



## HISTORIC PRESERVATION COMMISSION

Marc Elrich  
*County Executive*

Robert Sutton  
*Chairman*

Date: March 21, 2024

### MEMORANDUM

TO: Rabbiah Sabbakhan  
Department of Permitting Services

FROM: Chris Berger  
Historic Preservation Section  
Maryland-National Capital Park & Planning Commission

SUBJECT: Historic Area Work Permit # 1060757 - shared-use path

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The Montgomery County Historic Preservation Commission (HPC) has reviewed the attached application for a Historic Area Work Permit (HAWP). This application was **approved** at the March 20, 2024, HPC meeting.

The HPC staff has reviewed and stamped the attached construction drawings.

THE BUILDING PERMIT FOR THIS PROJECT SHALL BE ISSUED CONDITIONAL UPON ADHERENCE TO THE ABOVE APPROVED HAWP CONDITIONS AND MAY REQUIRE APPROVAL BY DPS OR ANOTHER LOCAL OFFICE BEFORE WORK CAN BEGIN.

Applicant: Jose Thommana (MCDOT)  
Address: 100 Edison Park Drive, Gaithersburg

This HAWP approval is subject to the general condition that the applicant will obtain all other applicable Montgomery County or local government agency permits. After the issuance of these permits, the applicant must contact this Historic Preservation Office if any changes to the approved plan are made. Once work is complete the applicant will contact Chris Berger at 301-495-4571 or [chris.berger@montgomeryplanning.org](mailto:chris.berger@montgomeryplanning.org) to schedule a follow-up site visit.





APPLICATION FOR HISTORIC AREA WORK PERMIT
HISTORIC PRESERVATION COMMISSION
301.563.3400

FOR STAFF ONLY:
HAWP# 1060757
DATE ASSIGNED

APPLICANT:

Name:
Address:
Daytime Phone:
E-mail:
City:
Zip:
Tax Account No.:

AGENT/CONTACT (if applicable):

Name:
Address:
Daytime Phone:
E-mail:
City:
Zip:
Contractor Registration No.:

LOCATION OF BUILDING/PREMISE: MIHP # of Historic Property

Is the Property Located within an Historic District? Yes/District Name
No/Individual Site Name

Is there an Historic Preservation/Land Trust/Environmental Easement on the Property? If YES, include a map of the easement, and documentation from the application.

Are other Planning and/or Hearing Examiner Approvals (Variance, Record Plat, etc.)? If Yes, include supplemental information.
REVIEWED
By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED
Montgomery County
Historic Preservation Commission
[Signature]

Building Number:
Street:
Town/City:
Nearest:
Lot:
Block:
Subdivision:
Parcer:

TYPE OF WORK PROPOSED: See the checklist on Page 4 to verify that all supporting items for proposed work are submitted with this application. Incomplete Applications will not be accepted for review. Check all that apply:

- New Construction
Addition
Demolition
Grading/Excavation
Deck/Porch
Fence
Hardscape/Landscape
Roof
Shed/Garage/Accessory Structure
Solar
Tree removal/planting
Window/Door
Other:

I hereby certify that I have the authority to make the foregoing application, that the application is correct and accurate and that the construction will comply with plans reviewed and approved by all necessary agencies and hereby acknowledge and accept this to be a condition for the issuance of this permit.

Jose Thommana

Signature of owner or authorized agent

Date

**HAWP APPLICATION: MAILING ADDRESSES FOR NOTIFYING**  
[Owner, Owner's Agent, Adjacent and Confronting Property Owners]

**Owner's mailing address**

**Owner's Agent's mailing address**

**Adjacent and confronting Property Owners mailing addresses**

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED

Montgomery County

Historic Preservation Commission



*Pamela A. Butler*

Description of Property: Please describe the building and surrounding environment. Include information on significant structures, landscape features, or other significant features of the property:

Description of Work Proposed: Please give an overview of the work to be undertaken:

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED

Montgomery County  
Historic Preservation Commission



Robert H. Potter



Work Item 1: \_\_\_\_\_

Description of Current Condition:

Proposed Work:

Work Item 2: \_\_\_\_\_

Description of Current Condition:

Proposed Work:

Work Item 3: \_\_\_\_\_

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

Description of Current Condition:

Proposed Work:

APPROVED  
Montgomery County  
Historic Preservation Commission  


**HISTORIC AREA WORK PERMIT  
CHECKLIST OF  
APPLICATION REQUIREMENTS**

	Required Attachments						
Proposed Work	I. Written Description	2. Site Plan	3. Plans/Elevations	4. Material Specifications	5. Photographs	6. Tree Survey	7. Property Owner Addresses
New Construction	*	*	*	*	*	*	*
Additions/Alterations	*	*	*	*	*	*	*
Demolition	*	*	*		*		*
Deck/Porch	*	*	*	*	*	*	*
Fence/Wall	*	*	*	*	*	*	*
Driveway/Parking Area	*	*		*	*	*	*
Grading/Excavation/Landscaping	*	*		*	*	*	*
Tree Removal	*	*		*	*	*	*
Siding/ Roof Changes	*	*	*	*	*		*
Window/Door Changes	*	*	*	*	*		*
Masonry Repair/Repoint	*	*	*	*	*		*
Signs	*	*	*	*	*		*

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED

Montgomery County

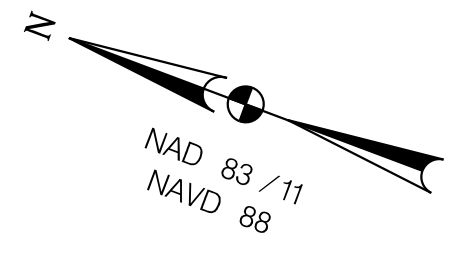
Historic Preservation Commission

- RIGHT-OF-WAY LEGEND**
- PROPOSED FEE SIMPLE
  - PROPOSED TEMPORARY CONSTRUCTION EASEMENT
  - SHARED MAINTENANCE AREA
  - EXISTING RIGHT-OF-WAY/PROPERTY LINES
  - EXISTING EASEMENT

- PAVING LEGEND**
- CONCRETE DRIVEWAY
  - ASPHALT SHARED USE PATH
  - FLEXIBLE SHARED USE PATH
  - MILL AND OVERLAY
  - CONCRETE SIDEWALK
  - DETECTABLE WARNING SURFACE
  - FULL DEPTH ASPHALT
  - PAVEMENT /SIDEWALK REMOVAL

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED  
Montgomery County  
Historic Preservation Commission  
*R. W. ...*



N 532750  
E 1304500

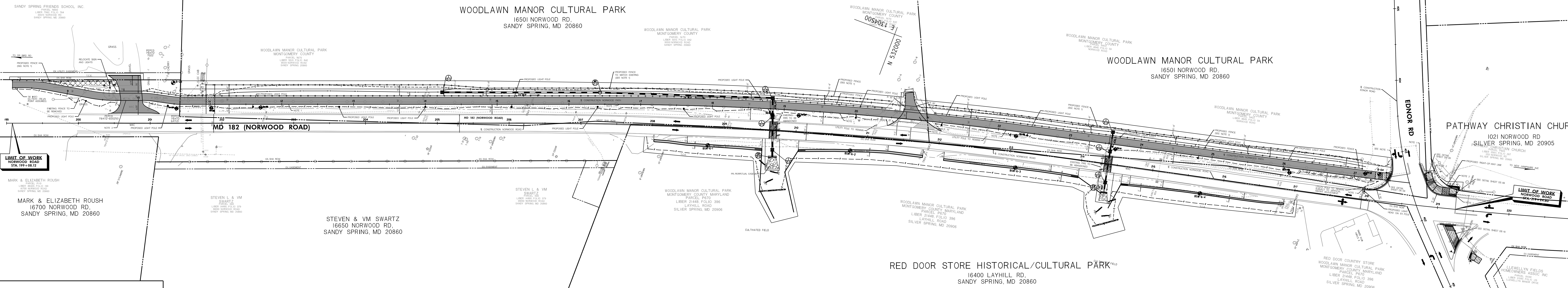
SANDY SPRINGS FRIENDS SCHOOL INC.  
16923 NORWOOD RD.  
SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK  
16501 NORWOOD RD.  
SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK  
16501 NORWOOD RD.  
SANDY SPRING, MD 20860

PATHWAY CHRISTIAN CHURCH  
1021 NORWOOD RD  
SILVER SPRING, MD 20905

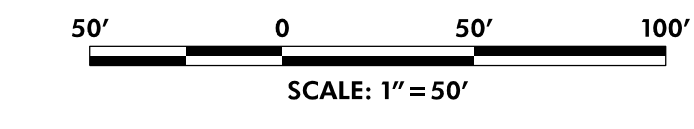
RED DOOR STORE HISTORICAL/CULTURAL PARK  
16400 LAYHILL RD.  
SANDY SPRING, MD 20860



LIMIT OF WORK  
NORWOOD ROAD  
STA. 1974+00.12

LIMIT OF WORK  
NORWOOD ROAD  
STA. 1974+00.12

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF TRANSPORTATION ENGINEERING  
HERITAGE TRIANGLE TRAIL PHASE 1  
DR. BIRD / NORWOOD ROAD  
SHARED USE PATH



NORWOOD SITE DISPLAY  
SCALE: 1" = 50' DWG. | OF |

APPROVED	DATE	Chief, Transportation Planning and Design Section
DESIGNED: MEG	CONTRACT NO. MR2022024	
DRAWN: MEG	ROAD NO.	JOB NO.
CHECKED: SAB	DATE	
APPROVED:	DATE	FILE NO.
Chief, Division of Transportation Engineering		

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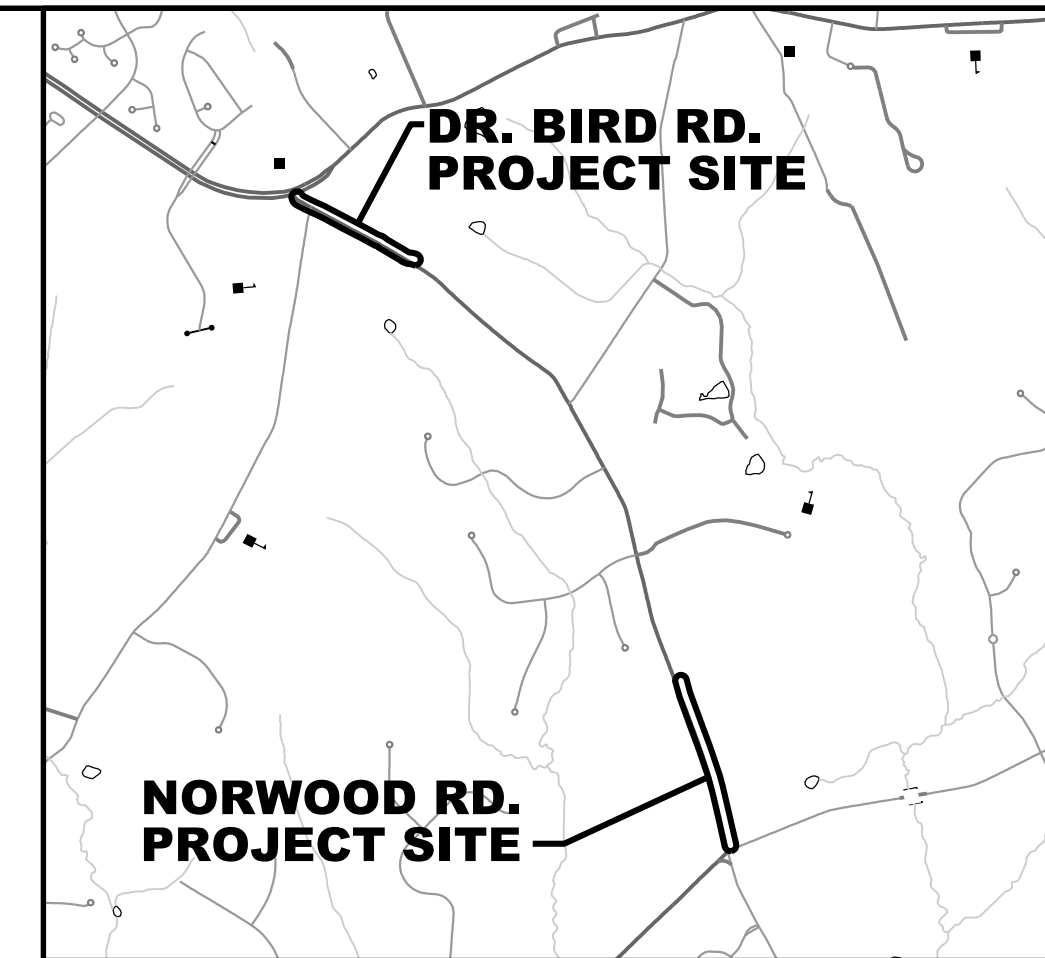
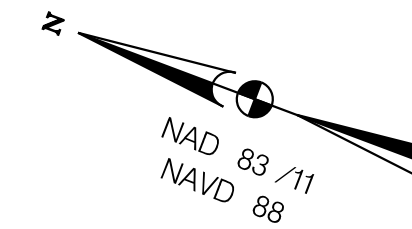


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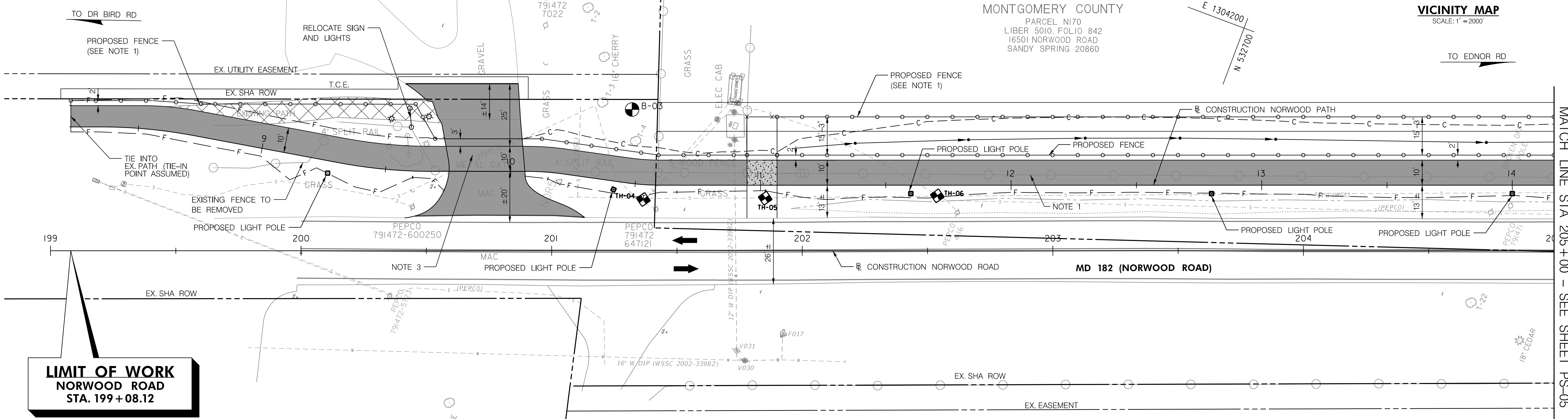
APPROVED  
Montgomery County  
Historic Preservation Commission  
*Robert L. Swartz*

SANDY SPRING FRIENDS SCHOOL INC.  
PARCEL N660  
LIBER 7992, FOLIO 784  
16923 NORWOOD RD  
SANDY SPRING, MD 20860

TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-04	4" GAS	532,900.4100	1,304,051.7500	422.34	425.94
TH-05	4" GAS	532,855.0800	1,304,069.3300	422.70	426.18
TH-06	4" GAS	(532,791.1100)	1,304,094.3400	422.61	426.78



VICINITY MAP  
SCALE: 1" = 2000'



**LIMIT OF WORK  
NORWOOD ROAD  
STA. 199+08.12**

MARK & ELIZABETH ROUSH  
PARCEL P110  
LIBER 48420, FOLIO 199  
16700 NORWOOD ROAD  
SANDY SPRING, MD 20860

STEVEN L & VM  
SWARTZ  
PARCEL 000  
LIBER 14490, FOLIO 379  
16650 NORWOOD ROAD  
SANDY SPRING, MD 20860

ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
4175 SF	MD 182 - STA. 200+88, LT TO STA. 205+00, LT
WOOD RAIL FENCE	
322 LF	MD 182 - STA. 201+78, 38.4' LT TO STA. 205+00, 38.2' LT
321 LF	MD 182 - STA. 201+79, 53.4' LT TO STA. 205+00, 53.4' LT

REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)	
134 LF	MD 182 - STA. 200+95, 31.5' LT TO STA. 201+79, 60.3' LT
310 LF	MD 182 - STA. 201+90, 30.9' LT TO STA. 205+00, 29.3' LT

**NOTES:**

- REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
- FENCE AND GATE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS. PROVIDE A 10' MINIMUM PEDESTRIAN PATHWAY WITH 2% MAXIMUM CROSS-SLOPE ACROSS ENTRANCE.



