badly cracked throughout, and has allowed considerable heaving inside and around the perimeter. Building Codes require a continuous concrete footing, and the existing slab would not be "reusable" and would have to be removed. A new concrete footing and reinforced slab would be necessary.
- The existing metal siding (which was to be replaced) was attached directly to the wall framing without any subsiding, simply nailed to the existing wall studs. Building Codes require subsiding (1/2" plyscore plywood) and house wrap / vapor barrier.
- The perimeter wall framing was (very) unconventional, with studs spaced from 30" on center to 60" on center. Building Codes require 16" on center. Also, the bottom plates of these walls are not treated, as required. All existing walls would require substantial reframing to comply with current codes.

- The existing roof and ceiling framing members were undersized and not spaced for proper support per current Building Code requirements. They would both have to be reframed.

All of the above-mentioned findings were discussed with Mr. Thompson and it was decided that STS and the Thompsons would save time and money if the existing structure and broken concrete slab was taken down and rebuilt to current Building Code requirements, under the following design criterion:

- A monolithic footing and concrete slab would be poured, matching the dimensions of the existing Carriage House, per current Building Codes.
- The new structure (Guest Suite) would be designed to match the exact dimensions of the existing Carriage House, including width, length, height, mass and scale. Framing of walls, roof, ceiling, headers and insulation would be per all applicable Building Codes. All exterior trim would match existing trim.
- As decided preliminarily, the front façade (the only wall visible from the public right-of-way) would be reconstructed using the existing, six (6) Carriage House doors, pilasters and trim, fitted into the new front wall with the same dimensions as the existing front wall.
- The windows on the side and rear walls (not visible from the front right-of-way) would be Lincoln, wood double hung or casements (sized and located as shown on Plans), with painted exteriors and interiors, simulated divided lites (muntins) and muntin patterns similar to the Main House windows.

After deciding on the scope of the revised Project, a contract price was agreed upon and STS prepared construction plans for submittal to the Historical Preservation Commission, the Montgomery County Building Permit division and the Chevy Chase Village.

Summary: After our in-depth structural review of the existing conditions, the revised construction scheme and the completion of the Construction Plans / Specifications for the guest Suite, it is hoped STS and the Thompsons will be approved to proceed with this Project as described above.

Respectfully Submitted,



John R. Gubisch, III  
Vice President / Secretary

**HAWP APPLICATION: MAILING ADDRESSES FOR NOTIFYING**  
 [Owner, Owner's Agent, Adjacent and Confronting Property Owners]

<p><b>Owner's mailing address</b></p> <p>W. REID THOMPSON          14 W. KIRKE STREET          CHEVY CHASE, MD 20815</p>	<p><b>Owner's Agent's mailing address</b></p> <p>SMITH, THOMAS &amp; SMITH INC.          4713 MAPLE AVENUE          BETHESDA, MD 20814</p>
<p align="center"><b>Adjacent and confronting Property Owners mailing addresses</b></p>	
<p>JAMES VAP          12 W. KIRKE STREET          CHEVY CHASE, MD 20815</p>	<p>JOSEPH MELROD          11 W. KIRKE STREET          CHEVY CHASE, MD 20815</p>
<p>EPHRAM JACOBS          16 W. KIRKE STREET          CHEVY CHASE, MD 20815</p>	<p>CATHERINE SILVERMAN          15 W. KIRKE STREET          CHEVY CHASE, MD 20815</p>
<p>DAVID BRALOVE          11 W. IRVING STREET          CHEVY CHASE, MD 20815</p>	<p>HENRY A. DUDLEY JR          13 W IRVING STREET          CHEVY CHASE, MD 20815</p>

**NOTES:**

Construction plans to comply with all local codes, including IRC 2010.

Smoke detector system to be hardwired, interconnected smoke detectors, on a dedicated circuit from the panel and with a battery back-up, located in each sleeping room, outside each sleeping area, and each habitable floor.

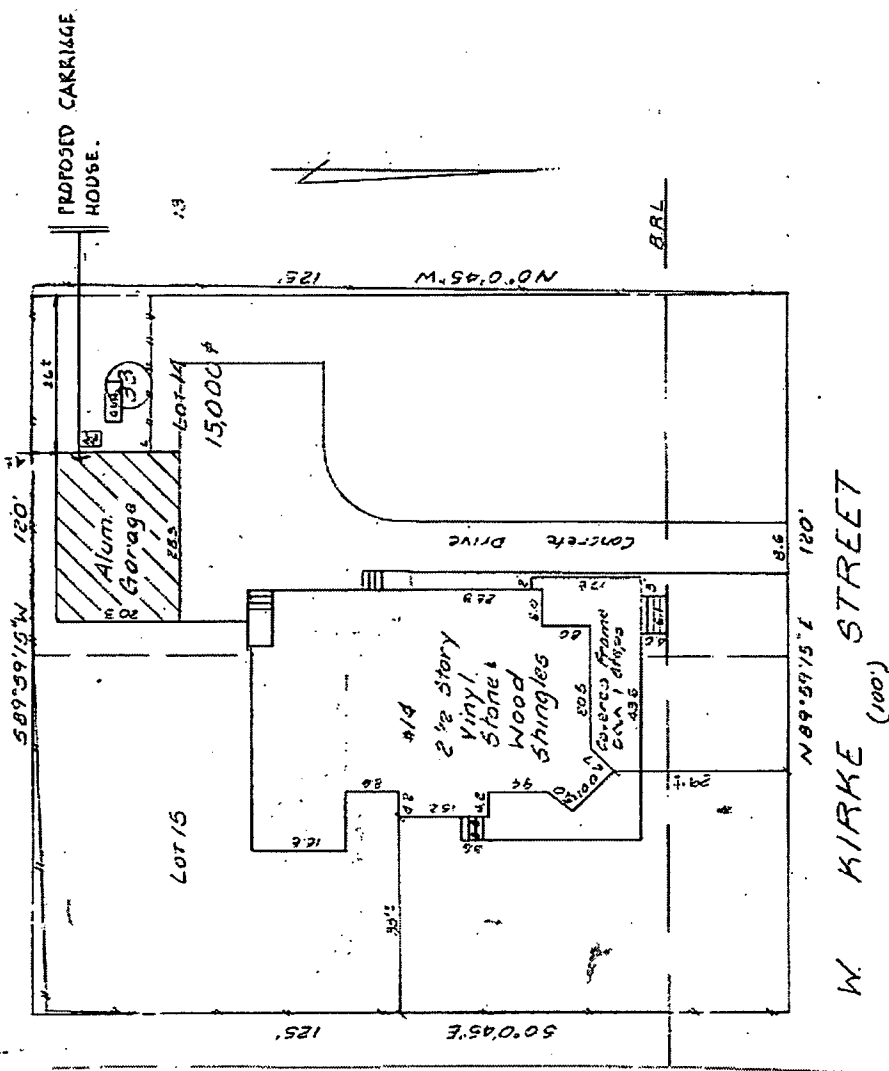
**SIGN CRITERIA:**

- Occupancy live load: 40 psf
- or dead load: 10 psf
- of live load/snow load: 30 psf
- or dead load: 15 psf
- wind load: 90 mph, 3 second gust
- and importance factor:  $IW = 1.0$
- and exposure category: B
- minimum design wind pressure for main wind force resisting system & components/cladding = 20 psf
- seismic importance factor = 1.0
- seismic design category = B
- framing lumber: kiln dried Western SPF
- framing studs: kiln dried Eastern SPF
- concrete: 2,500 psi minimum, no air
- undulation soil: 2,000 psf soil bearing
- all insulation to be 3 1/2" R-15 fiberglass insulation
- ceiling insulation to be 12" R-38 fiberglass insulation
- roof insulation to be 6" R-19 fiberglass insulation
- all windows to be Lincoln, vinyl clad, double hung and casement units,
- with low-E I/Argon insulated glass, U-value = 0.34
- all doors to be Lincoln, vinyl clad, French, with low-E II/Argon insulated glass, U-value = 0.34

**Leakage**

Sealing thermal envelope. The building thermal envelope shall be durably sealed to limit infiltration. The sealing shall be between dissimilar materials shall allow for differential expansion and contraction. The following shall be sealed, gasketed, weatherstripped or otherwise sealed with an air barrier material, suitable film or solid material:

- All joints, corners and penetrations.
- Site-built windows, doors and skylights.
- Openings between window and door assemblies and their respective jambs and framing.
- Utility penetrations.
- Drooped ceilings or chases adjacent to the thermal envelope.
- Knee walls.
- Walls and ceilings separating a garage from conditioned spaces.
- Bulb-in tubs and showers on exterior walls.
- Common walls between dwelling units.
- Attic access openings.
- Rim joist junction.
- Other sources of infiltration.