**OWNER / ADDRESS:**  
MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
100 EDISON PARK DRIVE  
GAITHERSBURG, MARYLAND

**CONTACT:**  
REBECCA PARK  
REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV  
MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
240-777-7263

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section \_\_\_\_\_ Date \_\_\_\_\_

APPROVED

Chief, Division of Transportation Engineering \_\_\_\_\_ Date \_\_\_\_\_

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY IMB

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF TRANSPORTATION ENGINEERING  
HERITAGE TRIANGLE TRAIL PHASE 1  
DR. BIRD / NORWOOD ROAD  
SHARED USE PATH  
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. PS-04 OF 07 SHEET NO. 22 OF 101

**LEGEND**

- CONCRETE DRIVEWAY
- ASPHALT SHARED USE PATH
- FLEXIBLE SHARED USE PATH
- MILL AND OVERLAY
- CONCRETE SIDEWALK
- DETECTABLE WARNING SURFACE
- FULL DEPTH ASPHALT
- PAVEMENT / SIDEWALK REMOVAL



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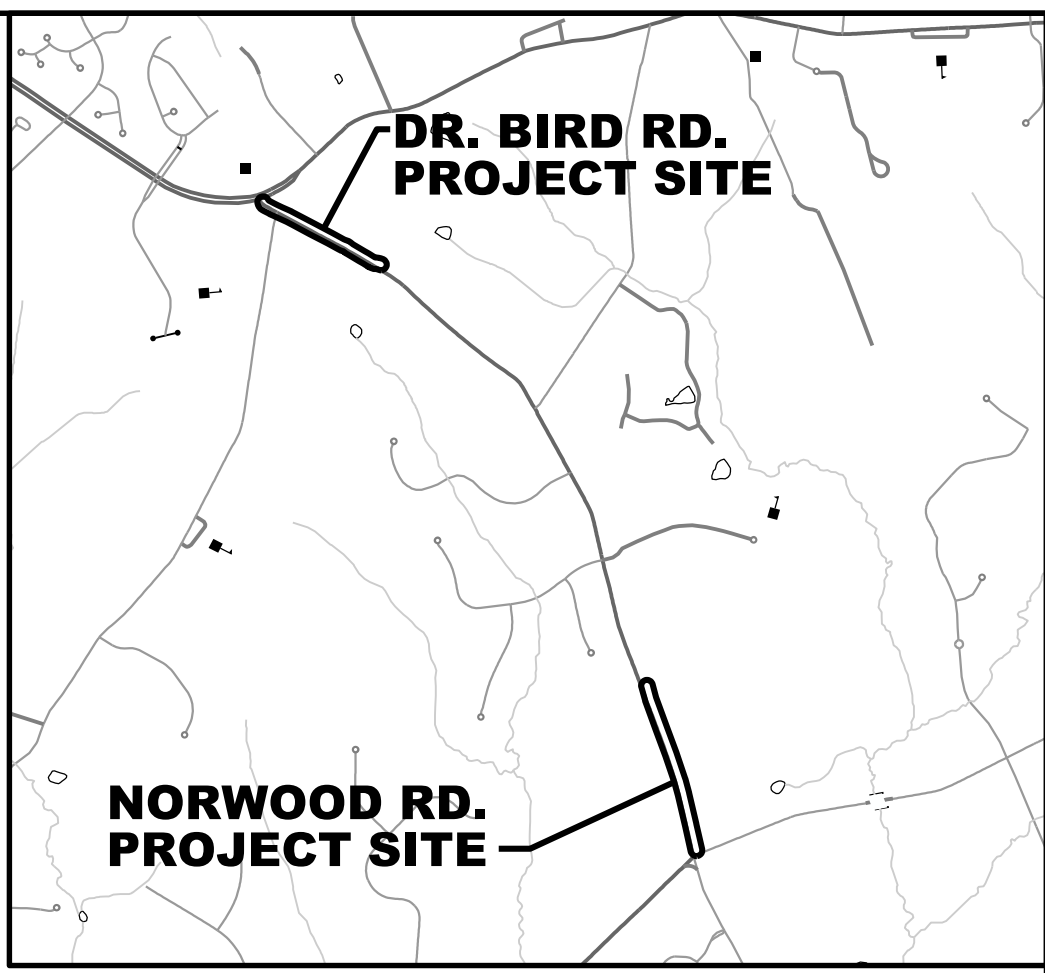
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FILE: \\ad.rk.com\ys\Cloud\Projects\2020\2097\_MCDOTransp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0004\_DrBird.dgn

PLOTTED: 3/14/2024  
FILE: \\ad.rk.com\ys\Cloud\Projects\2020\2097\_MCDOTransp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0004\_DrBird.dgn



ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
5022 SF	MD 182 - STA. 205+00, LT TO STA. 210+00, LT
WOOD RAIL FENCE	
322 LF	MD 182 - STA. 205+00, 38.2' LT TO STA. 210+00, 41.2' LT
321 LF	MD 182 - STA. 205+00, 53.4' LT TO STA. 210+00, 52.0' LT
REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)	
502 LF	MD 182 - STA. 205+00, 29.3' LT TO STA. 210+00, 28.8' LT

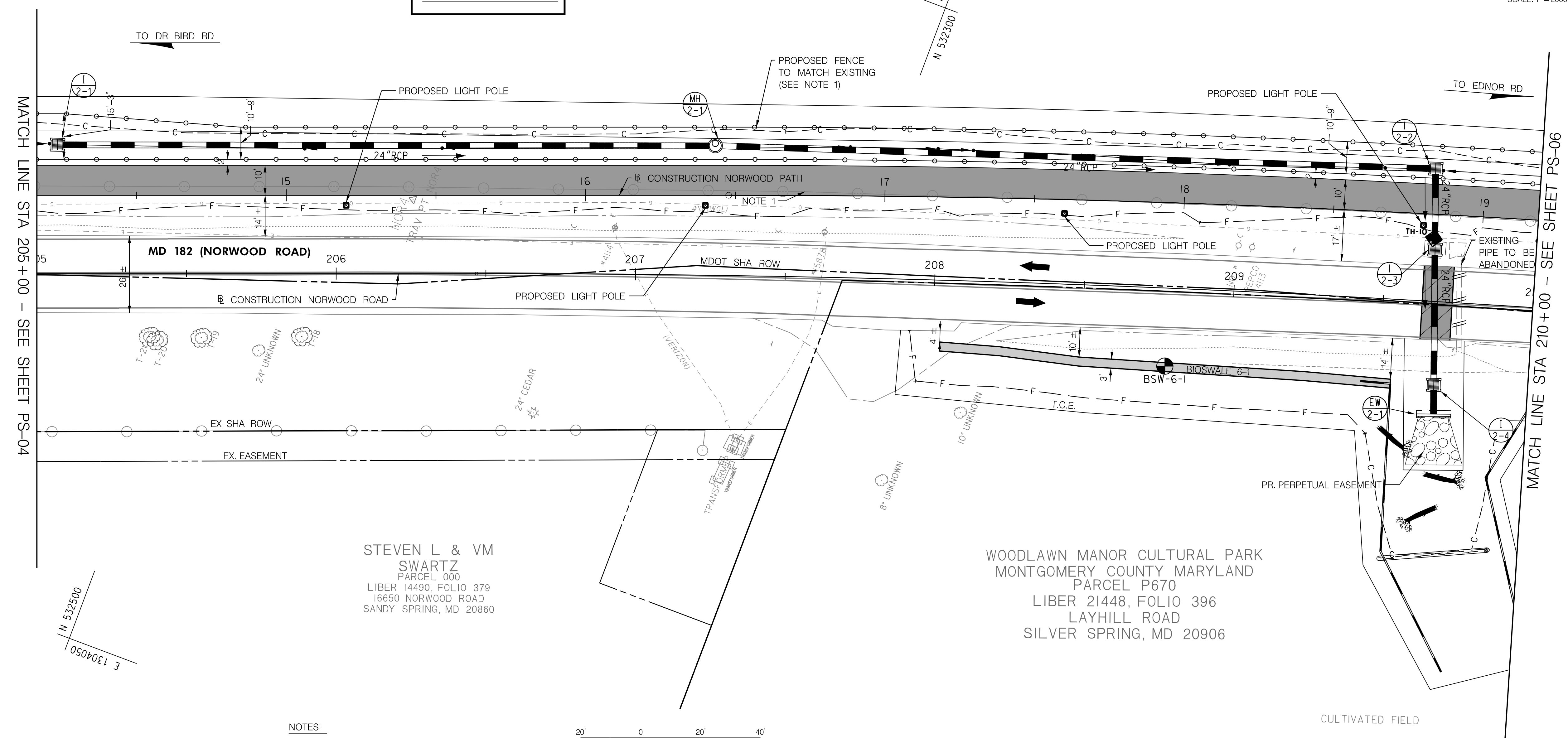
TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-10	4" GAS	532,119.8023	1,304,335.2552	417.82	422.09



**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED  
Montgomery County  
Historic Preservation Commission  
*Robert A. Norton*

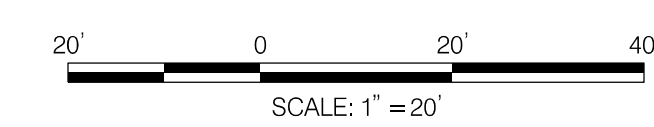
WOODLAWN MANOR CULTURAL PARK  
MONTGOMERY COUNTY  
PARCEL N170  
LIBER 5010, FOLIO 842  
16501 NORWOOD ROAD  
SANDY SPRING 20860



- NOTES:
- REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
  - FENCE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS.

LEGEND

	CONCRETE DRIVEWAY
	ASPHALT SHARED USE PATH
	FLEXIBLE SHARED USE PATH
	MILL AND OVERLAY
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	FULL DEPTH ASPHALT
	PAVEMENT / SIDEWALK REMOVAL



STEVEN L & VM SWARTZ  
PARCEL 000  
LIBER 14490, FOLIO 379  
16650 NORWOOD ROAD  
SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK  
MONTGOMERY COUNTY MARYLAND  
PARCEL P670  
LIBER 21448, FOLIO 396  
LAYHILL ROAD  
SILVER SPRING, MD 20906

OWNER / ADDRESS:  
MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
100 EDISON PARK DRIVE  
GAITHERSBURG, MARYLAND

CONTACT:  
REBECCA PARK  
REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV  
MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
240-777-7263

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section \_\_\_\_\_ Date \_\_\_\_\_

APPROVED

Chief, Division of Transportation Engineering \_\_\_\_\_ Date \_\_\_\_\_

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY IMB

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF TRANSPORTATION ENGINEERING  
HERITAGE TRIANGLE TRAIL PHASE 1  
DR. BIRD / NORWOOD ROAD  
SHARED USE PATH  
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. PS-05 OF 07 SHEET NO. 23 OF 101

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PLOTTED: 3/14/2024  
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FINE MILLING ASPHALT PAVEMENT 1 INCH TO 2.5 INCH DEPTH	
69 SY	MD 182 - STA. 211+27, LT TO STA. 212+00, LT

ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
1452 SF	MD 182 - STA. 210+00, LT TO STA. 211+47, LT
3273 SF	MD 182 - STA. 211+69, LT TO STA. 215+00, LT

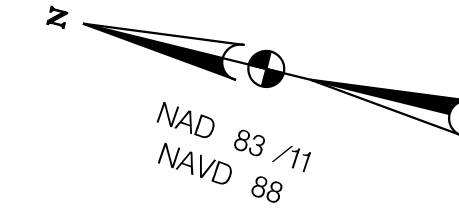
  

WOOD RAIL FENCE	
124 LF	MD 182 - STA. 210+00, 41.2' LT TO STA. 211+21, 39.2' LT
132 LF	MD 182 - STA. 210+00, 52.0' LT TO STA. 211+30, 51.9' LT
322 LF	MD 182 - STA. 211+81, 51.7' LT TO STA. 215+00, 51.0' LT
312 LF	MD 182 - STA. 211+90, 40.9' LT TO STA. 215+00, 40.3' LT

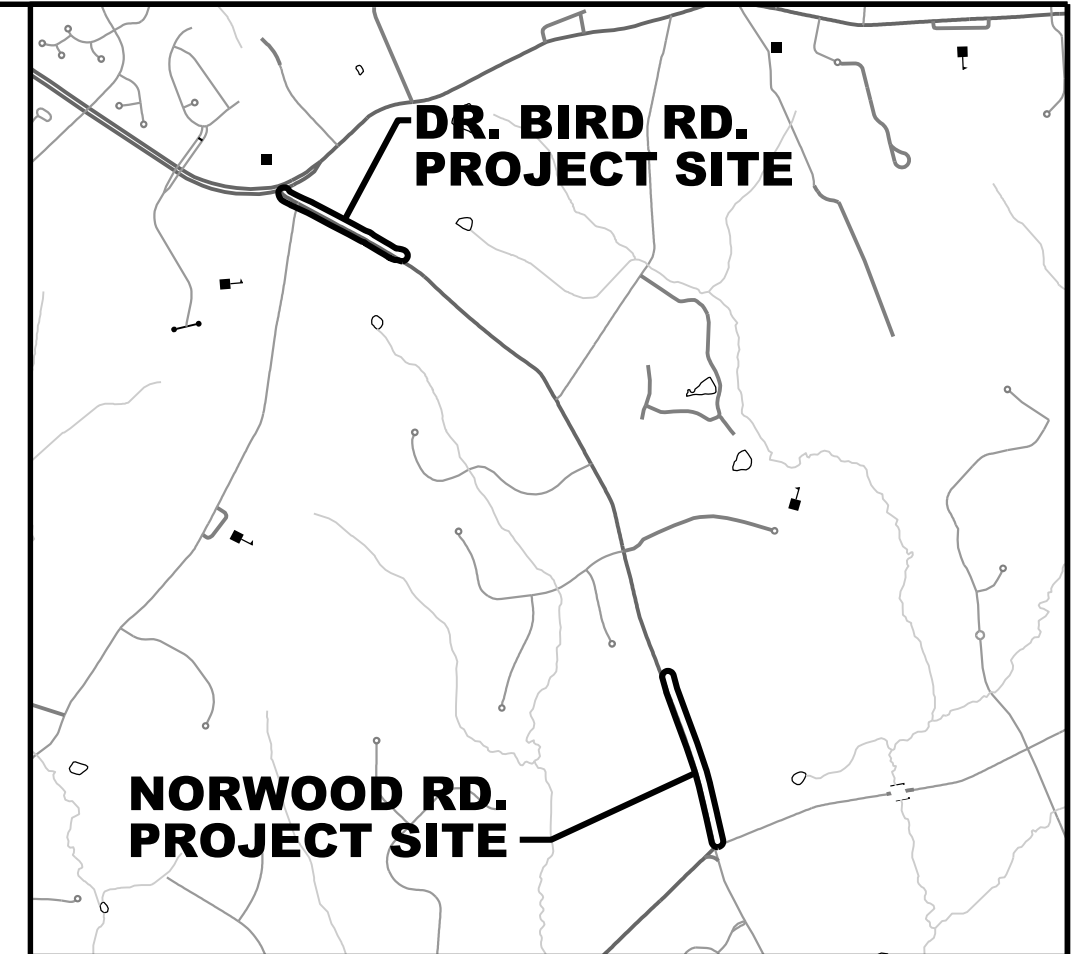
WOODLAWN MANOR CULTURAL PARK  
MONTGOMERY COUNTY  
PARCEL N170  
LIBER 5010, FOLIO 842  
16501 NORWOOD ROAD  
SANDY SPRING 20860

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED  
Montgomery County  
Historic Preservation Commission  
*[Signature]*

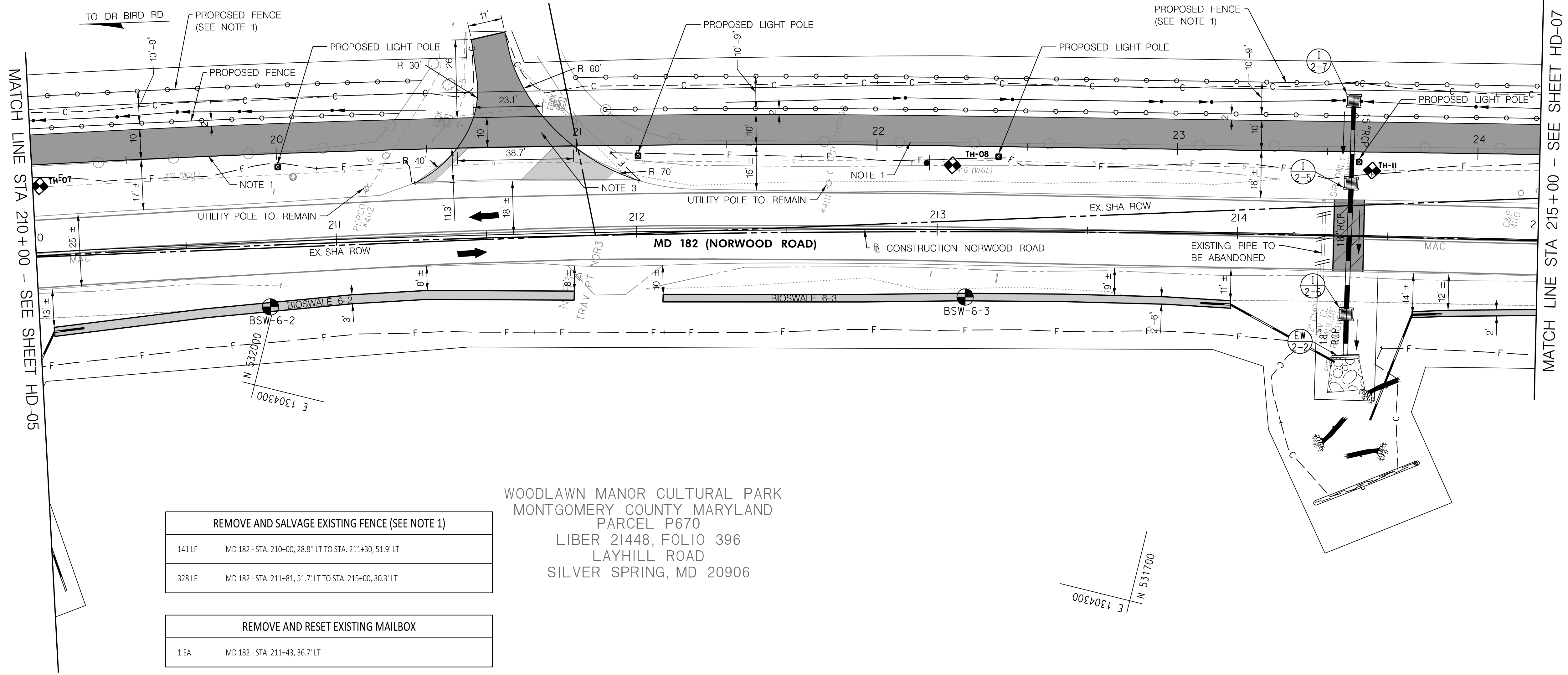


WOODLAWN MANOR CULTURAL PARK  
MONTGOMERY COUNTY  
PARCEL N422  
LIBER 4845, FOLIO 59  
NORWOOD ROAD



VICINITY MAP  
SCALE: 1" = 2000'

TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-07	4" GAS	532,085.2900	1,304,347.0200	419.60	421.76
TH-08	4" GAS	531,791.9800	1,304,426.4900	419.22	424.74
TH-11	4" GAS	531,655.9400	1,304,458.2100	418.88	422.00



REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)	
141 LF	MD 182 - STA. 210+00, 28.8' LT TO STA. 211+30, 51.9' LT
328 LF	MD 182 - STA. 211+81, 51.7' LT TO STA. 215+00, 30.3' LT

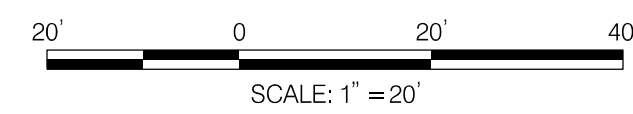
REMOVE AND RESET EXISTING MAILBOX	
1 EA	MD 182 - STA. 211+43, 36.7' LT

WOODLAWN MANOR CULTURAL PARK  
MONTGOMERY COUNTY MARYLAND  
PARCEL P670  
LIBER 21448, FOLIO 396  
LAYHILL ROAD  
SILVER SPRING, MD 20906

- NOTES:
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  - PROVIDE A 10' MINIMUM PEDESTRIAN PATHWAY WITH 2% MAXIMUM CROSS-SLOPE ACROSS ENTRANCE.

LEGEND

	CONCRETE DRIVEWAY
	ASPHALT SHARED USE PATH
	FLEXIBLE SHARED USE PATH
	MILL AND OVERLAY
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	FULL DEPTH ASPHALT
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MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
240-777-7263

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section \_\_\_\_\_ Date \_\_\_\_\_

APPROVED

Chief, Division of Transportation Engineering \_\_\_\_\_ Date \_\_\_\_\_

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY IMB

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF TRANSPORTATION ENGINEERING  
HERITAGE TRIANGLE TRAIL PHASE 1  
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SHARED USE PATH  
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. PS-06 OF 07 SHEET NO. 24 OF 101

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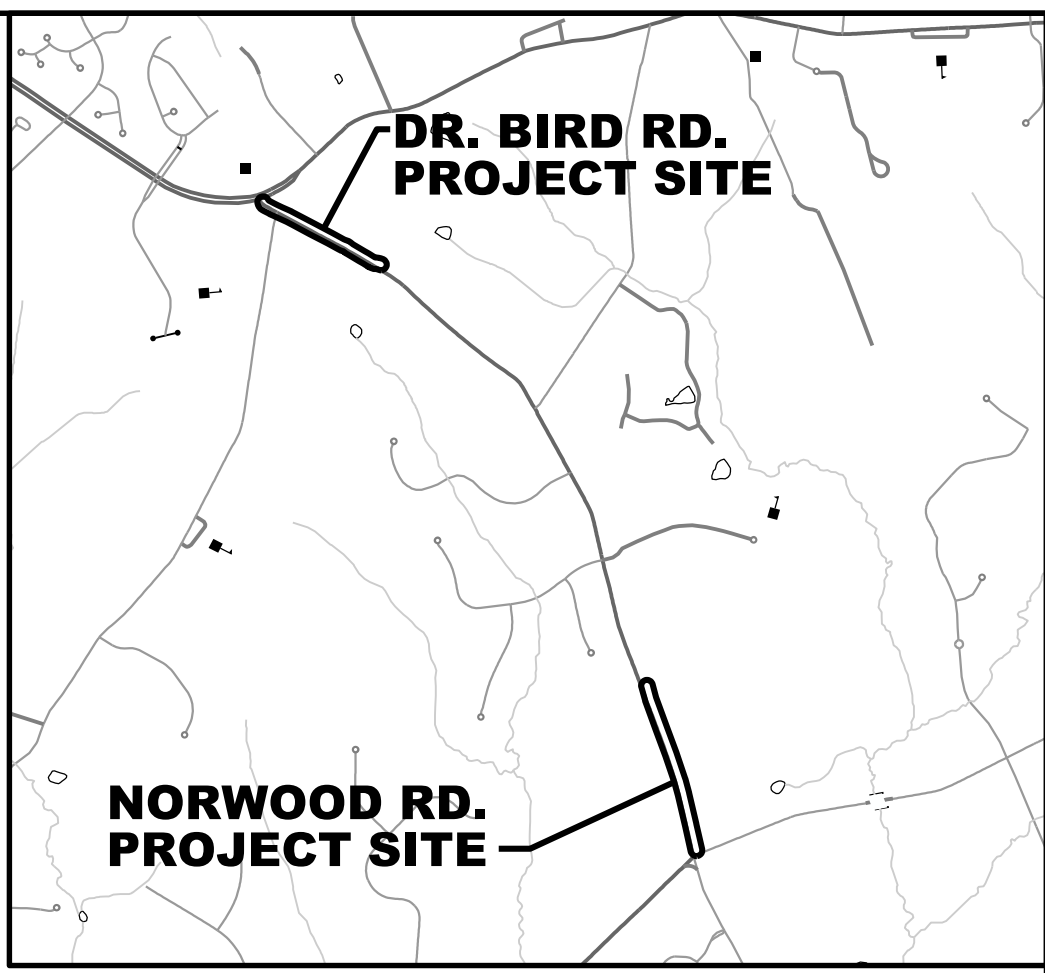
PLOTTER: 3142024  
 FILE: \\ad.rk.com\vs\Cloud\Projects\2020\2097\_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0006\_DrBird.dgn

PLOTTER: 3142024  
 FILE: \\ad.rk.com\vs\Cloud\Projects\2020\2097\_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0006\_DrBird.dgn



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TEST HOLE DATA					
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TH-10	4" GAS	532,119.8023	1,304,335.2552	417.82	422.09

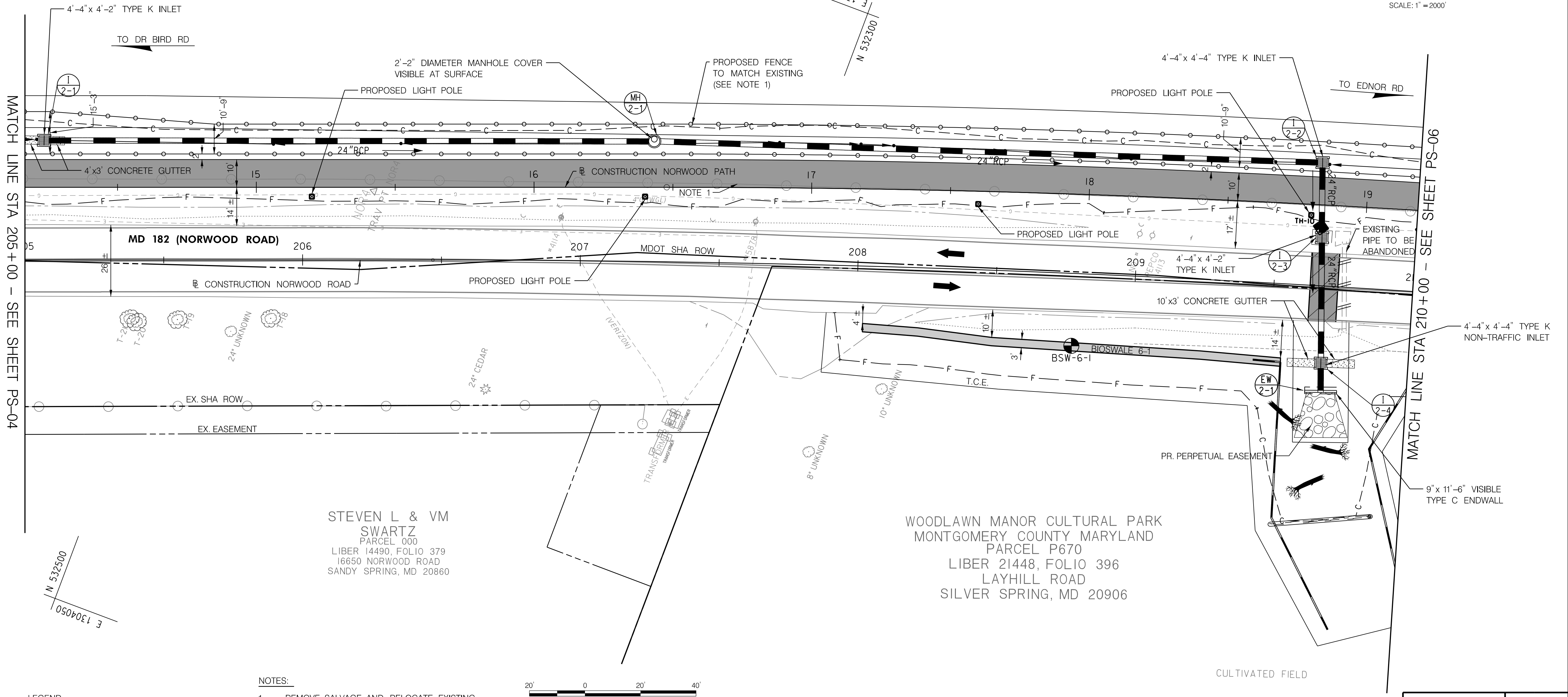


WOODLAWN MANOR CULTURAL PARK  
 MONTGOMERY COUNTY  
 PARCEL N170  
 LIBER 5010, FOLIO 842  
 16501 NORWOOD ROAD  
 SANDY SPRING 20860

APPROVED  
 Montgomery County  
 Historic Preservation Commission



**REVIEWED**  
 By Chris Berger at 11:20 am, Mar 21, 2024



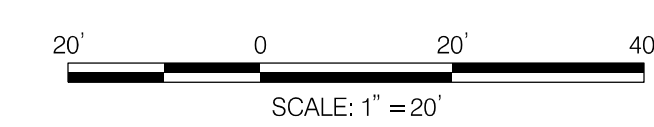
STEVEN L & VM  
 SWARTZ  
 PARCEL 000  
 LIBER 14490, FOLIO 379  
 16650 NORWOOD ROAD  
 SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK  
 MONTGOMERY COUNTY MARYLAND  
 PARCEL P670  
 LIBER 21448, FOLIO 396  
 LAYHILL ROAD  
 SILVER SPRING, MD 20906

- NOTES:
- REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
  - FENCE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS.

LEGEND

	CONCRETE DRIVEWAY
	ASPHALT SHARED USE PATH
	FLEXIBLE SHARED USE PATH
	MILL AND OVERLAY
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	FULL DEPTH ASPHALT
	PAVEMENT /SIDEWALK REMOVAL



OWNER / ADDRESS:  
 MONTGOMERY COUNTY  
 DEPARTMENT OF TRANSPORTATION  
 100 EDISON PARK DRIVE  
 GAITHERSBURG, MARYLAND

CONTACT:  
 REBECCA PARK  
 REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV  
 MONTGOMERY COUNTY  
 DEPARTMENT OF TRANSPORTATION  
 240-777-7263

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION ROCKVILLE, MARYLAND			
RECOMMENDED FOR APPROVAL			
Chief, Transportation Planning and Design Section	Date		
APPROVED			
Chief, Division of Transportation Engineering	Date		
DESIGNED BY _____	DRAWN BY _____	CHECKED BY _____	
NO.	REVISION	DATE	BY

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION DIVISION OF TRANSPORTATION ENGINEERING HERITAGE TRIANGLE TRAIL PHASE 1 DR. BIRD / NORWOOD ROAD SHARED USE PATH ROADWAY PLAN		
SCALE 1" = 20'	DATE MAY 2024	
DRAWING NO. _____	OF 07	SHEET NO. 23 OF 101



P: 410.728.2900  
 700 East Pratt Street, Suite 500 | Baltimore, MD 21202

Engineers | Construction Managers | Planners | Scientists  
 www.rk.com

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PLOTFILES: 3/14/2024  
 FILE: \\ad.rk.com\ys\Cloud\Projects\2020\202097\_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0005\_DrBird.dgn

PLOTTED: 3/14/2024  
 FILE: \\ad.rk.com\ys\Cloud\Projects\2020\202097\_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0005\_DrBird.dgn



ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
3081 SF	MD 182 - STA. 215+00, LT TO STA. 218+09, LT

5 INCH CONCRETE SIDEWALK	
232 SF	MD 182 - STA. 218+09, LT TO STA. 218+31, LT
648 SF	MD 182 - STA. 218+69, LT TO STA. 219+24, LT
352 SF	MD 182 - STA. 219+10, RT TO STA. 219+35, RT

MDOT SHA STANDARD TYPE A CURB 8 INCH X 16 INCH MINIMUM (STD. NO. MD 620.02)	
81 LF	MD 182 - STA. 218+69, LT TO STA. 219+24, LT
24 LF	MD 182 - STA. 219+23, 22.9 RT TO STA. 219+35, 43.4 RT