LOT 14 & 15 BLOCK 33  
 CHEVY CHASE VILLAGE  
 MONTGOMERY COUNTY  
 SCALE: 1" = 20'

**th, Thomas & Smith, Inc.**

Selected Contractor  
 4713 MAPLE AVENUE, BETHESDA, MARYLAND 20814  
 TEL: (301) 656-4141  
 FAX: (301) 656-4141

**THOMPSON RESIDENCE**

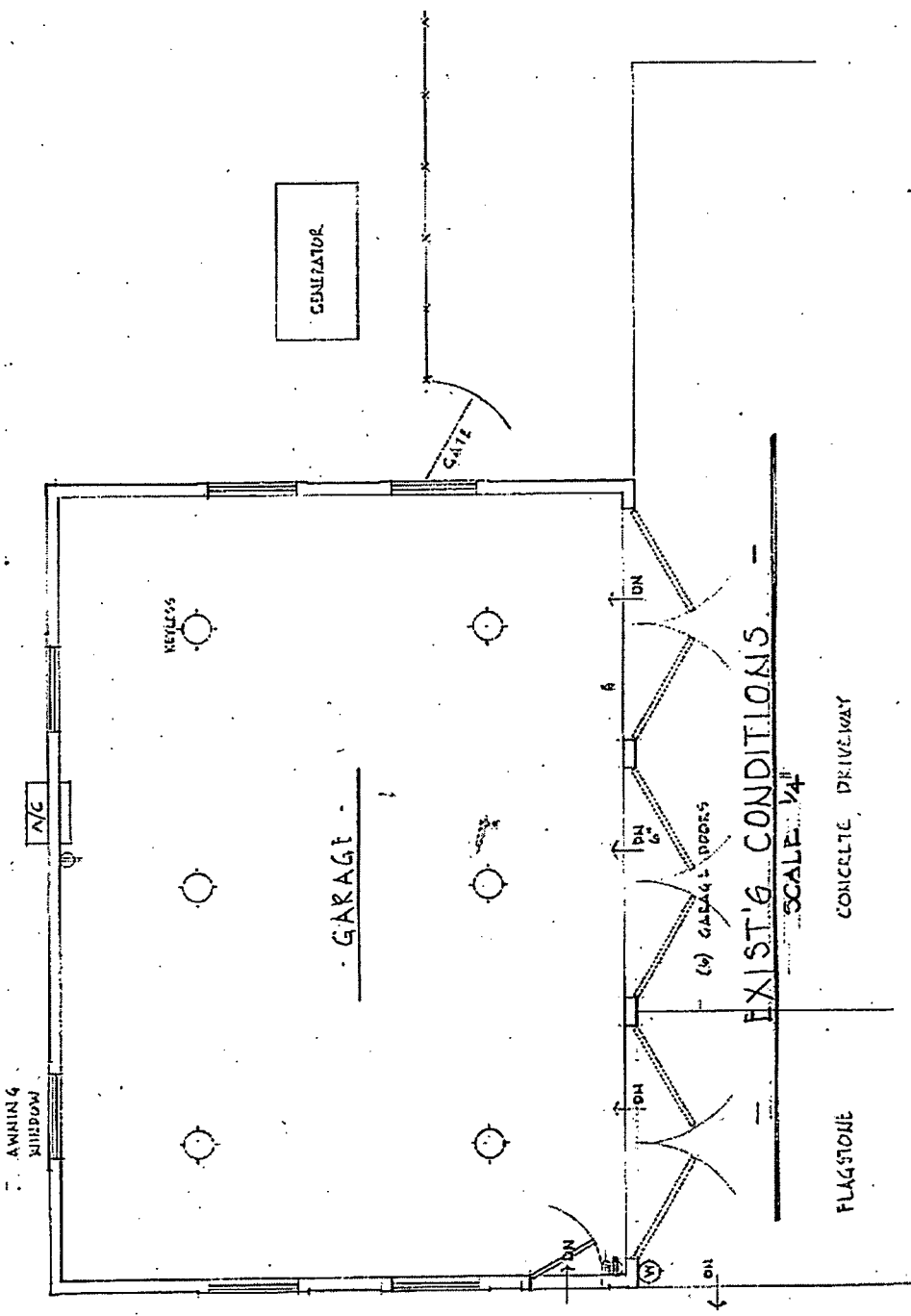
14 W. KIRKE STREET

**CARRIAGE HOUSE**

SCALE: AS NOTED  
 DATE: DEC. 27, 2011



FENCE / REAR PROPERTY LINE



**h, Thomas & Smith, Inc.**  
*General Contractors*  
 4113 MAPLE AVENUE, BETHESDA, MARYLAND 20814  
 TELEPHONE (301) 656-0144  
 M.D. LICENSE # 433  
 D.C. LICENSE # 17  
 V.A. LICENSE # 03579

THOMPSON RESIDENCE

CARRIAGE HOUSE

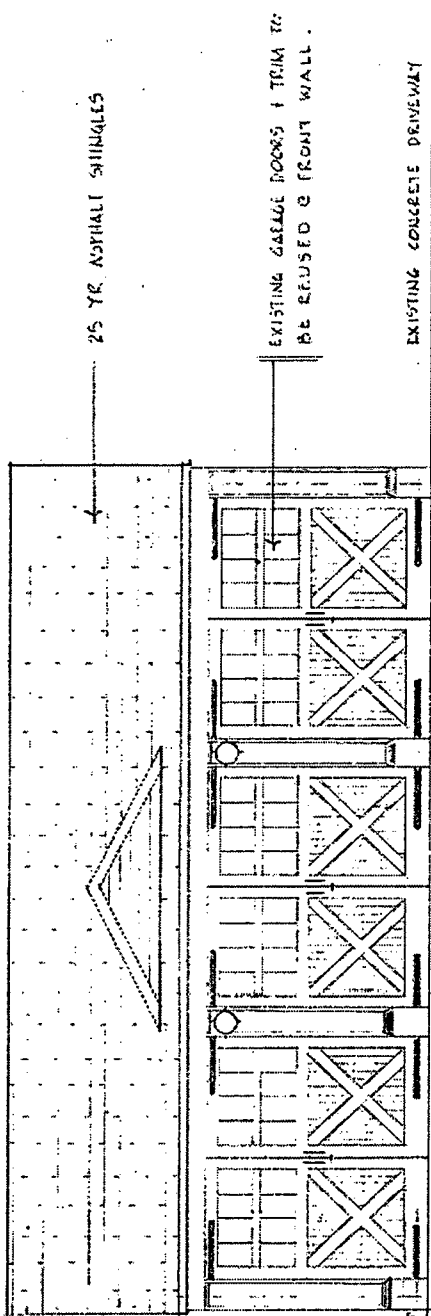
14 W. KIRE STREET, CHEVY CHASE, MD 20815

SCALE: AS NOTED

DATE: DEC. 27, 2011

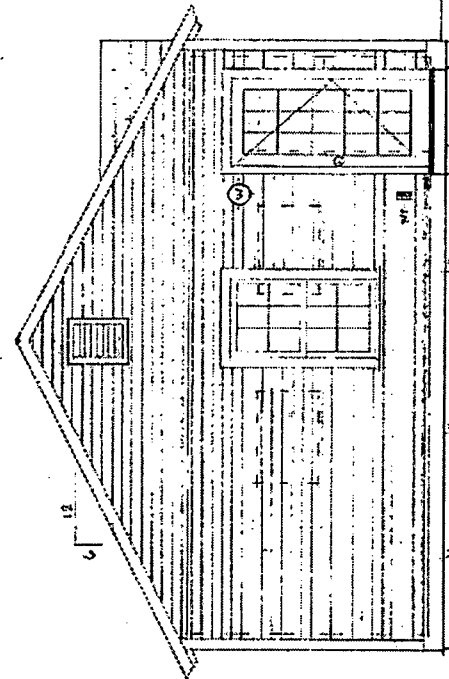




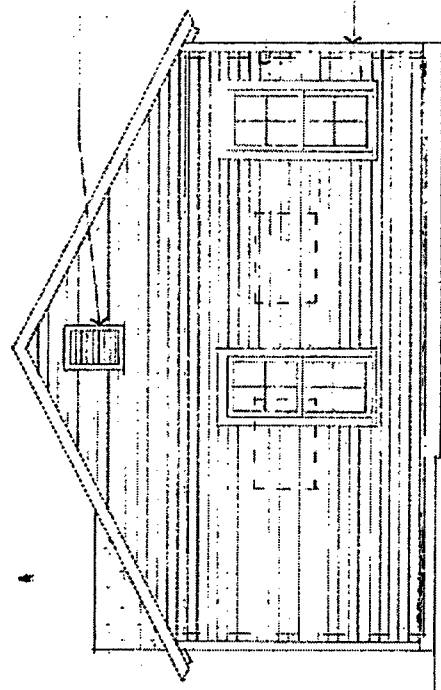


- FRONT ELEVATION -

SCALE: 1/4" = 1'-0"

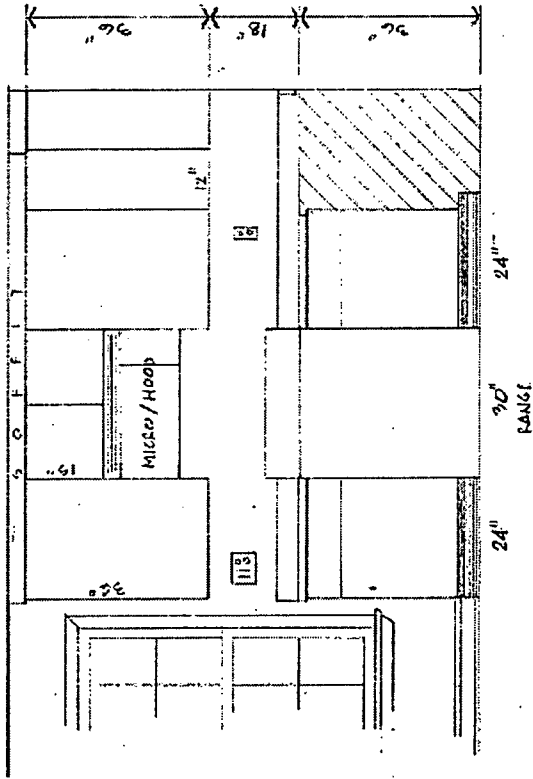


- LEFT SIDE ELEVATION -

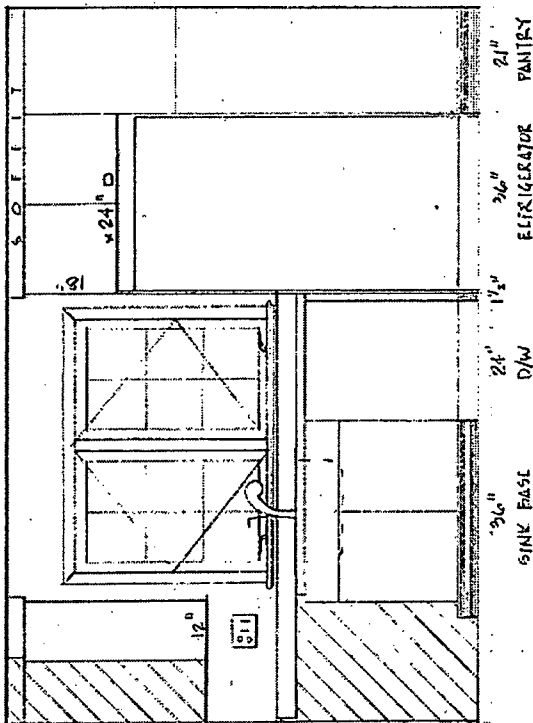


- RIGHT SIDE ELEVATION -



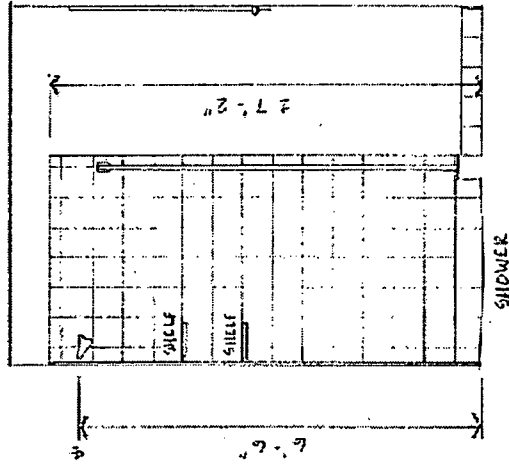


- VIEW B -

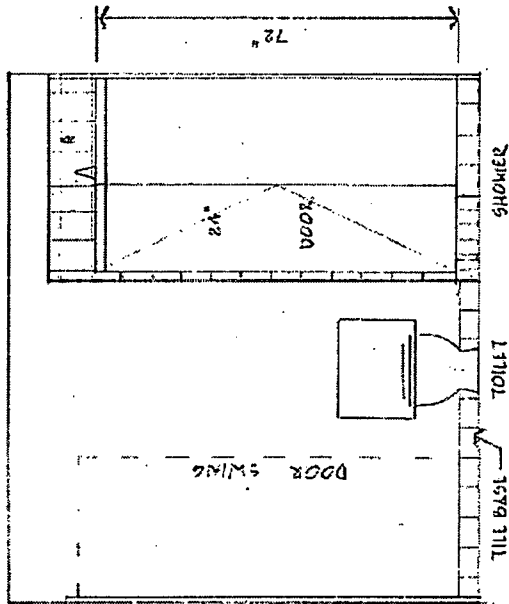


- VIEW A -

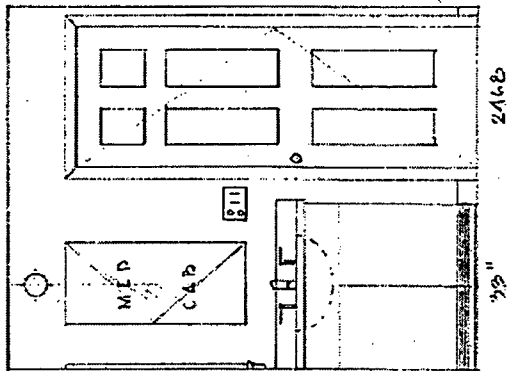
SCALE: 3/4" = 1'-0"



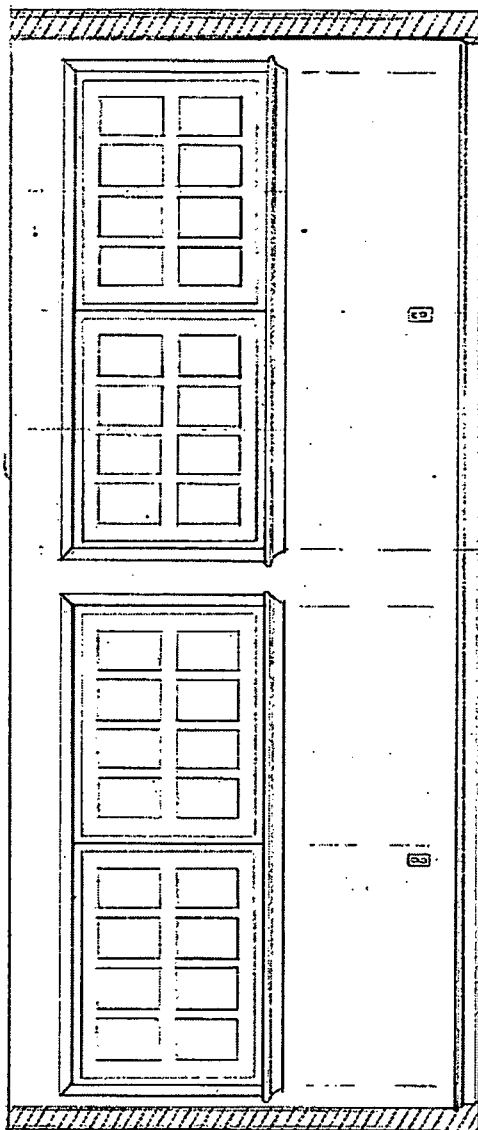
- VIEW E -



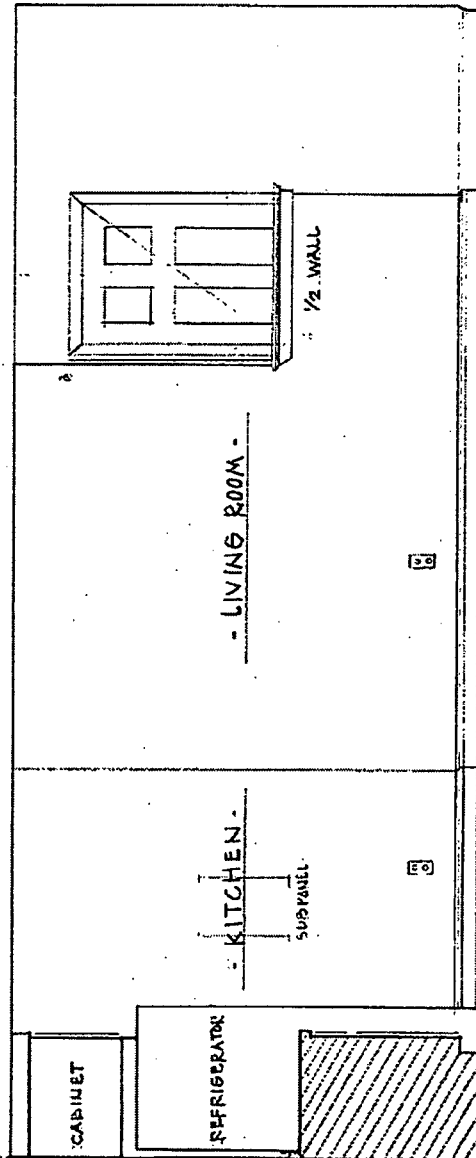
- VIEW D -



- VIEW C -



- INTERIOR GARAGE DOORS -



- LIVING ROOM -

- KITCHEN -

- KITCHEN / LIVING ROOM -

SCALE: 1/2"

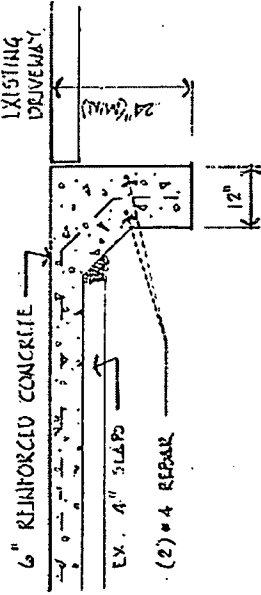
6

R506.1 General. Concrete slab-on-ground floors shall be a minimum 3.5 inches (89 mm) thick (for expansive soils, see Section R402.1.8). The specified compressive strength of concrete shall be as set forth in Section R402.2.