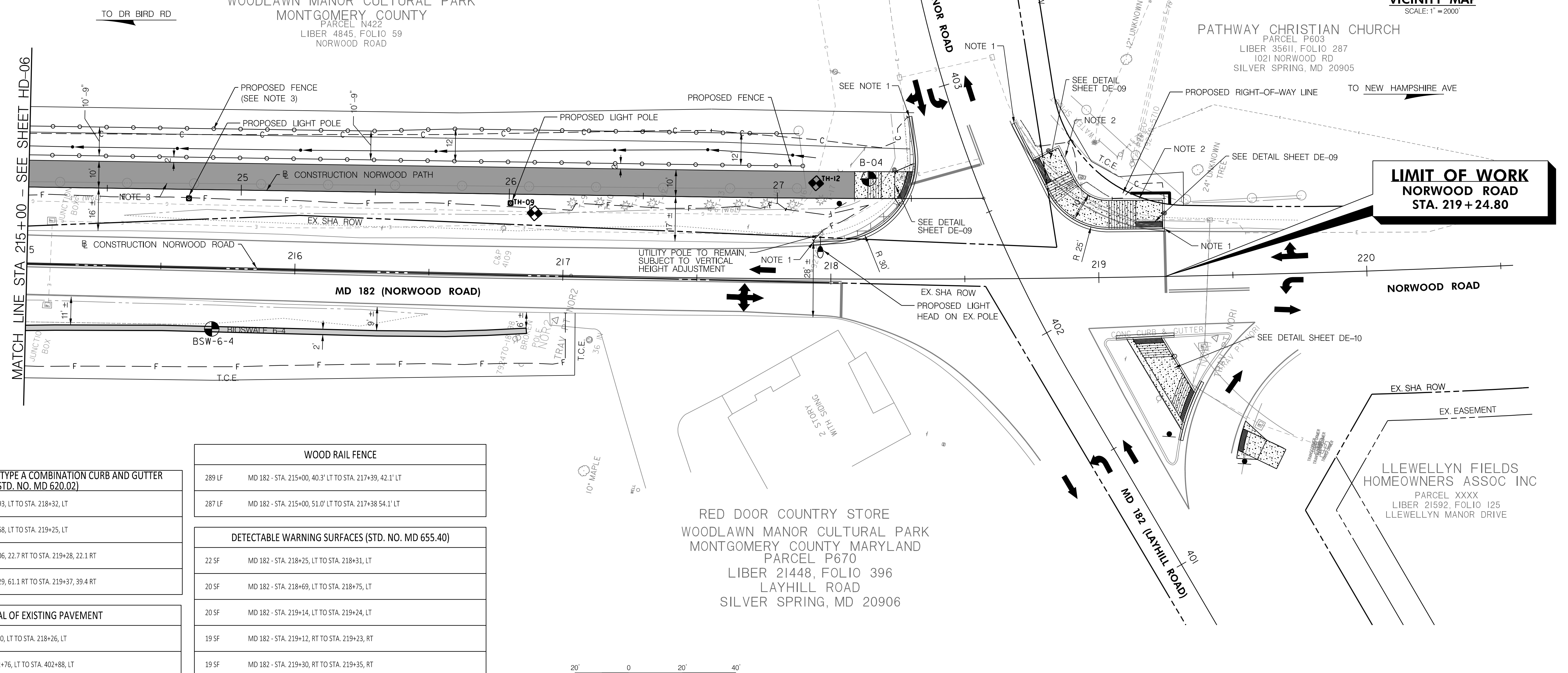
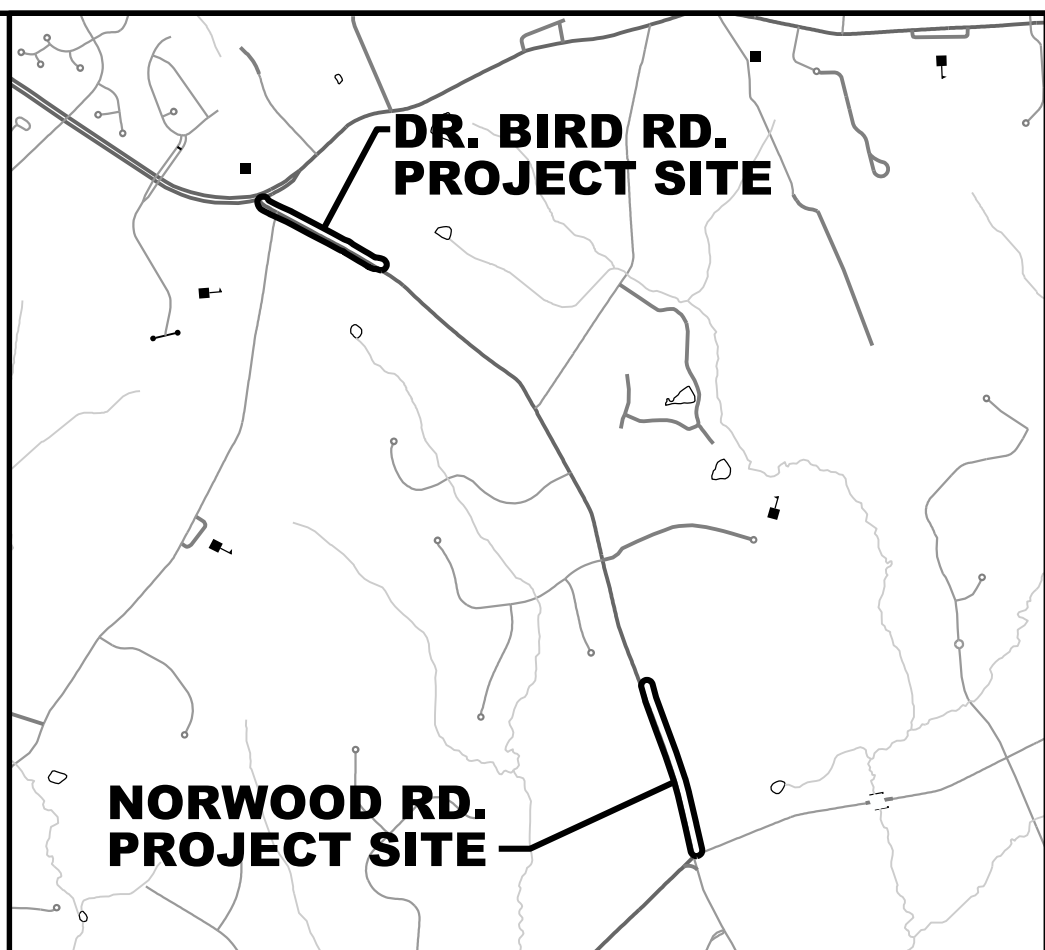
REMOVE AND SALVAGE EXISTING FENCE	
141 LF	MD 182 - STA. 215+00, 30.3' LT TO STA. 217+41, 54.1' LT

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED  
Montgomery County  
Historic Preservation Commission  
*Robert A. ...*

TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-09	4" GAS	531,417.3700	1,304,512.4600	421.14	425.72
TH-12	4" GAS	531,318.0700	1,304,548.4800	425.54	427.79

WOODLAWN MANOR CULTURAL PARK  
MONTGOMERY COUNTY  
PARCEL N422  
LIBER 4845, FOLIO 59  
NORWOOD ROAD



MDOT SHA STANDARD TYPE A COMBINATION CURB AND GUTTER (STD. NO. MD 620.02)	
72 LF	MD 182 - STA. 217+93, LT TO STA. 218+32, LT
78 LF	MD 182 - STA. 218+68, LT TO STA. 219+25, LT
22 LF	MD 182 - STA. 219+06, 22.7 RT TO STA. 219+28, 22.1 RT
24 LF	MD 182 - STA. 219+29, 61.1 RT TO STA. 219+37, 39.4 RT

REMOVAL OF EXISTING PAVEMENT	
3 CY	MD 182 - STA. 218+00, LT TO STA. 218+26, LT
1 CY	EDNOR RD - STA. 402+76, LT TO STA. 402+88, LT

WOOD RAIL FENCE	
289 LF	MD 182 - STA. 215+00, 40.3' LT TO STA. 217+39, 42.1' LT
287 LF	MD 182 - STA. 215+00, 51.0' LT TO STA. 217+38 54.1' LT

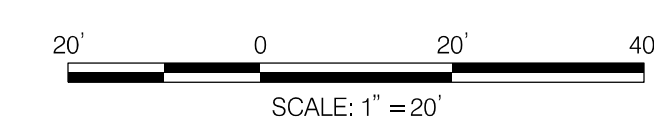
  

DETECTABLE WARNING SURFACES (STD. NO. MD 655.40)	
22 SF	MD 182 - STA. 218+25, LT TO STA. 218+31, LT
20 SF	MD 182 - STA. 218+69, LT TO STA. 218+75, LT
20 SF	MD 182 - STA. 219+14, LT TO STA. 219+24, LT
19 SF	MD 182 - STA. 219+12, RT TO STA. 219+23, RT
19 SF	MD 182 - STA. 219+30, RT TO STA. 219+35, RT

**LEGEND**

- CONCRETE DRIVEWAY
- ASPHALT SHARED USE PATH
- FLEXIBLE SHARED USE PATH
- MILL AND OVERLAY
- CONCRETE SIDEWALK
- DETECTABLE WARNING SURFACE
- FULL DEPTH ASPHALT
- PAVEMENT/SIDEWALK REMOVAL

- NOTES:**
- NOSE DOWN CURB OVER 2 FEET TO MATCH EXISTING EDGE OF ROAD.
  - ALL CONCRETE BACKING CURB SHALL BE PAID FOR AS MDOT SHA TYPE A CURB, ANY HEIGHT OR DEPTH, 12" MAXIMUM HEIGHT. REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
  - IF CONTRACTOR ENCOUNTERS LONGITUDINAL UNDERDRAIN THEY SHALL REPAIR AND REPLACE IN ACCORDANCE WITH STD. MD 387.11. FENCE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS.



**OWNER/ADDRESS:**  
MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
100 EDISON PARK DRIVE  
GAITHERSBURG, MARYLAND

**CONTACT:**  
REBECCA PARK  
REBECCA.PARK@MONTGOMERYCOUNTYMD.GOV  
MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
240-777-7263

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section \_\_\_\_\_ Date \_\_\_\_\_

APPROVED

Chief, Division of Transportation Engineering \_\_\_\_\_ Date \_\_\_\_\_

DESIGNED BY KBJ/RLW DRAWN BY KBJ CHECKED BY TMB

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF TRANSPORTATION ENGINEERING  
HERITAGE TRIANGLE TRAIL PHASE 1  
DR. BIRD / NORWOOD ROAD  
SHARED USE PATH  
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

DRAWING NO. PS-07 OF 07 SHEET NO. 25 OF 101

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PLOTTER: 3/14/2024  
 FILE: \\ad.rk.com\rs\Cloud\Projects\2020\202097\_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0007\_DrBird.dgn

PLOTTED: 3/14/2024  
FILE: \\ad.rk.com\rs\Cloud\Projects\2020\202097\_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0007\_DrBird.dgn

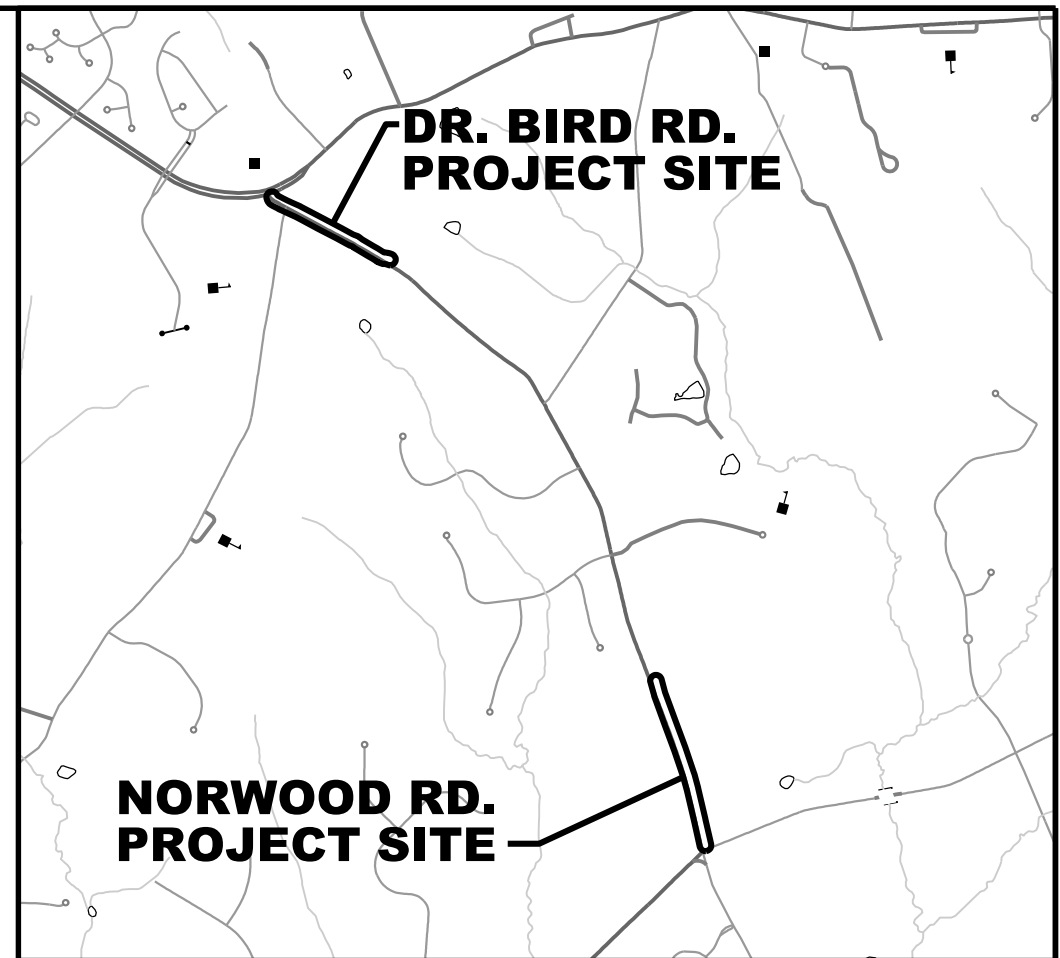
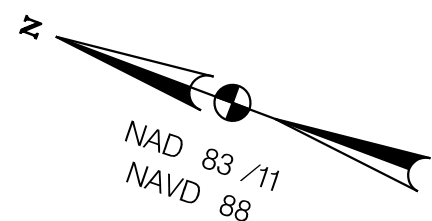


ASPHALT SHARED USE PATH (STD. NO. MD 580.08)	
5022 SF	MD 182 - STA. 205+00, LT TO STA. 210+00, LT
WOOD RAIL FENCE	
322 LF	MD 182 - STA. 205+00, 38.2' LT TO STA. 210+00, 41.2' LT
321 LF	MD 182 - STA. 205+00, 53.4' LT TO STA. 210+00, 52.0' LT
REMOVE AND SALVAGE EXISTING FENCE (SEE NOTE 1)	
502 LF	MD 182 - STA. 205+00, 29.3' LT TO STA. 210+00, 28.8' LT

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

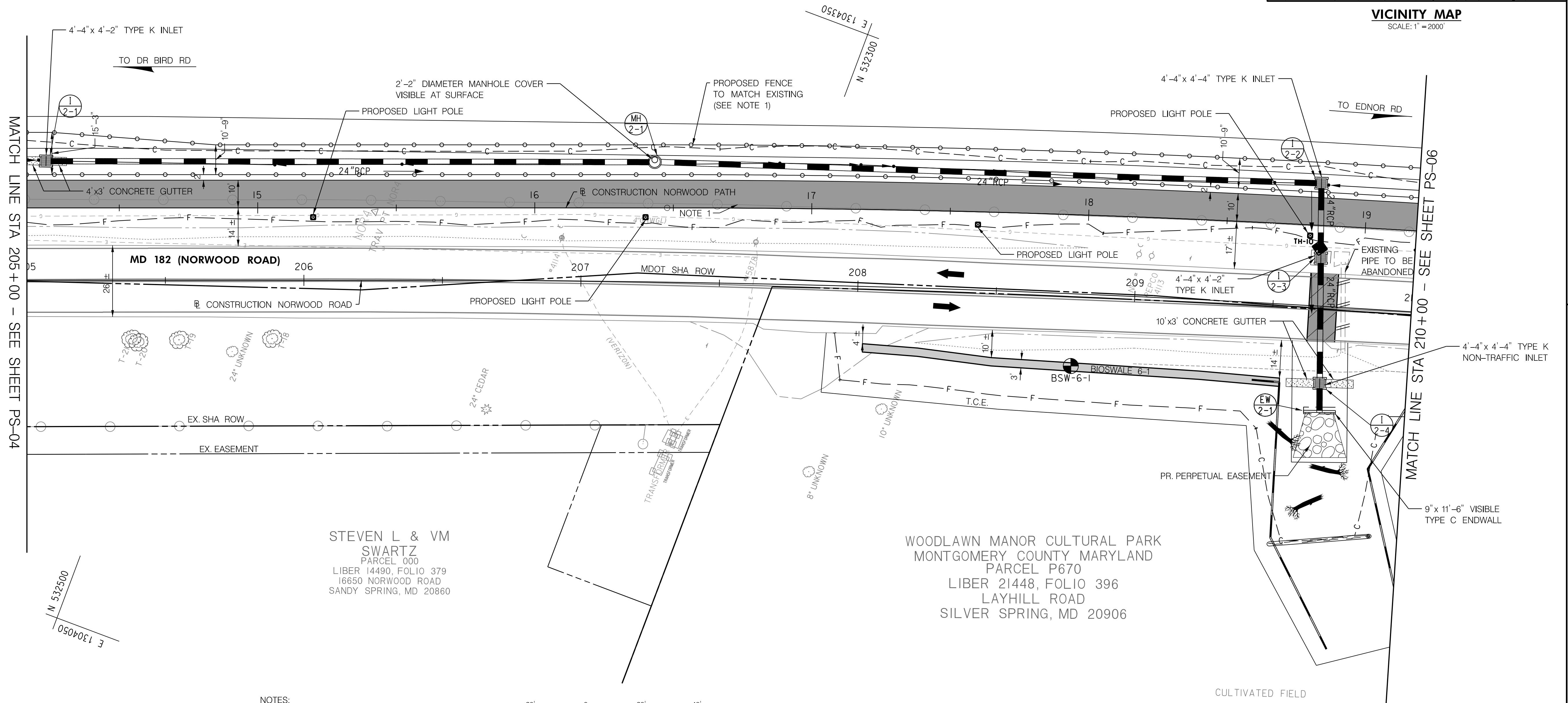
APPROVED  
Montgomery County  
Historic Preservation Commission  
*[Signature]*

TEST HOLE DATA					
NO.	UTILITY	NORTHING	EASTING	TOP UTILITY ELEVATION	SURFACE ELEVATION
TH-10	4" GAS	532,119.8023	1,304,335.2552	417.82	422.09



VICINITY MAP  
SCALE: 1" = 2000'

WOODLAWN MANOR CULTURAL PARK  
MONTGOMERY COUNTY  
PARCEL N170  
LIBER 5010, FOLIO 842  
16501 NORWOOD ROAD  
SANDY SPRING 20860



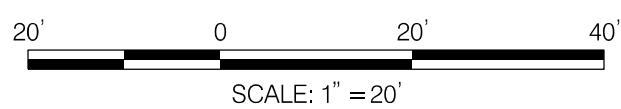
MATCH LINE STA 205+00 - SEE SHEET PS-04

MATCH LINE STA 210+00 - SEE SHEET PS-06

STEVEN L & VM  
SWARTZ  
PARCEL 000  
LIBER 14490, FOLIO 379  
16650 NORWOOD ROAD  
SANDY SPRING, MD 20860

WOODLAWN MANOR CULTURAL PARK  
MONTGOMERY COUNTY MARYLAND  
PARCEL P670  
LIBER 21448, FOLIO 396  
LAYHILL ROAD  
SILVER SPRING, MD 20906

- NOTES:
- REMOVE, SALVAGE, AND RELOCATE EXISTING FENCE. SEE FENCE RELOCATION SCHEDULE (THIS SHEET) AND SPECIAL PROVISION FOR FENCE RELOCATION.
  - FENCE IMPACTS OR RELOCATIONS WILL BE COORDINATED WITH PROPERTY OWNERS FOR SUBSEQUENT SUBMISSIONS.