R506.2 Site preparation. The area within the foundation walls shall have all vegetation, top soil and foreign material removed.

R506.2.2 Base. A 4-inch-thick (102 mm) base course consisting of clean graded sand, gravel, crushed stone or crushed blast-furnace slag passing a 2-inch (51 mm) sieve shall be placed on the prepared subgrade when the slab is below grade.

R506.2.3 Vapor retarder. A 6 mil (0.006 inch; 152 µm) polyethylene or approved vapor retarder with joints lapped not less than 6 inches (152 mm) shall be placed between the concrete floor slab and the base course or the prepared subgrade where no base course exists.

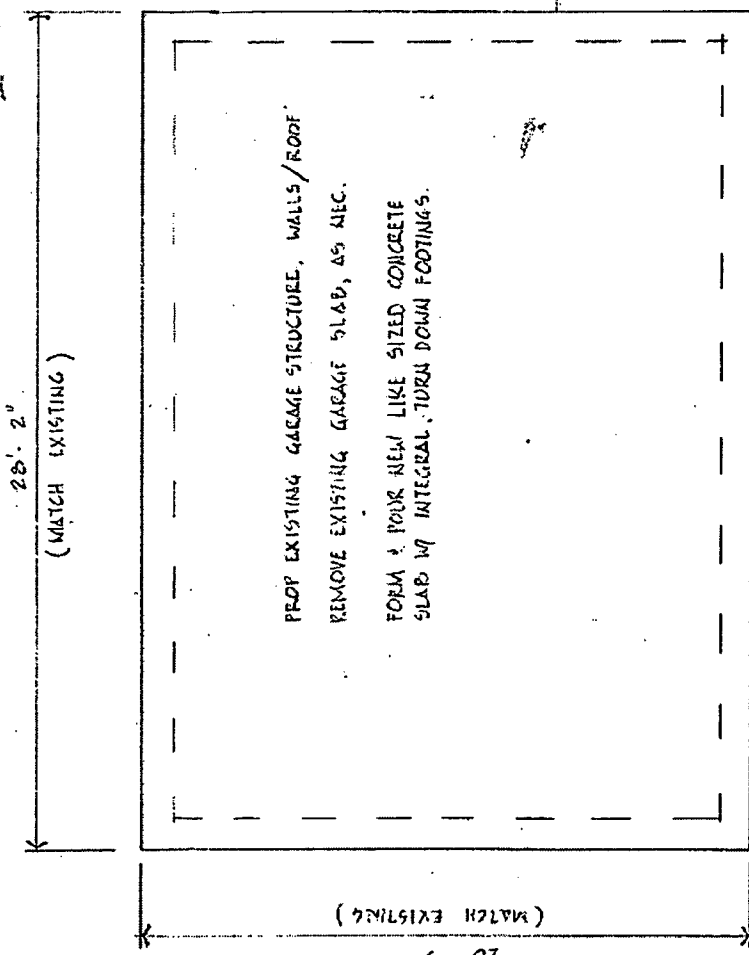


R506.3.4 Reinforcement support. Where provided in slabs on ground, reinforcement shall be supported to remain in place from the center to upper one third of the slab for the duration of the concrete placement.

R403.1.3.2 Slabs on ground with turned-down footings. Slabs on ground with turned down footings shall have a minimum of one No. 4 bar at the top and the bottom of the footing.

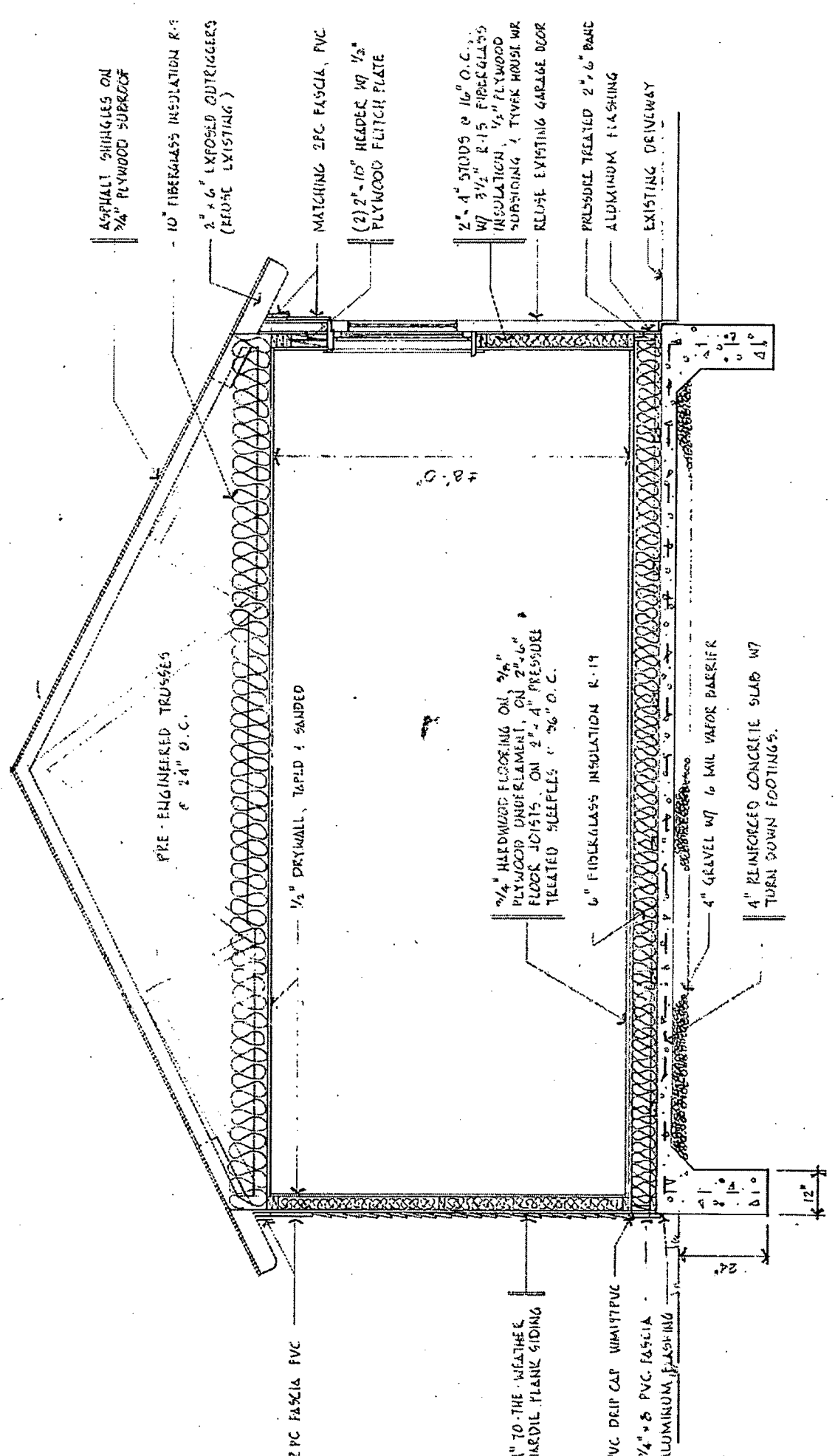
Exception: For slabs on ground cast simultaneously with the footing, locating one No. 5 bar or two No. 4 bars in the middle third of the footing depth shall be permitted as an alternative to placement at the footing top and bottom.

- DETAIL -  
SCALE: 1/2" = 1'-0"



EXISTING DRIVEWAY TO REMAIN

- FOUNDATION PLAN -  
SCALE: 1/4" = 1'-0"



ASPHALT SHINGLES ON  
3/4" PLYWOOD SUBDECK

PRE-ENGINEERED TRUSSES  
@ 24" O.C.

10" FIBERGLASS INSULATION R-19

2" x 4" EXPOSED OUTRIGGERS  
(REUSE EXISTING)

1/2" DRYWALL, TAPED & SANDED

MATCHING 2x4 PVC FASCIA, PVC  
(2) 2" x 10" HEADER W/ 1/2"  
PLYWOOD FLUSH PLATE

2" x 4" STUDS @ 16" O.C.,  
W/ 3/2" R-15 FIBERGLASS  
INSULATION, 1/2" PLYWOOD  
SHEATHING, TYVEK HOUSE WR

REUSE EXISTING GARAGE DOOR  
PRESSURE TREATED 2" x 6" BOND  
ALUMINUM FLASHING

EXISTING DRIVEWAY

3/4" HARDWOOD FLOORING ON 3/4"  
PLYWOOD UNDERLAMENT, ON 2" x 4"  
FLOOR JOISTS ON 2" x 4" PRESSURE  
TREATED SLEEPERS @ 36" O.C.