LEGEND

	CONCRETE DRIVEWAY
	ASPHALT SHARED USE PATH
	FLEXIBLE SHARED USE PATH
	MILL AND OVERLAY
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	FULL DEPTH ASPHALT
	PAVEMENT /SIDEWALK REMOVAL

OWNER /ADDRESS:  
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MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
240-777-7263

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
ROCKVILLE, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section \_\_\_\_\_ Date \_\_\_\_\_

APPROVED

Chief, Division of Transportation Engineering \_\_\_\_\_ Date \_\_\_\_\_

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF TRANSPORTATION ENGINEERING  
HERITAGE TRIANGLE TRAIL PHASE 1  
DR. BIRD / NORWOOD ROAD  
SHARED USE PATH  
ROADWAY PLAN

SCALE 1" = 20' DATE MAY 2024

NO.	REVISION	DATE	BY

DESIGNED BY \_\_\_\_\_ DRAWN BY \_\_\_\_\_ CHECKED BY \_\_\_\_\_

DRAWING NO. \_\_\_\_\_ OF 07 SHEET NO. 23 OF 101

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PLOTTER: 3/14/2024  
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PLOTTER: 3/14/2024  
FILE: \\ad.rkk.com\rs\Cloud\Projects\2020\202097\_MCDOT\Transp\Task 27 - Heritage Triangle Trail\CADD\Plans\pHD-0005\_DrBird.dgn









Photo 1: Woodlawn property and proposed SUP location, looking southeast from intersection of Norwood Road and Excalibur Way (RK&K, March 2021)



Photo 2: Proposed SUP location, looking southeast from northern limits of Woodlawn property (RK&K, March 2021)





Photo 3: Proposed SUP location, looking southeast from just north of the Woodlawn driveway (RK&K, March 2021)



Photo 4: Existing culvert at north end of Woodlawn property (RK&K, March 2021)





Photo 5: Woodlawn driveway and northern sign, looking north from the driveway (RK&K, March 2021)



Photo 6: Woodlawn driveway and southern sign, looking north from the driveway (RK&K, March 2021)



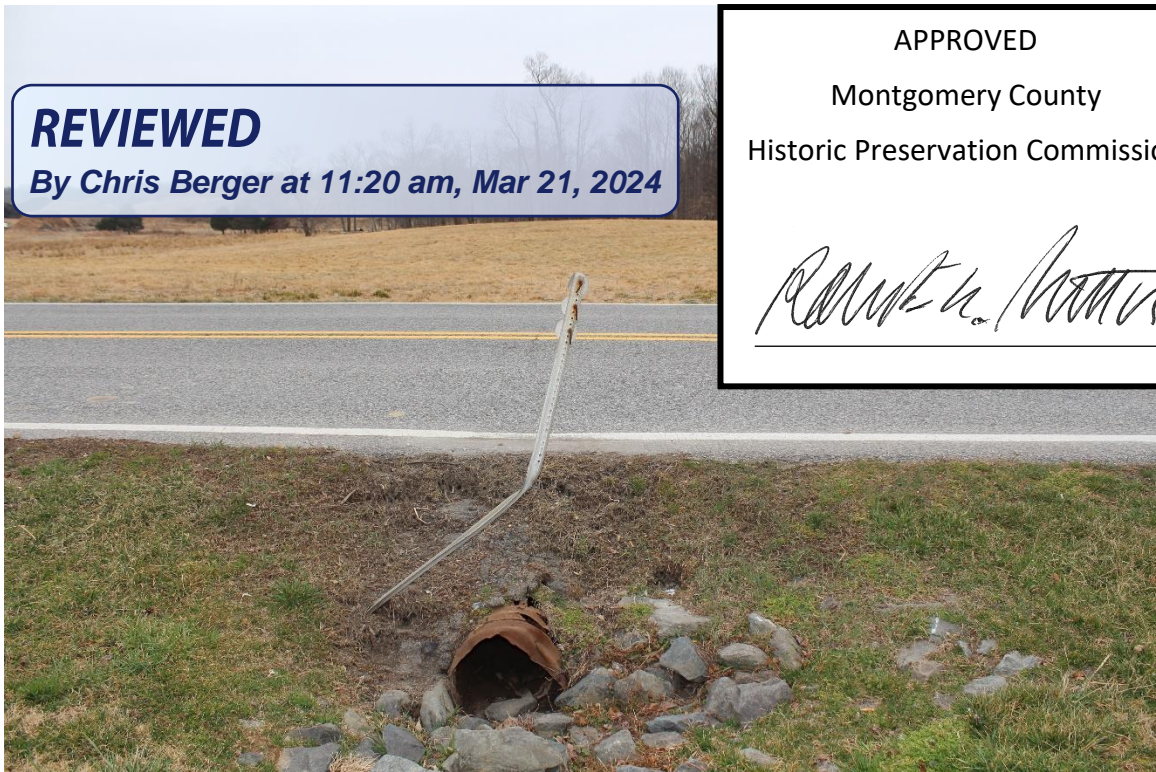


Photo 7: Existing culvert south of driveway, looking southwest to the Red Door Store Property (RK&K, March 2021)



Photo 8: Proposed SUP location, looking northwest from southern limits of Woodlawn property (RK&K, March 2021)



Photo 9: View of existing SUP south of Ednor Road, looking southeast from the Red Door Store property (RK&K, March 2021)

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED

Montgomery County

Historic Preservation Commission





Photo 1: Existing SUP along Norwood Road, looking southeast from north of Hennessy Terrace, similar to what will be installed along the Woodlawn property (Google Street View, November 2022)



Photo 2: Existing SUP along Norwood Road, looking north from just north of Hennessy Terrace at Norwood, similar to what will be installed along Woodlawn (Google Street View, November 2022)





Photo 3: Existing SUP along Norwood Road, looking north from Hennessy Terrace, similar to what will be installed along Woodlawn (Google Street View, November 2022)



Photo 4: Image of bioswale similar to what is proposed on the Red Door Store property (RKK Photo)



Photo 5: Image of bioswale similar to what is proposed on the Red Door Store property (RK&K photo)

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED  
Montgomery County  
Historic Preservation Commission  






Photo 1: Example of a K inlet with concrete gutter. Note that the gutter is not used for all K inlets and where it is used, it may be used on both sides.



Photo 2: Example

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED  
Montgomery County  
Historic Preservation Commission





Photo 3: Example manhole cover.



Photo 4: Example of type C endwall with outfall protection.

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED

Montgomery County

Historic Preservation Commission

Handwritten signature of Robert G. Adams

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED

Montgomery County

Historic Preservation Commission

LIGHT HARDWARE



MONTGOMERY  
DEPARTMENT OF TRANSPORTATION  
TRAFFIC ENGINEERING AND OPERATIONS

JUNE 2016

RESIDENTIAL, DIRECT BURIAL FIBERGLASS POLE  
ROUND, TAPERED, POST-TOP  
GRAY OR BLACK

1) DESCRIPTION

The residential, round, tapered, direct burial fiberglass pole shall be made of a fiberglass reinforced composite (fiberglass filament and color pigmented resin), with a polyurethane and UV inhibitor coating, with a natural finish. This fiberglass pole is intended for use on residential roadways, walkways, and tunnels throughout Montgomery County. Any manufacturer, distributor or vendor who submits a bid shall agree to comply with these specifications and the attached drawings.

2) DESIGN CRITERIA

2.1 AASHTO Standards

The residential, round, tapered, direct burial fiberglass pole shall meet the requirements of the American Association of State Highway and Transportation Officials (AASHTO) Standard, "Specification for Structural supports for Highway Signs, Luminaires and Traffic Signals," latest edition.

2.2 Wind Load

The residential, round, tapered, direct burial fiberglass pole shall be designed to resist (at yield strength of the material without permanent deflection or destruction) test loads equivalent to the calculated wind loads developed by the velocity pressures of an 80 MPH wind with a 30% gust factor. A minimum safety factor of 1.82 on the yield strength shall be maintained.

2.3 Effective Projected Area (EPA)

The residential, round, tapered, direct burial fiberglass pole shall be designed



**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED  
Montgomery County  
Historic Preservation Commission  


GHT HARDWARE

using the following assumption:

a) The streetlight luminaires shall be mounted at a minimum height of 7 feet above the level of the surrounding terrain.

et above the

b) One (24" x 36") traffic sign may be mounted with the sign's bottom edge 7 feet above the ground. (EPA of 6 Sq. Ft. +/-).

3) MATERIALS

a) The residential, round, tapered, direct burial fiberglass pole shall be constructed by a winding filament process with color pigmented polyester resin impregnated into the filaments. The filament winding shall be continuously applied with uniform tension.

b) The resin used will be color pigmented and shall be ultraviolet resistant. A highly weather resistant pigmented polyurethane coating shall be applied to the pole at a minimum thickness of 1.5 mils.

4) FINISH

The residential, round, tapered, direct burial fiberglass pole shall be of a natural finish for the entire length of the pole.

5) TENONS

The residential, round, tapered, direct burial fiberglass pole shall have a permanently bonded, hot-dipped galvanized steel or aluminum, 3 inch tenon.

6) HAND-HOLES

The residential, round, tapered, direct burial fiberglass pole shall have one 2 ½ inch x 5 inch hand-hole, with a non-metallic cover secured with a vandal-resistant, stainless steel screws.

7) POLE

7.1 Shaft

The residential, round, tapered, direct burial fiberglass pole shaft shall have a bottom pole diameter of 5.5 inches (+/- 0.1 inches), and a top pole diameter of 2.9 inches (+/- 0.1 inches)

7.2 Length

The residential, round, tapered, direct burial fiberglass pole shall have a nominal minimum luminaire mounting height of 12 feet and a maximum of 14 feet above the surrounding ground. The shaft shall be embedded a minimum of 3 feet in the ground.

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED

Montgomery County

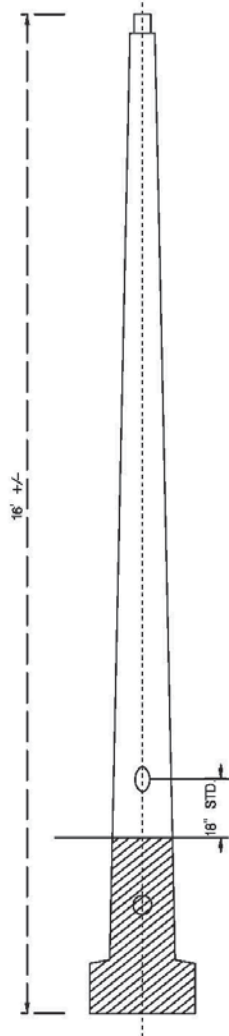
Historic Preservation Commission



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SPECIFICATIONS FOR STREETLIGHT HARDWARE

*SPECIFICATIONS FOR STREETLIGHT HARDWARE*



**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED  
Montgomery County  
Historic Preservation Commission

*Robert H. [Signature]*



MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION  
TRAFFIC ENGINEERING AND OPERATIONS

JUNE 2016

RESIDENTIAL, COLONIAL POST-TOP,  
LED OPTICS, TYPE III DISTRIBUTION, STYLE LUMINAIRE

1) PURPOSE

The purpose of these specifications is to prescribe the minimum requirements for the design, manufacture, fabrication, finishing and delivery of colonial post-top, LED optics, type III distribution, style luminaire. This luminaire is intended for use on or with the black fiberglass pole. These colonial post-tops, LED optics, type III distribution, style luminaires are intended for use along residential roadways, walkways, and tunnels throughout Montgomery County. Any manufacturer, distributor or vendor who submits a bid shall agree to comply with these specifications and attached drawings.

2) DESCRIPTION

The residential, colonial post-top, LED optics, type III distribution, style luminaire is made of a cast aluminum alloy housing.

Each streetlight luminaire shall include the following:

- a) Cast aluminum housing and hinged top canopy;
- b) 120 volt LED Driver;
- c) 10KV Surge Suppression Device built in;
- d) NEMA standard photoelectric control receptacle and NEMA multi-volt standard photocell;
- f) Acrylic or Polycarbonate resin refractor side panels (lens);
- h) All necessary hardware required for mounting on fiberglass poles, as specified.

3) DESIGN CRITERIA

3.1) AASHTO Standards

The luminaire shall meet the requirements of American Association of State Highway and Transportation Officials (AASHTO) Standard, "Specification for Structural supports for Highway Signs, Luminaires and Traffic Signals," latest edition.

3.2) Shape and Minimum Size

- a) The luminaire shall be of a trapezoidal shape. The minimum size for the luminaire shall 40.0 inches (sum of the length plus height), when viewed from the side.
- b) The luminaire shall be suitable for mounting on a 10KV Surge Suppression Device, NEMA standard photoelectric control receptacle and NEMA multi-volt standard photocell.

**REVIEWED**

By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED  
Montgomery County  
Historic Preservation Commission



The luminaire shall have a maximum estimated allowable EPA for the luminaire of  $0.7 \pm$  square feet.

3.4 Finish

The luminaire shall have a black polyester powder coat finish. During the finishing process, all critical openings shall be plugged to prevent contamination of the threads or reduction of other critical openings.

4) MATERIALS

4.1 Housing

The luminaire shall consist of a water tight housing fabricated from die-cast aluminum with a gasketed die-cast aluminum canopy. The canopy shall be hinged on one side and secured on the opposite side with a captive stainless steel screw. All castings used to fabricate the luminaire housing shall be clean and smooth with details defined and true to pattern. The housing shall be suitable to accommodate 120 volt LED Driver, 10KV Surge Suppression Device and NEMA standard photoelectric control receptacle and NEMA multi-volt standard photocell.

4.2 Driver & Surge Protection

The driver shall be mounted to facilitate easy removal for maintenance operations. The driver shall be equipped with a 10KV Surge Protection and suppression system. All electrical connections shall be polarized and of plug-in design. The driver shall be wired to receive 120 volt AC current. The driver shall reliably start and operate the lamp in ambient temperatures down to minus 30 degrees. The terminal block shall be capable of accepting up to a #6 AWG wire.

4.3 LED Color Temperature (CCT) and Rendering Index (CRI)

The Correlated Color Temperature (CCT) shall be a nominal Kelvin Temperature of  $3500K \pm 200K$  with a minimum Color Rendering Index (CRI) of 70.

4.4 Photoelectric Cell

The photocell receptacle shall be mounted for easy access and maintenance. The photocell shall be of the NEMA twist-lock type.

4.5 Side refractor panels

The luminaire shall be equipped with acrylic or polycarbonate resin refractor panels, with spring loaded retainer clips to hold refractor panels.

4.6 Slip Fitter

The slip fitter shall have a nominal inside diameter of 3.375 inches +/- 0.25 and shall be secured to the lamp post tenon with three or four evenly spaced set screws. The slip fitter shall accommodate a tenon 3.0 inches long.

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

APPROVED

Montgomery County

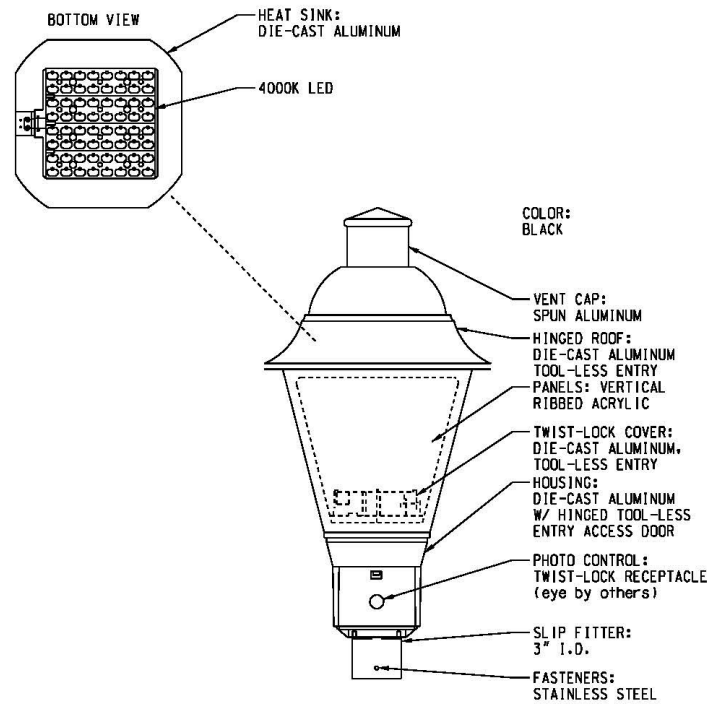
Historic Preservation Commission



SPECIFICATIONS FOR STREETLIGHT HARDWARE

IFB # 1063092

*SPECIFICATIONS FOR STREETLIGHT HARDWARE*



**REVIEWED**

By Chris Berger at 11:20 am, Mar 21, 2024

APPROVED

Montgomery County  
Historic Preservation Commission

November 15, 2022

Mr. Christopher Conklin, PE, Director  
Montgomery County Department of Transportation  
Executive Office Building  
101 Monroe Street, 10th Floor  
Rockville, Maryland 20850

Dear Mr. Conklin:

At its regularly scheduled meeting on November 3, 2022, the Montgomery County Planning Board voted to provide the following comments for Mandatory Referral No. 20220024 by a vote of 4:0 (Commissioner Piñero was absent), for the design of sidepath improvements on Doctor Bird Road and Norwood Road between Olney-Sandy Spring Road (MD 108) and Layhill Road/Ednor Road:

1. Construction plans must be submitted to the Montgomery Parks Department for review as part of the Park Construction Permit process to ensure that all work is performed in accordance with M-NCPPC standard details, specifications, and policies. No work on parkland may occur until an approved Park Construction Permit is issued for the project.
2. The final Memorandum of Understanding regarding access and maintenance must be agreed to and finalized between MCDOT and M-NCPPC before the issuance of a Park Construction Permit. MCDOT will be responsible for maintaining the asphalt sidepath facility and the proposed lighting.
3. MCDOT shall continue to coordinate with M-NCPPC on the design of the path and other elements including lighting, user safety elements, stormwater culvert and outfalls, stormwater bioswales, security cameras, double fence design including a mow strip, and natural resource protection and mitigation.
4. Mitigation for impacts to Park trees (with a 6" diameter at breast high or greater) damaged or removed, shall either be (1) replacement planting on parkland at a rate of one inch to one inch diameter or (2) a monetary per inch caliper basis at the rate of \$100/diameter inch, to be paid to Montgomery Parks prior to completion of construction.
5. Where the sidepath crosses driveways, maintain the sidepath's elevation across the driveway. Where possible, the sidepath should be pu
6. Given the open section cross section propo existing utility poles close to the road's ed perspective should be given to relocating

**REVIEWED**

By Chris Berger at 11:42 am, Mar 21, 2024

APPROVED

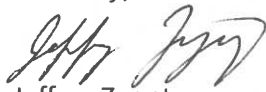
Montgomery County  
Historic Preservation Commission



Mr. Christopher Conklin, PE, Director  
November 15, 2022  
Page Two

The Planning Board appreciates the opportunity to review this project and continue coordinating with your staff on this and future projects. If you have questions, please contact Stephen Aldrich at (301) 495-4528 or [Stephen.Aldrich@MontgomeryPlanning.org](mailto:Stephen.Aldrich@MontgomeryPlanning.org).

Sincerely,



Jeffrey Zyontz  
Chair

cc: Stephen Aldrich, Planner IV, M-NCPPC  
Jason Sartori, Chief, M-NCPPC  
Dan Sheridan, MCDOT

Enclosure:

M-NCPPC Staff Mandatory Referral Staff Report

JZ:SA:aj

**REVIEWED**

By Chris Berger at 11:42 am, Mar 21, 2024

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Montgomery County  
Historic Preservation Commission



HERITAGE TRIANGLE TRAIL – PHASE 1  
DOCTOR BIRD ROAD/NORWOOD ROAD SIDEPATH  
MANDATORY REFERRAL NO. 2022024

Description

Montgomery County Department of Transportation (MCDOT) proposed to use along two sections of MD 182 – 1, at its intersection with Olney/Sandy Spring Road (MD 108) and starting at the Norwood Road intersection with Layhill Road north. The project is located in the Olney/Sandy Spring area and include construction of a ten-foot-wide sidepath to tie into the network at the project limits.

**REVIEWED**

**By Chris Berger at 11:20 am, Mar 21, 2024**

APPROVED

Montgomery County

Historic Preservation Commission



No. MR2022024

Completed: 10-06-2022

MCPB

Item No. 9

November 3, 2022

Montgomery County

Planning Board

2425 Reddie Drive,

Floor 14

Wheaton, MD 20902

## Planning Staff

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DS	Douglas Stephens, Principal Natural Resources Specialist, Park Planning & Stewardship, <a href="mailto:Douglas.Stephens@MontgomeryPlanning.org">Douglas.Stephens@MontgomeryPlanning.org</a> , 301-650-4367
JS	Jason Sartori, Chief, Countywide Planning & Policy Division, <a href="mailto:Jason.Sartori@MontgomeryPlanning.org">Jason.Sartori@MontgomeryPlanning.org</a> , 301-495-2172

### LOCATION

Doctor Bird Road starts at Sandy Spring Road (MD 108) a distance of 1,400 feet, and Norwood Road from Layhill Road/Ednor Road intersection running north a distance of 1,700 feet (adjacent to Woodlawn Manor Cultural Park)

**REVIEWED**

By Chris Berger at 11:20 am, Mar 21, 2024

### Summary:

Montgomery County to construct sidepath of Doctor Bird Road on Sandy Spring Road and a 1,700 foot-long section starting at the intersection of Layhill Road/Ednor

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### MASTER PLAN

1998 Sandy Spring Ashton Master Plan, 2018 Bicycle Master Plan, 2018 Master Plan of Highways and Transitways

### APPLICANT

Montgomery County Department of Transportation

### ACCEPTANCE DATE

August 22, 2022

### REVIEW BASIS

20-302 of the Land Use Article  
(Mandatory Referral)

- Forest Conservation exemption #42021057E was confirmed on February 27, 2015 as a linear county road project per Section 22A-5(e).
- Staff recommends approval of the Mandatory Referral with comments. .

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**REVIEWED** PLANS FINDINGS AND ANALYSIS  
*By Chris Berger at 11:20 am, Mar 21, 2024*

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## MANDATORY REFERRAL REVIEW BASIS

This proposal for the construction of pedestrian and bicycle improvements is required to undergo the Mandatory Referral review process under the Montgomery County Planning Department's Uniform Standards for Mandatory Referral Review. State law requires all federal, state, and local governments and public utilities to submit proposed projects for a Mandatory Referral review by the Commission. The law requires the Planning Board to review and approve the proposed location, character, grade and extent of any road, park, public way, or ground, public (including federal) building or structure, or public utility (whether publicly or privately owned) prior to the project being located, constructed, or authorized. This review is typically performed in context with the relevant master plans, guidelines, and policies in effect.

Planning Staff acknowledges that the implementation of master plan transportation recommendations is a challenge faced by the Applicant in developing design plans to convert desired master plan recommendations into engineering design drawings. The design process up to 30/35 percent design typically brings clarity with considerably more detail than considered during a master plan, and issues such as environmental impacts, historic impacts, and construction costs may introduce new factors that need to be weighed in developing a final design solution. It is the intention of the Board that the Mandatory Referral process aids in this process to develop an optimal or at least an improved design solution.

## SECTION 1: RECOMMENDATIONS SUMMARY

Planning Staff recommends approval of the Her

design plans (MR2022024) with the following co

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**By Chris Berger at 11:20 am, Mar 21, 2024**

1. Construction plans must be submitted to the Historic Preservation Commission (HPC) for review and approval. The HPC will review the plans for compliance with the National Historic Preservation Act (NHPA) and the Montgomery County Historic Preservation Ordinance (M-COHPA). The HPC will provide written comments on the plans within 30 days of receipt. The Applicant must address all HPC comments and resubmit the plans for approval. The HPC will not approve the plans until all comments are addressed.

standard details, specifications, and po  
approved Park Construction Permit is is

2. The final MOU regarding access and ma  
MCDOT and M-NCPPC before the issuan  
responsible for maintaining the asphalt sidepath facility and the proposed lighting.
3. MCDOT shall continue to coordinate with M-NCPPC on the design of the path and other elements including lighting, user safety elements, stormwater culvert and outfalls, stormwater bioswales, security cameras, double fence design including a mow strip, and natural resource protection and mitigation.
4. Mitigation for impacts to Park trees (with a 6" DBH or greater) damaged or removed, shall either be (1) replacement planting on parkland at a rate of one inch to one inch diameter or (2) a monetary per inch caliper basis at the rate of \$100/diameter inch, to be paid to Montgomery Parks prior to completion of construction.

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5. Where the sidepath crosses driveways, maintain the sidepath's elevation across the driveway. Where possible, the sidepath should be pulled away from the roadway at driveways.
6. Given the open section cross section proposed along Norwood Road and the proximity of existing utility poles close to the road's edge on the east side, consideration from a Vision Zero perspective should be given to relocating or undergrounding these utilities in the future.

## SECTION 2: PROJECT DESCRIPTION

### PROPOSED DESIGN PROJECT

The Montgomery County Department of Transportation (MCDOT) recently completed the 35% design phase of the Heritage Triangle Trail - Phase 1 Doctor Bird / Norwood Road Sidepath Project. The project provides a well-connected pedestrian and bicycle network sidepath along Doctor Bird Road (MD 182) and Norwood Road (MD 182). The project limits, shown below in Figure 1, complete two missing links in the bicycle network and provide a pedestrian and bicycling space where none exists today by adding the following:

- Approximately 1,400 feet of new shared-use path along MD 182 starting at MD 108 (Olney Sandy Spring Road) and traveling south towards Norwood Road.
- Approximately 1,700 feet of new shared-use path along MD 182 starting at the driveway of The Adventure Park at Sandy Spring and traveling south to Ednor Road.
- Lighting, stormwater management facilities, and any required signal/crosswalk improvements at the intersections of MD 108 / MD 182 and Ednor Road / MD 182.

Completing the missing links in the bicycle network

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By Chris Berger at 11:20 am, Mar 21, 2024

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### PEDESTRIAN AND BICYCLE SAFETY IMPACT

This project is anticipated to significantly improve safety in addition to improvements along Doctor Bird Road. The project completes two missing links in the larger bicycle network, providing a safe north/south connection for bicyclists and pedestrians in the Sandy Spring area.

In addition, the grading associated with the sidepath will provide adequate horizontal sight distance for motorists along a roadway where sight distance does not meet current AASHTO requirements. This will allow motorists to see pedestrians and bicyclists using the sidepath, improving safety for all parties.

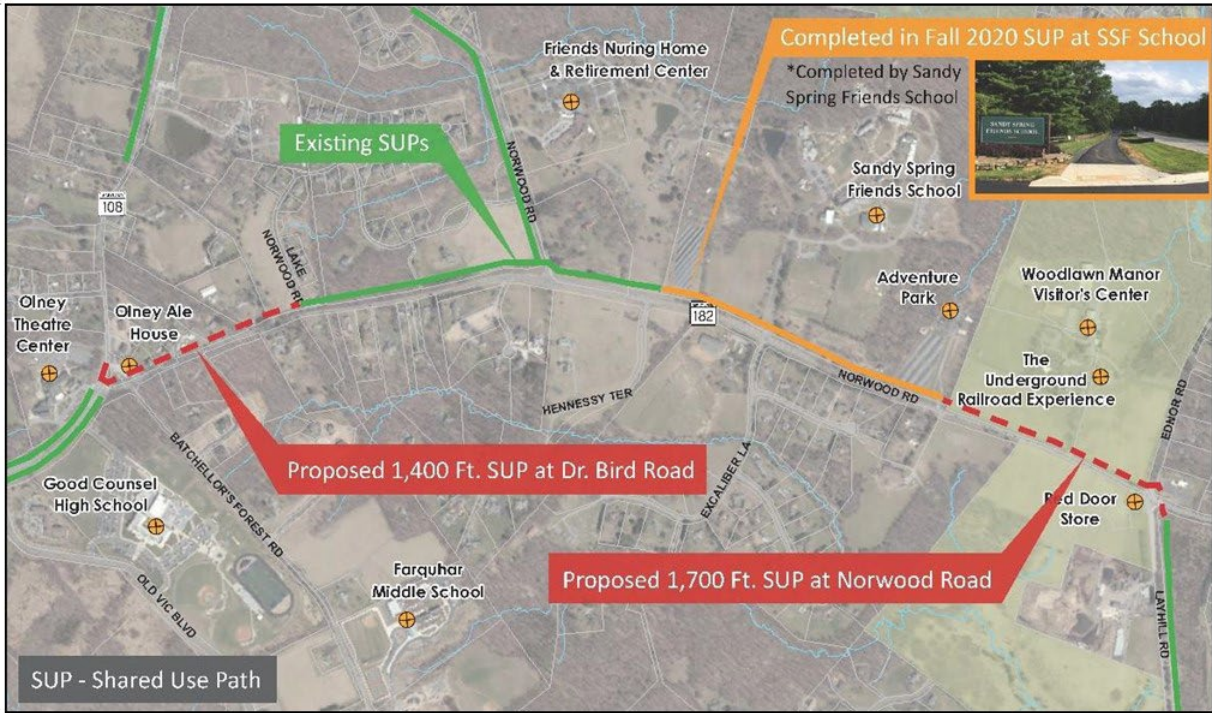


Figure 1 – Vicinity map

**Doctor Bird Road** - The existing roadway (see Figure 2) has two travel lanes with some shoulder towards the intersection of MD 182 / MD 108, resulting in bicyclists sharing the narrow lanes with vehicles. The proposed sidepath (see Figure 3) will provide an off-road separated bicycle facility to accommodate bicycle users who are not comfortable riding in a shared lane on a narrow roadway. It

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*[Signature]*

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By Chris Berger at 11:20 am, Mar 21, 2024

**Norwood Road** - The existing roadway (see Figure 4) has a narrow shoulder, which results in bicyclists sharing narrow lanes with vehicles. The proposed sidepath (see Figure 5) will provide an off-road separated bicycle facility to accommodate bicycle users who are not comfortable riding in a shared lane on a narrow roadway. It will also accommodate pedestrians through an area that currently does not have any separated pedestrian space. The path will connect to existing sidepaths at both termination points, improving the overall transportation network.





Figure 2 – Doctor Bird Road: Existing



Figure 3 – Doctor Bird Road: Proposed





Figure 4 – Norwood Road: Existing



Figure 5 – Norwood Road: Proposed



**TYPICAL SECTIONS**

**Doctor Bird Road** – Doctor Bird Road (MD 182) is classified as a Major Road in the County Plan of Highway and Transitways (MPOHT). The existing roadway is 12 feet wide and one westbound through lane with a 10-foot grass buffer. The immediate area has a rural character.

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 By Chris Berger at 11:20 am, Mar 21, 2024



Under the proposed condition, the existing roadway alignment is to be widened at the intersection with MD 108, to provide space for a buffer separate from the existing roadway. The proposed typical section will have a lane configuration of one eastbound 11-foot eastbound through lane, one 11-foot westbound through lane, a six-foot grass buffer, and an additional 10-foot sidepath adjacent to the westbound lane. Details are provided below:

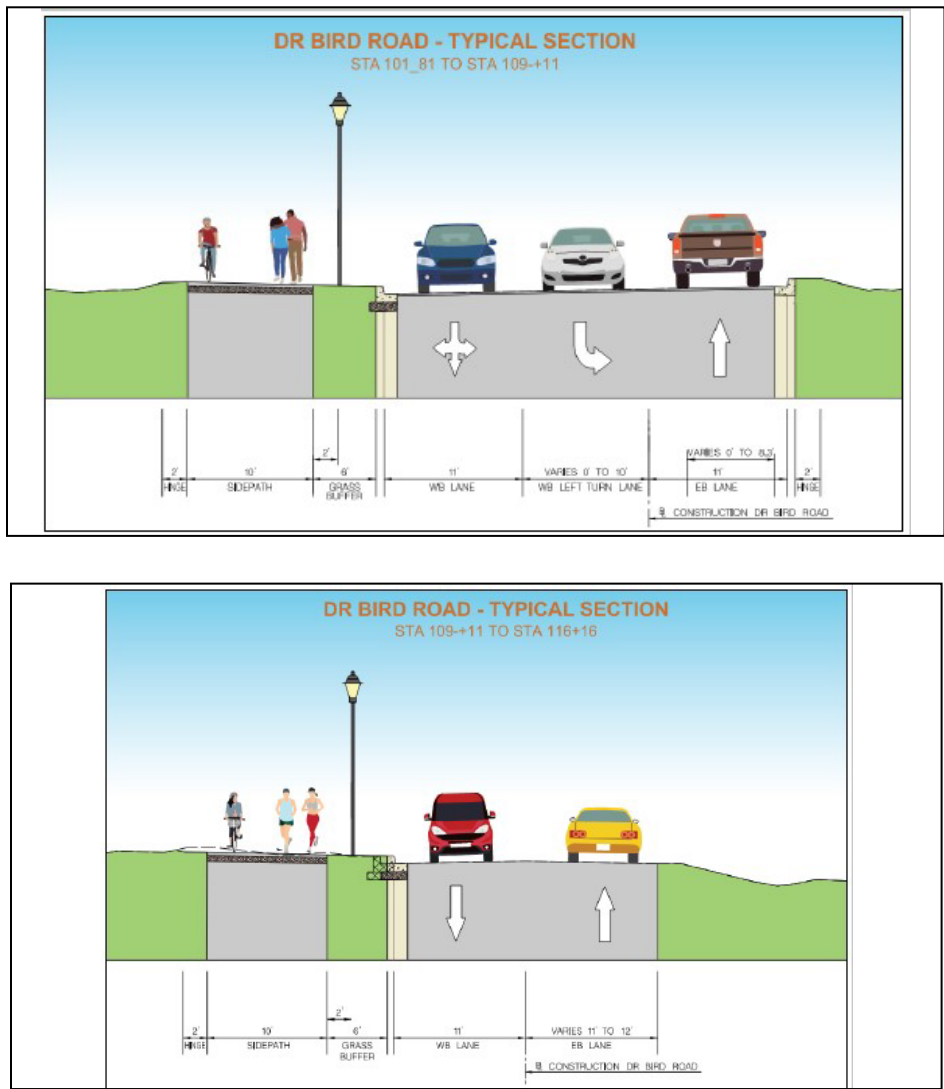


Figure 6 – Doctor Bird Road Proposed Typical Sections

**Norwood Road** – Norwood Road (MD 182) is classified as a two-lane Major Highway in the MPOHT. The existing typical section consists of one northbound lane (11' to 13') and one southbound lane (10' to 14') without shoulders or safety grading. The immediate area has a rural character.