6" FIBERGLASS INSULATION R-19

4" GRAVEL W/ 6 MIL VAPOR BARRIER

4" REINFORCED CONCRETE SLAB W/  
TURN DOWN FOOTINGS.

2x4 PVC FASCIA

1" TO THE WEATHER  
HARDIE PLANK SIDING

PVC DEEP CAP WITH PVC

1/4" x 8" PVC FASCIA  
ALUMINUM FLASHING

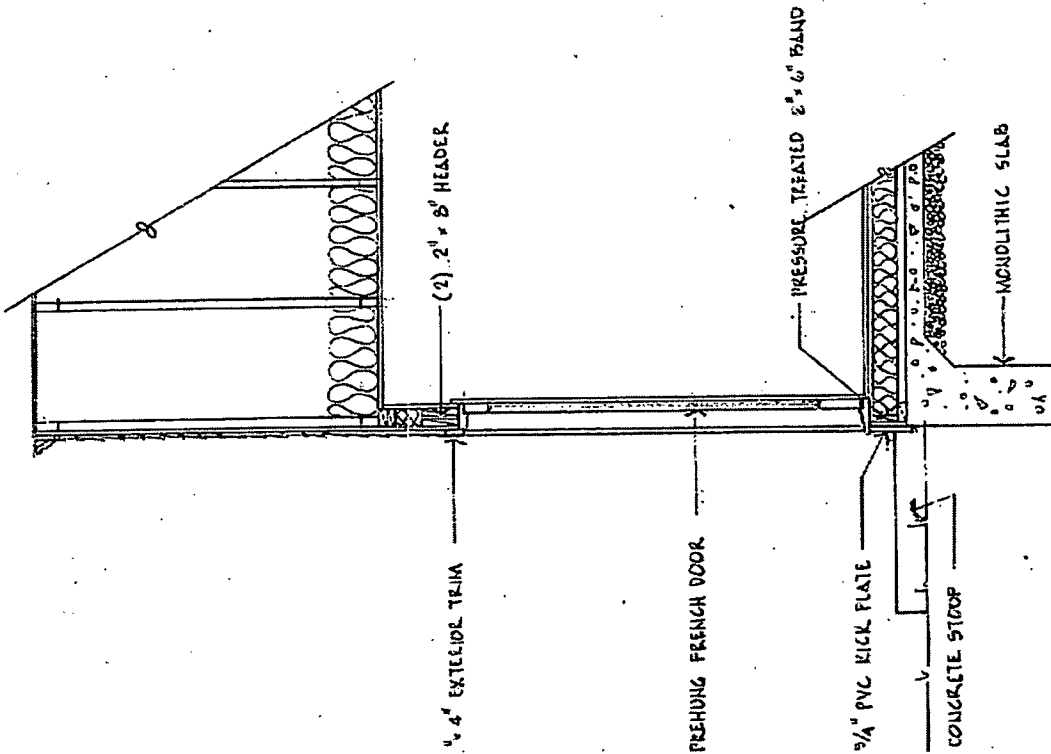
SECTION -

SCALE: 1/2" = 1'-0"



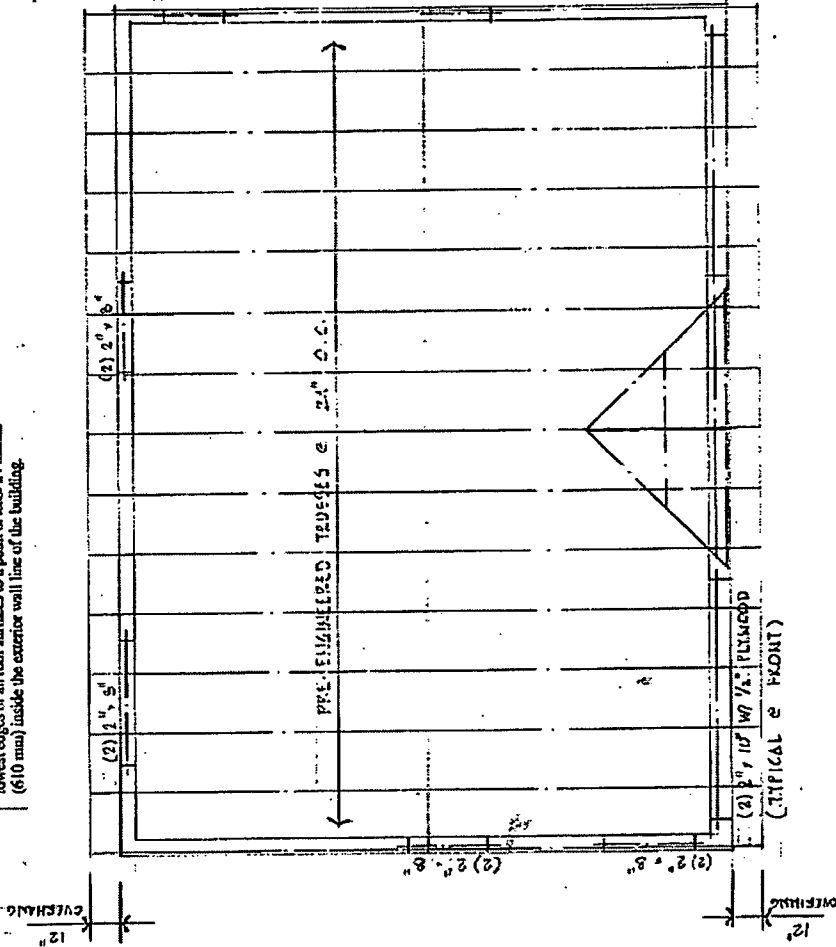
R905.2.7.1 Ice barrier. In areas where there has been a history of ice forming along the eaves causing a backup of water as designated in Table R301.2(1), an ice barrier that consists of at least two layers of underlayment (untempered together or of a self-adhering polymer modified bitumen sheet) shall be applied in lieu of normal underlayment and extend from the lowest edges of all roof surfaces to a point at least 24 inches (610 mm) inside the exterior wall line of the building.

R905.2.7.2 Underlayment and high wind. Underlayment applied in areas subject to high winds (above 110 mph (89 m/s) per Figure R301.2(4)) shall be applied with corrosion-resistant fasteners in accordance with manufacturer's installation instructions. Fasteners are to be applied along the overlap and fasteners spaced at 36 inches (914 mm) on center.



- DETAIL -

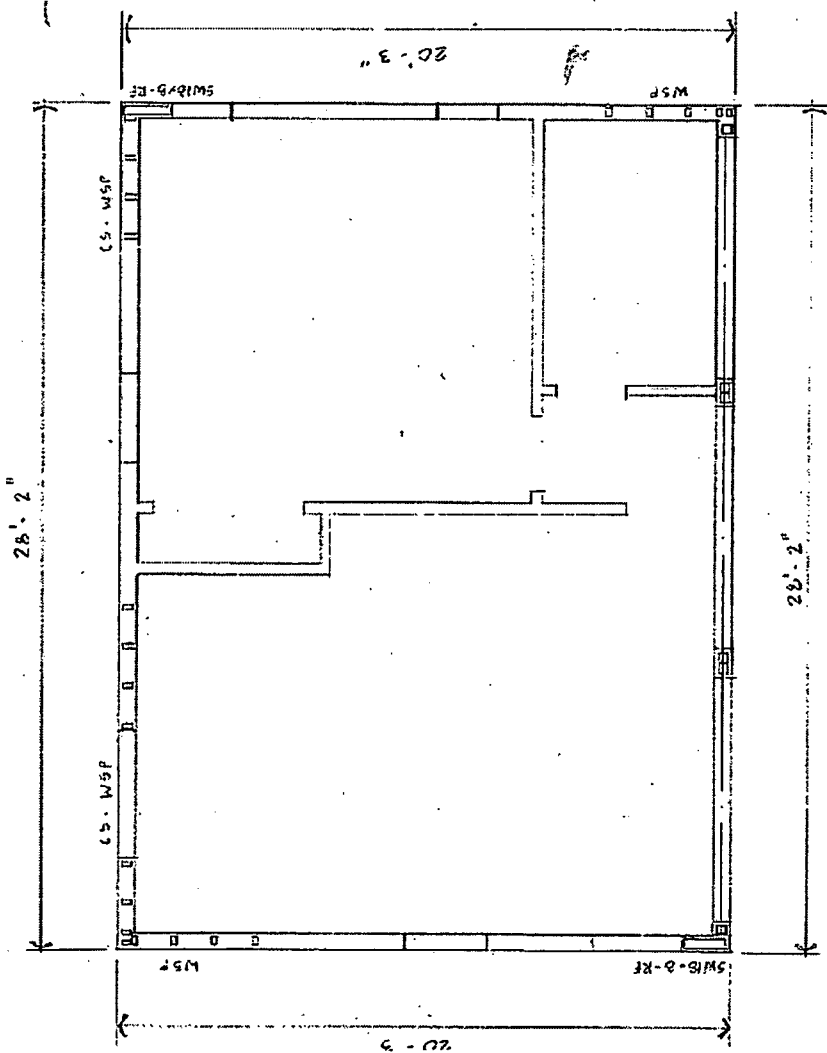
SCALE: 1/2" = 1'-0"



- ROOF FRAMING PLAN -

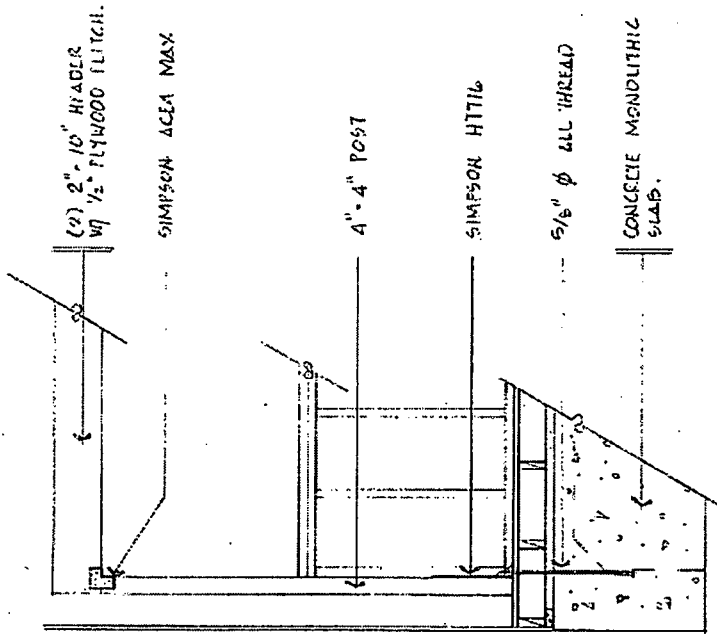
SCALE: 1/4" = 1'-0"





- WALL BRACING PLAN -

SCALE 1/4" = 1'-0"



- DETAIL @ GARAGE DOORS -

SCALE: 1/2" = 1'-0"

R602.10.4 Continuous sheathing. Braced wall lines with continuous sheathing shall be constructed in accordance with this section. All braced wall lines along exterior walls on the same story shall be continuously sheathed.

Exceptions: Within Seismic Design Categories A, B and C or in regions where the basic wind speed is less than or equal to 100 mph (45 m/s), other bracing methods prescribed by this code shall be permitted on other braced wall lines on the same story level or on any braced wall line on different story levels of the building.

R602.10.4.1 Continuous sheathing braced wall panels. Continuous sheathing methods require structural panel sheathing to be used on all sheathable surfaces on one side of a braced wall line including areas above and below openings and gable end walls. Braced wall panels shall be constructed in accordance with one of the methods listed in Table R602.10.4.1. Different bracing methods, other than those listed in Table R602.10.4.1, shall not be permitted along a braced wall line with continuous sheathing.

R602.10.4.2 Length of braced wall panels with continuous sheathing. Braced wall panels along a braced wall line with continuous sheathing shall be full-height with a length based on the adjacent clear opening height in accordance with Table R602.10.4.2 and Figure R602.10.4.2. Within a braced wall line when a panel has an opening on either side of differing heights, the taller opening height shall be used to determine the panel length from Table R602.10.4.2. For Method CS-PF, wall height shall be measured from the top of the header in the bottom of the bottom plate as shown in Figure R602.10.4.1.1.

R602.10.4.3 Length of bracing for continuous sheathing. Braced wall lines with continuous sheathing shall be provided with braced wall panels in the length required in Tables R602.10.1.2(1) and R602.10.1.2(2). Only those full-height braced wall panels complying with the length requirements of Table R602.10.4.2 shall be permitted to contribute to the minimum required length of bracing.

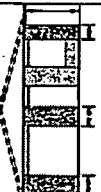
R602.10.4.4 Continuously sheathed braced wall panel location and corner construction. For all continuous sheathing methods, full-height braced wall panels complying with the length requirements of Table R602.10.4.2 shall be located at each end of a braced wall line with continuous sheathing and at least every 25 feet (7620 mm) on center. A minimum 24 inch (610 mm) wood structural panel corner return shall be provided at both ends of a braced wall line with continuous sheathing in accordance with Figures R602.10.4.4(1) and R602.10.4.4(2). In lieu of the corner return, a hold-down device with a minimum uplift design value of 800 pounds (3560 N) shall be fastened to the corner stud and to the foundation or framing below in accordance with Figure R602.10.4.4(3).

Exception: The first braced wall panel shall be permitted to begin 12.5 feet (3810 mm) from each end of the braced wall line in Seismic Design Categories A, B and C and 8 feet (2438 mm) in Seismic Design Categories D<sub>s</sub>, D<sub>1</sub> and D<sub>2</sub> provided one of the following is satisfied:

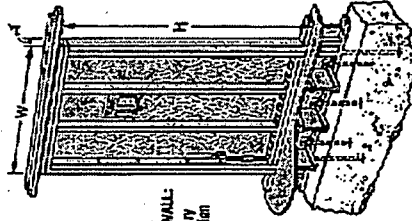
1. A minimum 24 inch (610 mm) long, full-height wood structural panel is provided at both ends of a corner constructed in accordance with Figure R602.10.4.4(1) at the braced wall line ends in accordance with Figure R602.10.4.4(4), or

2. The braced wall panel closest to the corner shall have a hold-down device with a minimum uplift design value of 800 pounds (3560 N) fastened to the stud at the edge of the braced wall panel closest to the corner and to the foundation or framing below in accordance with Figure R602.10.4.4(3).

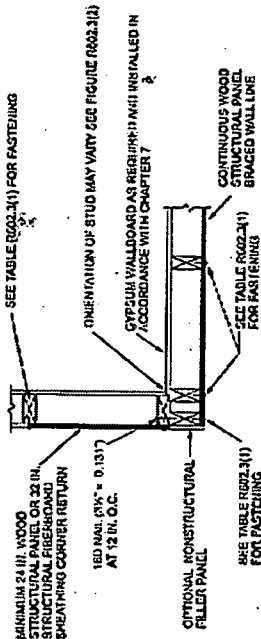
TABLE R602.10.4.1 CONTINUOUS SHEATHING METHODS

METHOD	MATERIAL	MINIMUM THICKNESS	FIGURE	CONNECTION CRITERIA
CS-WSP	Wood structural panel	3/4"		At minimum (2" x 0.131") nails at 6" spacing (panel edges) and at 12" spacing (intermediate supports) or 16 ga. x 1 1/2 staples at 6" spacing (panel edges) and 12" spacing (intermediate supports)

WOOD STRONG-WALL®  
See pages 79-80 for installation information.



RAISED-FLOOR WALL - First-Story Application



(a) OUTRIGS CORNER DETAIL

WOOD STRONG-WALL® WOOD FLOOR MODELS

Model No.	W (ft)	H (ft)	Y (ft)	Number of Fasteners to Top of Wall	Number of Fasteners to Side of Wall	Minimum Nails per Bolt	Qty. (Each)
SW1000-RF	10	23 1/4	2 1/2	9-SDS 3/8" x 8"	12-SDS 3/8" x 8"	2	2



Notes

Map of:  
**14 W Kirke St**  
Chevy Chase, MD 20815-4246



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GARAGE



Existing Property Condition Photographs (duplicate as needed)