Under the proposed condition, the existing roadway will not be impacted. The proposed typical section will add a variable width buffered grass shoulder and variable width bio-swale adjacent to the southbound lane as well as a variable width buffered grass shoulder, 10-foot sidepath with buffered grass shoulders on each side, and a variable width drainage ditch adjacent to the northbound lane.

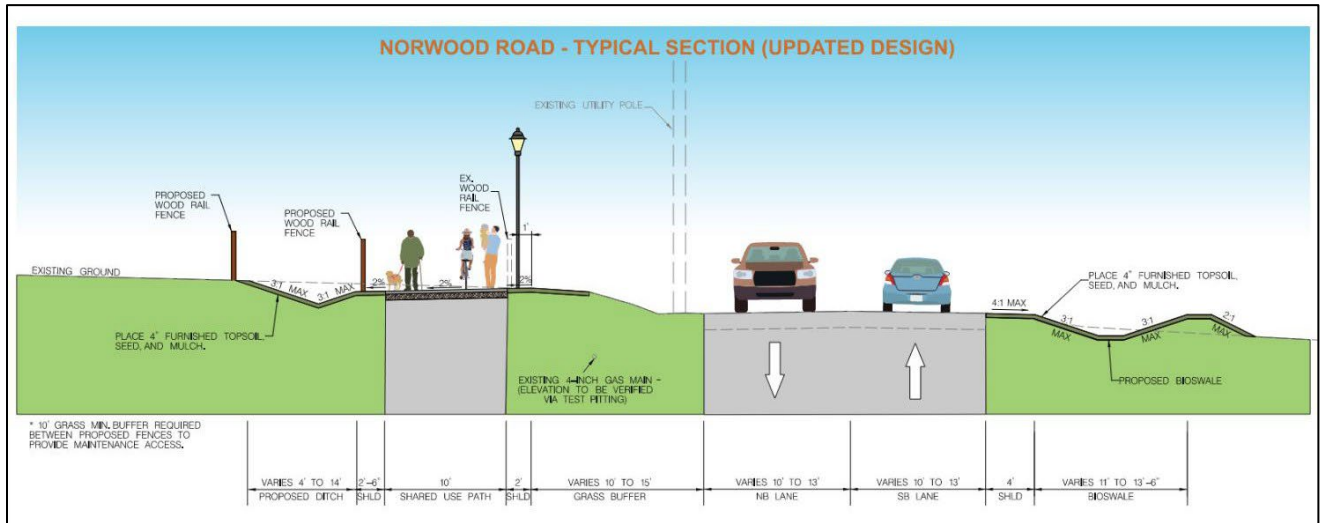


Figure 7 – Norwood Road Proposed Typical Section

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By Chris Berger at 11:20 am, Mar 21, 2024

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*[Signature]*

surrounding residential properties, utilities, and other high-value park properties adjacent to portions of the project. Stormwater management facilities for stormwater management are limited. Through the project, it was decided that stormwater management treatment facilities be located to the south side of Norwood Road. This limits the ability to treat the newly proposed path; however, treatment in these facilities has been maximized to the extent possible.

ever, due to the existence of the bioswale, it was decided to locate the bioswale to the south side of Norwood Road.

The project provides environmental site design (ESD) to the maximum extent practicable (MEP) within the project area through the provision of one (1) Micro-bioretention Facility along the Doctor Bird portion of the project, and four (4) Bioswale facilities location along the south side of Norwood Road. The one micro-bioretention facility will be built within MDOT SHA ROW and use MDOT SHA details, while the bioswales will be built on parks property and use Montgomery County Stormwater Management details. Though all available space within the project area is utilized to provide as much stormwater management treatment as possible, these 5 facilities still do not treat the full stormwater

management requirement. Therefore, MCDOT will be seeking a waiver for the treatment that cannot be provided.

The proposed work will not impact any stream buffers, floodplains, or wetlands. The project site drains to the Watershed of the Anacostia River (02140205) to the South and the Watershed of Rocky Gorge Dam (02-13-11-07) to the North.

Drainage design within the project area is limited to replacing existing roadside drainage amenities in-kind along Doctor Bird Road. Along Norwood Road, a drainage ditch is proposed and designed between the newly proposed shared-use path and the roadway. This ditch provides conveyance to existing cross-culverts, which will be replaced and upgraded under this task to meet current criteria. The outfalls of these cross culverts are being upgraded with a riprap design to provide a stable outfall for crossflows and the proposed bioswales, which outfall into these areas.

### PHASING SCHEDULE

This project represents Phase 1 of the Heritage Triangle Trail, a phased project that will connect four heritage tourism destinations—the Olney Theatre Center, Sandy Spring Museum, Underground Railroad Experience Trail Hike, and Woodlawn Manor Cultural Park—via a sidepath for bicyclists and pedestrians. Approximately one mile of trail exists along the eastern portion of Doctor Bird Road/Norwood Roads. A segment along the Sandy Spring Friends School’s Norwood Road frontage was recently completed in 2020. The current phase closes the gap between the Olney Theatre and the existing trail while establishing a new connection to Woodlawn Manor Cultural Park. An overview map is provided in Figure 8.

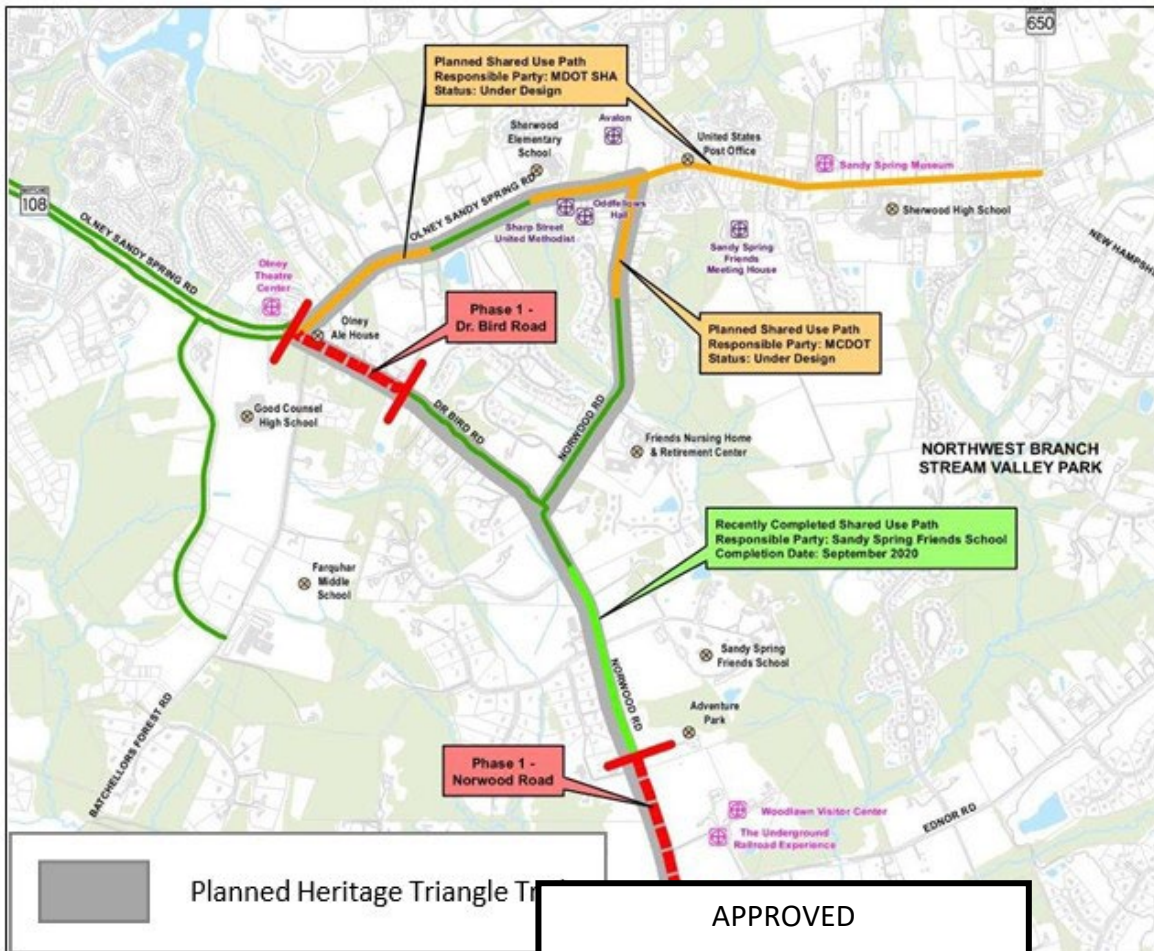
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By Chris Berger at 11:20 am, Mar 21, 2024



### PUBLIC LAND

The proposed sidepath will be constructed largely within the right-of-way; however, some parts of the path along Doctor Bird Road will be installed on private residential property. Additionally, portions of the path along Norwood Road will be installed on Montgomery Parks property. The project team has been coordinating with both groups of stakeholders regarding the project design and will prepare full right-of-way/ easement plats as part of the final design process. Temporary construction easements will be required on both the private residential properties and Montgomery Parks property.



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 By Chris Berger at 11:20 am, Mar 21, 2024

Figure 8 – Heritage

The design and construction of the project is currently funded by a grant of \$100,000 from the Maryland Heritage Areas Authority.

**LIGHTING**

The initial design plans for this project included pedestrian lighting along all sections of the newly installed sidepath. During the project’s stakeholder outreach, Parks identified several concerns with the installation of lighting along the Norwood Road section. Over the course of several months, MCDOT held several meetings with Parks and several associated stakeholders to review and resolve the lighting concerns identified. Following several discussions, all parties agreed that lighting fixtures would be acceptable along Norwood Road under the following established criteria:

- Lighting Fixtures – MCDOT agreed to select a colonial post lighting fixture that met Parks’ requirements for light transmission (dark sky compliant, shielding, low wattage LED bulbs)
- Lighting Maintenance – MCDOT agreed to maintain the lighting fixtures
- Lighting Continuity – MCDOT agreed to coordinate the future lighting of currently un-lit portions of the surrounding sidepaths that are a part of the proposed Heritage Triangle Trail system (Sandy Spring Friends School Path and the sidepath directly north of this newly constructed path)
- Trail Counters – MCDOT agreed to install bicycle/pedestrian trail counters along the sidepath

MCDOT reviewed several proposed fixtures with Parks and it was agreed that the MCDOT standard colonial post fixture would be acceptable. Regarding the lighting continuity issue, MCDOT and Parks agreed to the following conditions per an email from Montgomery Parks in Spring 2022:

*“The installation of lighting on parkland along Norwood Road is contingent upon lighting the adjacent path sections, North Path and Sandy Spring Friends School, concurrently. If lighting the adjacent path sections (North Path and Sandy Spring Friends School) is not to be included in HTT Phase 1, then conduit only will be approved for the parkland section at this time, with delayed approval for light fixtures when funds are procured for the adjacent path lighting to be installed concurrently. Parks’ goal is to provide lighting uniformity along the route that doesn’t lead trail users to unsafe dark zone.”*

MCDOT AGREES TO THESE CONDITIONS AND CONFIRMED THAT THE LIGHTING FOR THE CURRENTLY UN-LIT AREAS WILL BE INSTALLED TO THE APPROPRIATION OF FUNDS. ALL

SUBJECT TO

**REVIEWED**

By Chris Berger at 11:20 am, Mar 21, 2024

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APPROPRIATION. MEMORANDUM OF UNDERSTANDING  
 In addition to the lighting coordination described above, Montgomery Parks conducted extensive coordination regarding maintenance responsibilities for the new project sidepath, drainage facilities, and stormwater management. Below is an outline of the preliminary responsibilities to be completed by the team:

Montgomery division of the planned project, the team will complete the construction of the team:

- **Sidepath Facility – MCDOT:** MCDOT will be responsible for maintaining the asphalt sidepath facility. This will include repairs to the asphalt surface and any additional maintenance required.
- **Vegetation and Fences – Parks:** Montgomery Parks will maintain the lawns, trees, and fences/railings located along the path on Parks property.
- **Drainage Facility – Parks:** Montgomery Parks will maintain the drainage swales installed on Parks property. This will include mowing, weeding, trash removal, and any other associated maintenance for the swales. MCDOT shall be responsible for structural maintenance (inlets, pipes, endwalls, and headwalls).



- **Stormwater Management Facility – Parks:** Montgomery Parks will maintain the stormwater management facilities on Parks property. This includes managing the vegetation in the bioswale, removing trash/debris, and inspecting and maintaining outfalls to ensure they are not blocked. The Department of Environmental Permitting (DEP) will be responsible for structural maintenance per the existing agreement between Parks and DEP (Chapter 19, Article II).
- **Path Lighting – MCDOT:** Currently MCDOT and Montgomery Parks are coordinating regarding project lighting. MCDOT will maintain the lighting if standard Montgomery County light posts and fixtures are installed. MCDOT and the project team are currently researching and investigating ways to amend the contract and maintain lighting if timers are included.
- **Security System – Parks:** Montgomery Parks to own and maintain security cameras installed along the sidepath.

All parties reviewed and acknowledged that this outline should be considered a starting point for the detailed MOU that will be developed to document the final maintenance responsibilities. MCDOT will continue to coordinate with Montgomery Parks and all associated stakeholders to develop, review, and finalize a maintenance MOU for this project.

#### IMPACTS TO MONTGOMERY PARKS PROPERTIES

This project will have an impact on the Woodlawn Manor Park and MCDOT has been coordinating closely with M-NCPPC, Montgomery County Parks, and the Park Police to incorporate these stakeholders' concerns into its preliminary design. During early coordination with Parks, MCDOT redesigned the preliminary stormwater management plan to significantly limit the property impacts on Woodlawn Manor Park, there are no

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

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#### PUBLIC OUTREACH

A newsletter was mailed to residents within the vicinity of the project in February 2021. A virtual public meeting was held on February 25, 2021. A link to the project website is below:

<https://www.montgomerycountymd.gov/dot-dte/projects/heritageTTph1/index.html>

The project team is continuing to coordinate with all project stakeholders as the design is finalized.

### SECTION 3: 35% DESIGN PLANS FINDINGS AND ANALYSIS

The primary purpose of the proposed facility is to improve connectivity and safety for both pedestrians and bicyclists along Doctor Bird and Norwood Roads. Providing a ten-foot-wide sidepath facility parallel to Doctor Bird and Norwood Road in two distinct segments will fill in missing sidepath segments and improve the safety for users along the corridor and reduce the potential for crashes by minimizing conflict points with vehicles. The facility will improve the level of comfort for users of all ages and abilities by providing a dedicated facility with horizontal separation from the roadway in the form of a proposed grass buffer with a minimum width of five feet (primarily six feet wide or better) between the roadway and the proposed facility. The proposed facility will connect to existing shared use facilities on Olney Sandy Spring Road (MD 108) to the north and Layhill Road to the south.

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## DESIGN ELEMENTS - TRANSPORTATION

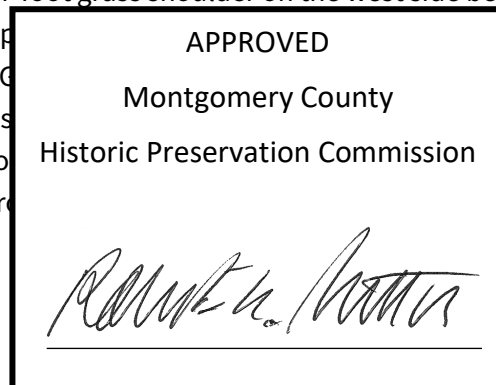
### ROADWAY DESIGN

Doctor Bird Road and Norwood are considered to be Country Connectors with a 40-mph target speed under the Planning Board-approved Complete Streets Design Guide (CSDG). The road currently has a 35-mph posted speed along its length.

### SHOULDER DESIGN

Doctor Bird Road and Norwood Road do not currently have paved shoulders, and short portions of the corridor are curbed (typically near new intersections and recently approved driveways). Along Norwood Road, the design will be adding a four-foot grass shoulder on the west side between the roadway and a drainage swale. If a shoulder is present on the east side of the roadway, the minimum shoulder width identified in the CSDG is the minimum shoulder width identified in the CSDG. The proximity of utility poles very close to the roadway is the concern along this corridor than the lack of shoulder conditions along the east side of the roadway. The design will be adding a four-foot grass shoulder on the west side between the roadway and a drainage swale. If a shoulder is present on the east side of the roadway, the minimum shoulder width identified in the CSDG is the minimum shoulder width identified in the CSDG. The proximity of utility poles very close to the roadway is the concern along this corridor than the lack of shoulder conditions along the east side of the roadway.

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024



### BUFFER DESIGN

The buffers proposed in this design are primarily six feet or wider for the majority of the project limits. This is consistent with the minimum buffer width identified in the CSDG.

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## ENVIRONMENTAL REVIEW

### ENVIRONMENTAL GUIDELINES

The project area does not contain any environmental buffers, streams, other sensitive features. The project area does contain 0.07 acres of forest. The project is within the Northwest Branch watershed

and partially within the Hawlings River Watershed, both of which have a Use Class IV<sup>1</sup> (recreational trout waters) designation.

The project proposes 0.07 acres of forest removal and has no impacts on a stream valley buffer, wetlands, or the 100-Year-Floodplain. The environmental impacts are necessary and unavoidable to achieve the design standards of creating the new pathway, and those impacts have been minimized to the greatest extent possible.

### FOREST CONSERVATION

The project is exempt from submission of a forest conservation plan. A forest conservation exemption (#42021057E) was granted under the provisions of Section 22A-5(e) as “a state or county highway project.” The exemption was confirmed on February 27, 2015.

While the project is exempt, the Applicant is still required under section 22A-9 of the County Code to:

- a) Minimize forest cutting, clearing, and loss of specimen trees to the extent possible while balancing other design, construction, and environmental standards. The constructing agency must make a reasonable effort to minimize the cutting or clearing of trees and other woody plants.
- b) If the forest to be cut or cleared for a county highway project equals or exceeds 20,000 square feet, the constructing agency must reforest a suitable area at the rate of one acre of reforestation for each acre of forest cleared.
- c) Mitigation for loss of specimen or champion trees. Mitigation amounts are based on the size and character of the tree.

The Applicant has minimized the limits of disturbance, minimizing the amount of forest clearing and impacts to large and specimen trees. However, the project still has impacts to forest and specimen trees. The Applicant has provided a plan to highlight forest loss/mitigation, tree save, and specimen tree mitigation.

### **Forest loss/mitigation**

Even with minimizing the limits of disturbance and altering some design aspects there are some necessary and unavoidable impacts to forest. The project proposes to remove 0.07 acres (~3,050 square feet) of forest which is below the forest clearing threshold of .46 acres (20,000 square feet) allowed under the exemption. Therefore, the Applicant is not required to provide reforestation.

**REVIEWED**

**By Chris Berger at 11:20 am, Mar 21, 2024**

<sup>1</sup> Environmental Guidelines: Guidelines for Environment 2021, page 58.





### **Tree Save**

The Applicant has submitted a tree save plan in conjunction with the Mandatory Referral process to show how the impacts have been minimized and what stress reduction methods are being used to further minimize impacts on trees. Three specimen trees are being removed by this project.

### **PARKLAND IMPACTS**

The proposed sidepath project along Norwood Road (MD182) will impact approximately 64,590 SF square feet of parkland at the frontage of Woodlawn Manor Cultural Park on the east side of Norwood Road. In addition, there are approximately 22,180 SF of proposed parkland impacts, at the Red Door Store Cultural Park on the west side of Norwood Road to accommodate stormwater management swales (Figure 9). There are no parkland impacts associated with the approximately 1,400-foot-long section along Doctor Bird Road starting at MD 108 (Olney Sandy Spring Road) and traveling south towards Norwood Road.

Woodlawn Manor Cultural Park is an 80-acre park dedicated to the preservation and interpretation of cultural resources. Woodlawn Manor is part of the National Park Service's National Underground Railroad Network to Freedom and is an active educational site serving thousands of children and families each year. The two main functions of Woodlawn Manor include the museum and Park Police operations:

- 1) The museum located in the \$3.5 million renovated barn project, historic house, and Underground Railroad Experience Trail functions to tell the story of the Underground Railroad, Quakers, enslaved and Free African Americans of Sandy Spring and Montgomery County.
- 2) Park Police Special Operations have their headquarters at the park, where they prepare for emergency operations as well as stable and pasture their horses in barns and within electrified fencing.

The park is located in the Northwest Branch watershed and in the North Olney Manor Management Region.

**REVIEWED**

*By Chris Berger at 11:20 am, Mar 21, 2024*

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# MCDOT Heritage Triangle Trail-Phase 1 at Woodlawn Manor Cultural Park



Map created on 7-21-22 by Resource Analysis,  
Park Planning and Stewardship Division, for  
Planning Purposes Only

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Figure 9 – Project Impacts to Parkland

Montgomery Parks supports the project’s goal to improve pedestrian and cyclist connectivity to these important cultural resources and is working with MCDOT to reduce the impacts to parkland where feasible and provide appropriate mitigation for unavoidable impacts. The 10-foot-wide sidepath connector proposed on the east side of Norwood Road along Woodlawn Manor Cultural Park, includes a variable 10- to 15-foot-wide grass buffer between the path and the roadway (see Figure 7). The pasture will be reduced by 22 to 30 feet along the approximately 1,600-foot length on park property.

## LIGHTING

Montgomery Parks supports pedestrian lighting (as described in Section 2) for the Heritage Triangle Trail as long as all sections of the path are lit to prevent a discontinuity of lighting along the trail. MCDOT will be responsible for the maintenance of the lighting posts and fixtures. Since there is currently no existing path lighting on the sections adjacent to park property Parks is requiring that conduit only be installed until lighting for the adjacent trail sections is funded. Additional coordination on lighting and minimizing impacts from lighting in this section will be required and included in the Memorandum of Understanding (MOU) and Park Construction Permit.

## CULTURAL RESOURCES

Due to the historical context of this site, MCDOT has been in coordination with the Maryland Historical Trust, the Maryland State Historic Preservation Office (MHT) and Parks’ Cultural Resources Staff. An archeological investigation was performed along the length of the project. Along the Norwood Road section, five shovel test pits identified a very low-density field scatter of seven artifacts, broadly dating from the late 18th to early 20th century. MHT determined that archeological investigations will be required and that the project should coordinate with MHT and the State Historic Preservation Office (SHPO) staff and is pursuing Historic Area Work Permit (HAWP) application. The MHT advised that “MCDOT should exercise caution to ensure that project construction activities, equipment, and materials do not impact the Woodlawn Manor property outside the Area of Potential Effect (APE). Any findings uncovered during construction shall be reported to the MHT and the SHPO for review and approval.”

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024



## NATURAL RESOURCES

The proposed trail runs across the road frontage and the Woodlawn site and will impact landscape plantings including trees and shrubs. Nine red cedars (not specimen trees) on parkland at the intersection of Ednor Road and Norwood Road will be removed as part of the project (see Figure 10). At the entrance to Woodlawn Manor two large shrubs and one bur oak (not a specimen tree) will need to be removed to improve sightlines (see Figure 11). Parks and MCDOT will work collaboratively to fulfill Parks tree mitigation requirements through either the planting of new trees, mitigation fee payment, or other natural resource mitigation. Parks Urban Forester will provide guidance during the



Park Construction Permit review to avoid and minimize impacts to the critical root zones to the large oaks lining the entrance to the park.

On the west side of Norwood Road, is the Red Door Store Cultural Park, an approximately 80-acre park which houses the Red Door Store, a distinctive historical example of a mid-19th century rural crossroads store. In 2018, architectural, engineering and market studies were undertaken to better understand the level of effort required to rehabilitate and activate the structure. The Red Door Store Cultural Park contains an approximately 58-acre meadow management site. Meadow areas at the site are primarily composed of old field non-native species, with some native grasses and wildflowers



*Figure 10 - Red Cedars to be removed adjacent to Woodlawn Manor Cultural Park*

present. Active management currently includes annual mowing to keep the site open and dominated by herbaceous species. It is currently identified as an important area for open habitat species within the Montgomery Parks system. The meadow and adjacent forested area slope down to Batchellor’s Run, a restored tributary of Northwest Branch.

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

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Montgomery County  
Historic Preservation Commission



Figure 11 – Entrance to Woodlawn Manor Cultural Park

### STORMWATER MANAGEMENT

Four bioswales are proposed along the west side of Norwood Road within the Red Door Store Cultural Park to provide required stormwater management for the project. Parks has committed to maintaining the stormwater facilities built by MCDOT on parkland per the agreement with the Montgomery County Department of Environment under Norwood Rd which currently do not function to the meadow area on the west side of stable and environmentally sensitive outfall stream meadow.

**REVIEWED**  
By Chris Berger at 11:20 am, Mar 21, 2024

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Historic Preservation Commission  
*Robert A. [Signature]*

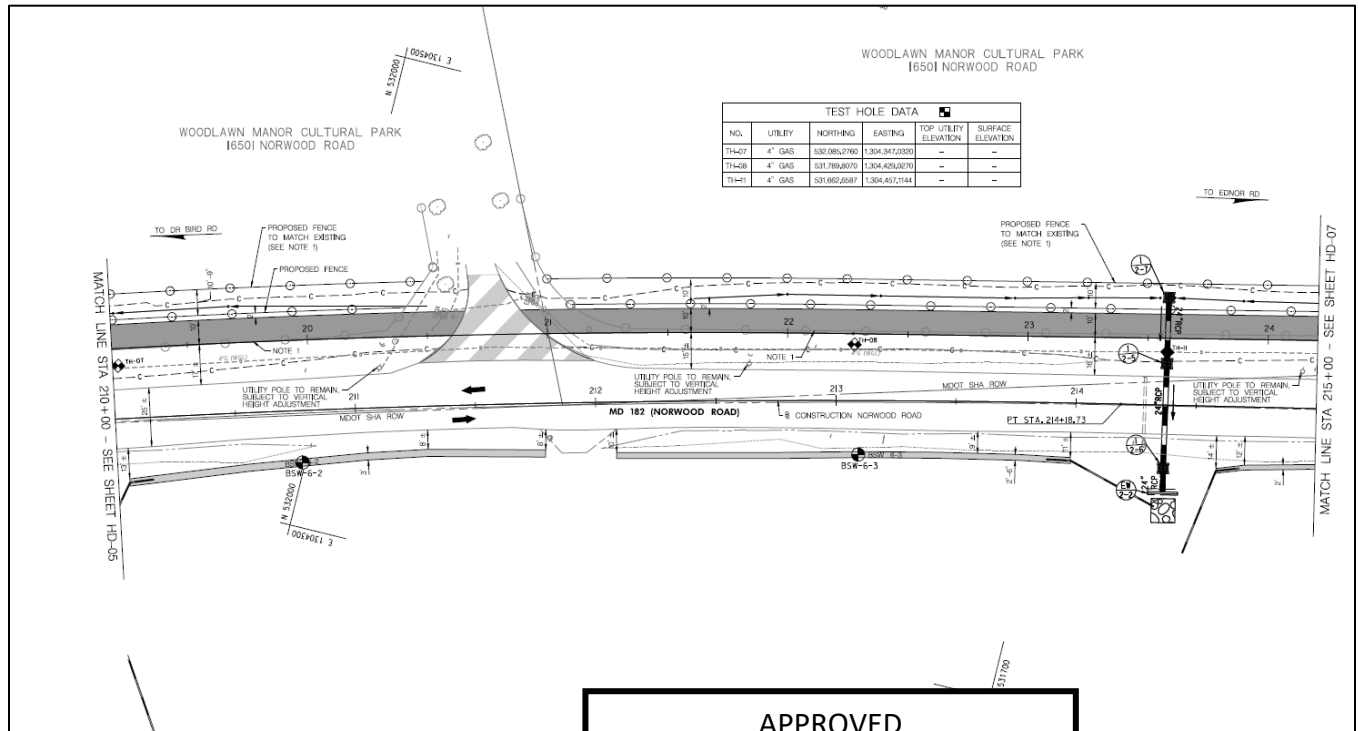
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### SAFETY AND SECURITY

Montgomery Parks looks forward to the improvement of Woodlawn Manor Cultural Park, however the new path does present new challenges for safety and security. Figure 12 shows the proposed shared-use path plan view at the entrance to Woodlawn Manor. Parks Staff have noted concerns with the vehicular traffic utilizing this entrance as a cut-through and additional measures should be taken to improve the safety of the path crossing including better signage to reduce thru traffic, a marked crossing and detectable warning strips for pedestrian and cyclists, and other appropriate measures identified during detailed design. The existing signage at the entrance to Woodlawn will need to be relocated and replaced at the direction of Parks Staff.

Along the length of the east side of the path, at the request of Park Police, a double fence separated by a 10-foot width is proposed to separate the equestrian pasture from the path for the safety of both the

horses and path users. The fence must match the existing fence in design and materials. In addition, due to increased access and activity that the path will provide around the Park Police headquarters and equestrian facilities MCDOT will also provide four security cameras along the frontage of the Woodlawn Property. Finally, MCDOT will provide a trail counter along this section of the path to Parks' specification to allow for user data tracking and analysis.



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*[Signature]*

Parks and MCDOT have agreed that Parks will m... will be granted. A Memorandum of Understandi... agreements for each agency.

ements  
tenance

MCDOT will be required to obtain a Park Construction Permit from Montgomery County Department of Parks prior to commencement of any construction activities on parkland. Plans submitted for Park Construction Permit review must include existing topography, utilities, and identify and locate all trees (with size and species) larger than 6" diameter at breast height (DBH) and greater within 100 feet of the proposed limit of disturbance on park property. Mitigation for impacts to Park trees (with a 6" DBH or greater) damaged or removed, shall either be (1) replacement planting on parkland at a rate of one inch to one inch diameter or (2) a monetary per inch caliper basis at the rate of \$100/diameter inch, to be paid to Montgomery Parks prior to completion of construction. Tree impacts will be determined by an M-NCPPC forester prior to construction based on the Final Design. During Park



Construction Permit Review, Parks Staff will work with MCDOT to minimize impacts to parkland to the greatest extent possible and avoid all critical resources identified.

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## HISTORIC PRESERVATION

MCDOT's consultant completed detailed cultural resources analysis and submitted a Phase 1 Cultural Resources Report to the Maryland Historic Trust (MHT) in June 2021. MHT responded in August 2021 and determined that no additional Phase 2 archaeological investigations will be required and that the project will have no adverse effect on historic properties. The Applicant is continuing to coordinate with MHT and Montgomery Planning's Historic Preservation (HP) staff as the design progresses forward. Per coordination with HP staff, the Applicant has initiated the submission of draft Historic Area Work Permits (HAWP) via the online portal for the Red Door Store and Woodlawn properties.

The Doctor Bird/Norwood Road sidepath project is adjacent to several sites on the Master Plan for Historic Preservation including Woodlawn (28/14), the Holland Store and House/Red Door Store (23-119), and the Doctor Bird House (23/121). The project limits of disturbance will not encroach on the Doctor Bird House master plan site; but impacts both the Woodlawn and the Holland Store and House/Red Door Store sites, so a HAWP is required before the work begins.

Historic Preservation Staff supports the project and is not concerned about the potential impacts the work will have on the designated sites. The sidepath, lighting, and relocated fencing adjacent to Woodlawn will not significantly impact the historic character of the site's environmental setting. Additionally, HP Staff finds the re-grading necessary for the bioswale and culverts installed on the Holland Store and House/Red Door Store site will only minimally impact the visual character of the site.

A Phase 1 archaeological survey was completed under the MHT's oversight. MHT determined that no further archaeology in the project area was warranted.

Historic Preservation Staff encourages the Applicant to apply for a HAWP around the 70% design phase or at their earliest convenience.

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*By Chris Berger at 11:20 am, Mar 21, 2024*

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## MASTER PLAN CONFORMANCE

The Heritage Triangle Trail – Phase 1 Sidepath is subject to three master plans: the 1998 Ashton Sandy Spring Master Plan, the 2018 Bicycle Master Plan and the 2018 Master Plan of Highways and Transitways.

The 2018 Bicycle Master Plan includes the following recommendation for MacArthur Boulevard:

BIKEWAY	FROM	TO	FACILITY TYPE	BIKEWAY TYPE
Doctor Bird Road (MD 182)	Olney Sandy Spring Road (MD 108)	Norwood Road	Separated Bikeway	Sidepath
Norwood Road (MD 182)	Doctor Bird Road (MD 182)	Layhill Road/Ednor Road	Separated Bikeway	Sidepath

The 2018 Master Plan of Highways and Transitways and the 1998 Ashton Sandy Spring Master Plan classifies Doctor Bird Road/Norwood Road (MD 182) from Olney Sandy Spring Road to Layhill Road/Ednor Road as a two-lane major highway with a master plan right-of-way of 120 feet. Per the Complete Streets Design Guide, these roads would

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By Chris Berger at 11:20 am, Mar 21, 2024

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A newsletter was mailed to residents within the vic meeting was held on February 25, 2021. A link to th

<https://www.montgomerycountymd.gov/dot-dte/projects/heritageTTph1/index.html>

The project team is continuing to coordinate with all project stakeholders as the design is finalized.

## SECTION 5: CONCLUSION

Based on information provided by the Applicant, Planning Staff recommends approval of the Mandatory Referral with comments listed on pages 3 and 4 of this report to be transmitted to the Montgomery County Department of Transportation. The Applicant is requested to reply in writing to the Planning Board, addressing all comments.

## ATTACHMENTS

Attachment A: 35 Percent Design Plans