Detail: REAR OF GARAGE

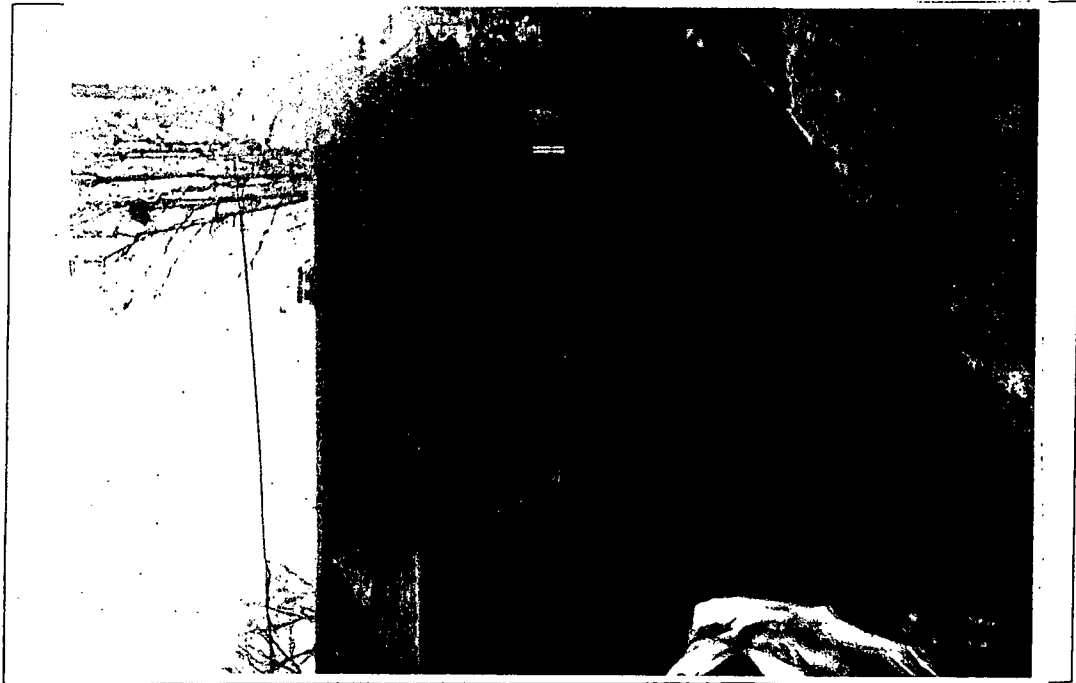


Detail: VIEW OF GARAGE FROM STREET

Applicant: W. REID THOMPSON

Page:

Existing Property Condition Photographs (duplicate as needed)



Detail: FRONT OF GARAGE



Detail: LEFT SIDE OF GARAGE

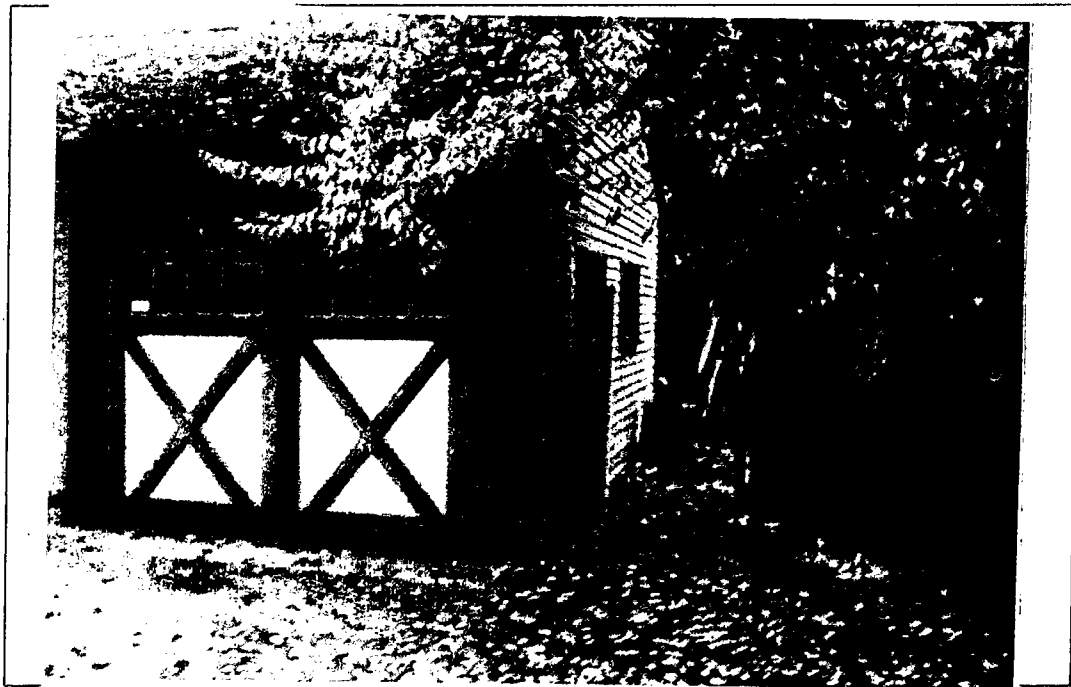
Applicant: W. REID THOMPSON

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Detail: FRONT OF GARAGE

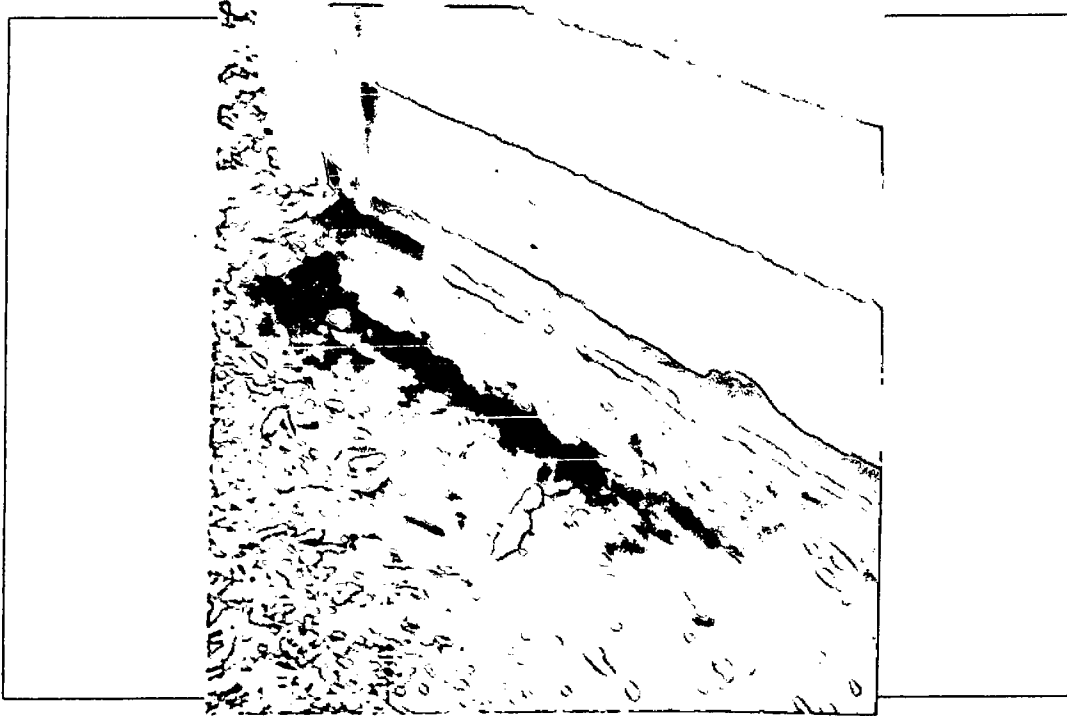


Detail: FRONT / RIGHT SIDE OF GARAGE

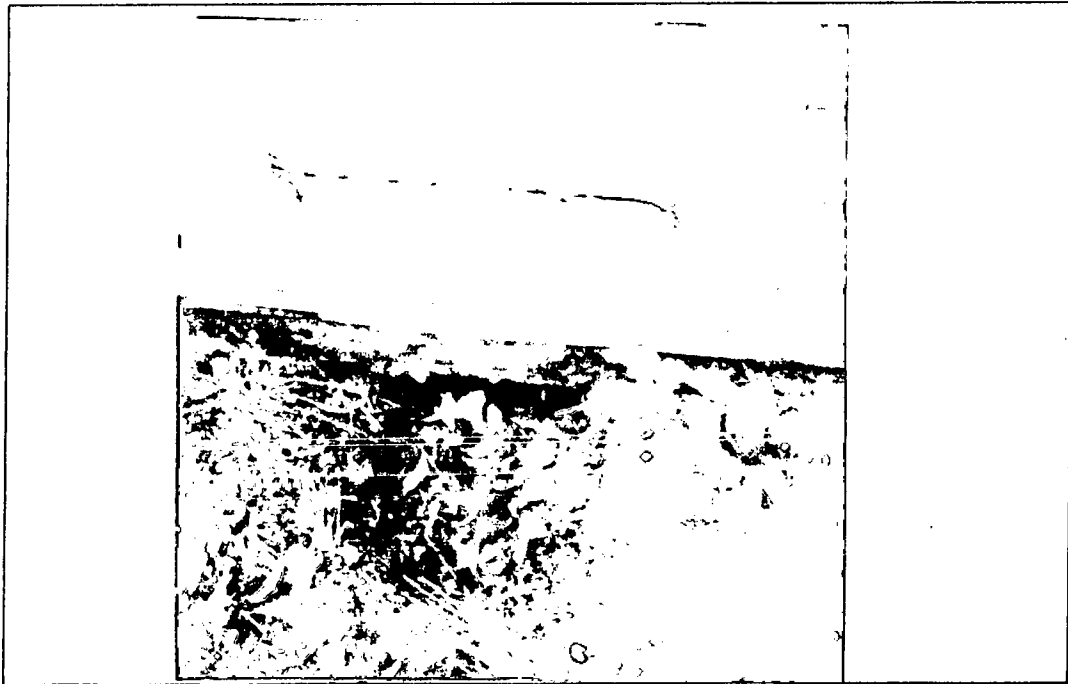
Applicant: W. REID THOMPSON

Page:

Existing Property Condition Photographs (duplicate as needed)



Detail: REAR CORNER, NO FOOTING BELOW SLAB



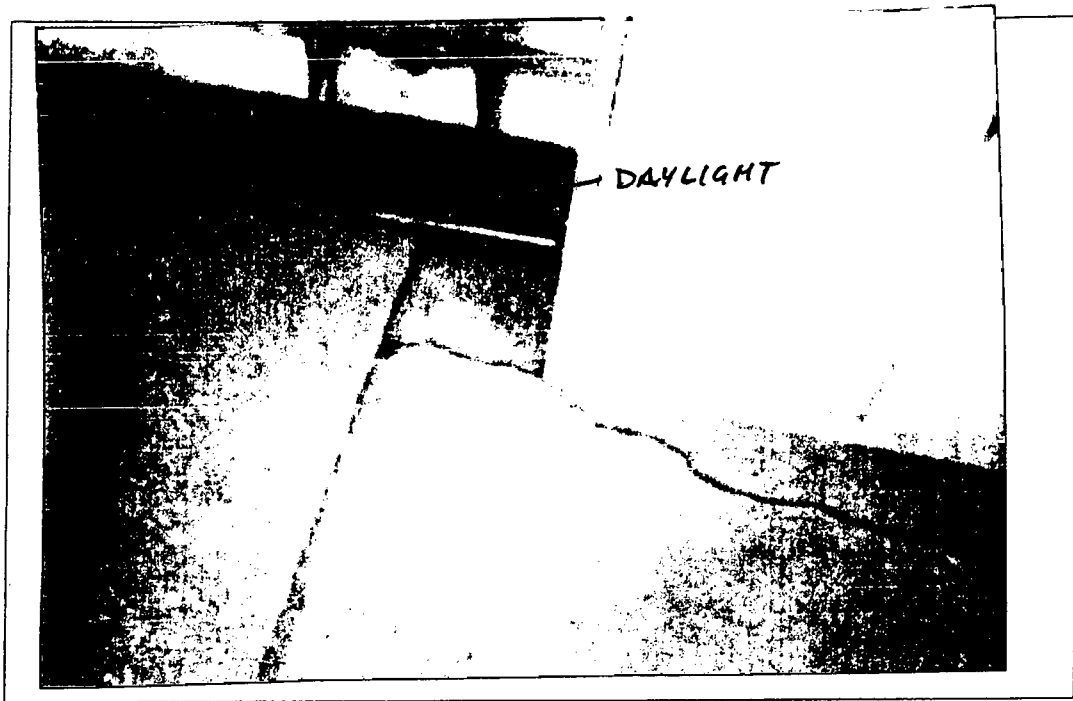
Detail: RIGHT SIDE, NO FOOTING BELOW SLAB

Applicant: \_\_\_\_\_

Page: \_\_\_



Existing Property Condition Photographs (duplicate as needed)



Detail: BADLY CRACKED SLAB



Detail: BADLY CRACKED SLAB

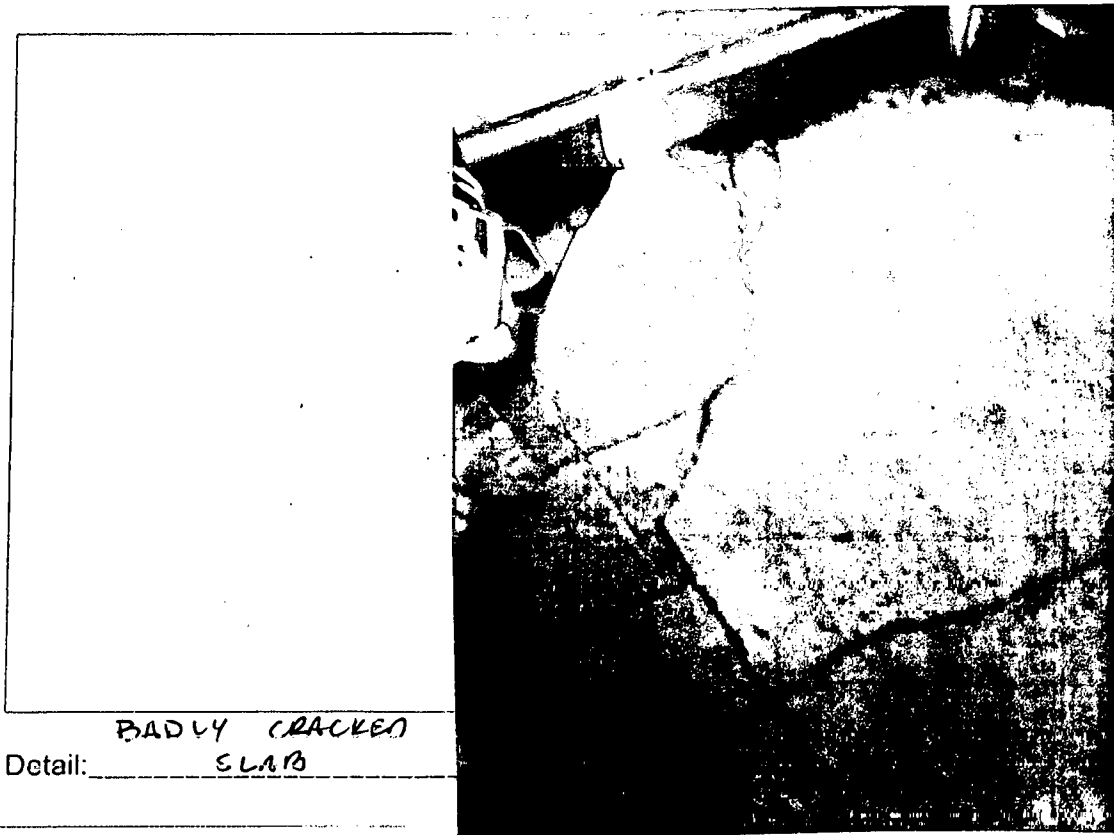
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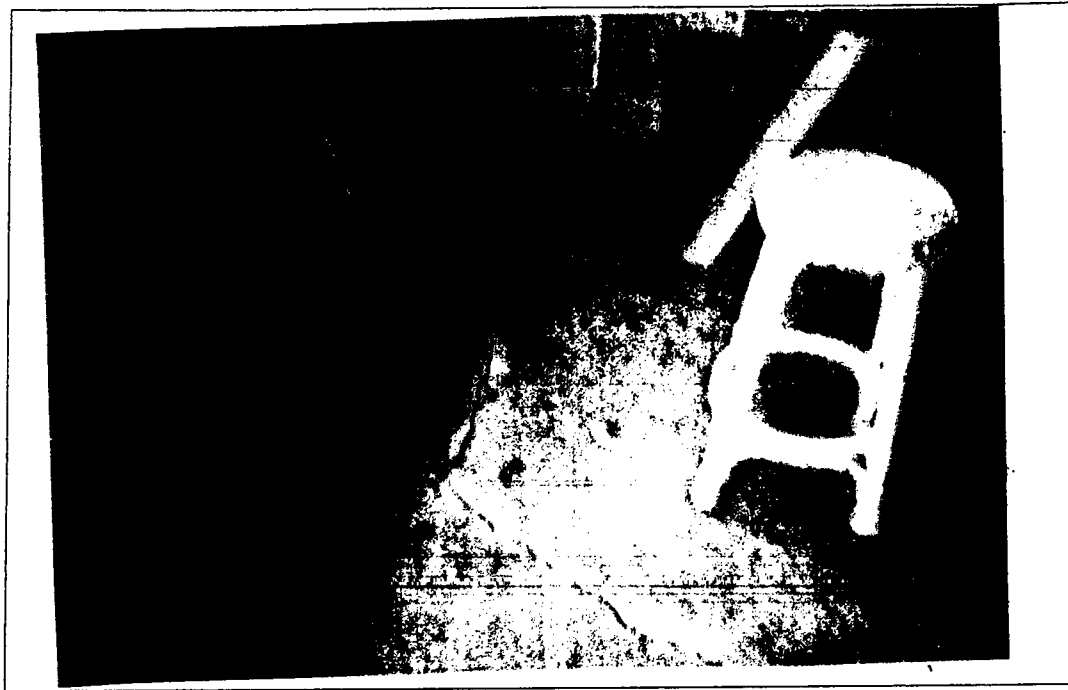


Detail: BADLY CRACKED SLAB

Applicant: W. REID THOMPSON

Page:

Existing Property Condition Photographs (duplicate as needed)



Detail: BADLY CRACKED SLAB



Detail: UNCONVENTIONAL SIDE WALL FRAMING  
NO SUBSIDING

Applicant: W. REID THOMPSON

Page:

Existing Property Condition Photographs (duplicate as needed)



Detail: UNCONVENTIONAL SIDE WALL FRAMING  
NO SUBSIDING



Detail: UNCONVENTIONAL REAR WALL FRAMING  
NO SUBSIDING

Applicant: W.L. REID THOMPSON

Page:

