

GERMANTOWN HISTORIC DISTRICT

MD. MIA Parking Lots/Parking Garage

#19/13

Parking garage on hold for MARC

by Karen Loudon
Staff Writer

Plans for a parking garage at the Germantown MARC train station are on hold as transportation officials look at an alternative to accommodate commuters.

The station is located in Germantown's historic district off Route 118 near Wisteria Drive. The station has two parking lots on the east side of Route 118, one on each side of the train tracks. The existing lots can hold a total of 348 vehicles.

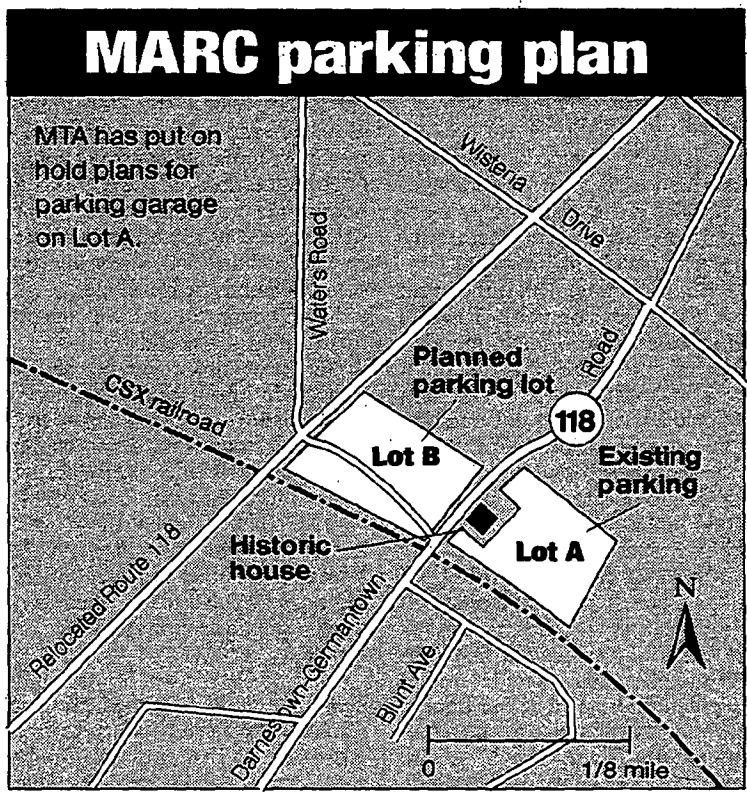
Last summer, the Maryland Mass Transit Administration (MTA) suspended preliminary engineering and design plans for a parking garage at the Maryland Rail Commuter station.

MTA is moving forward with a proposal approved by the Montgomery County Planning Board last year for a 340-space lot on the west side of Route 118, opposite the station.

Anthony Brown, MTA spokesman, said the cost of the lot is \$1.5 million, and is scheduled to be completed in the summer of 1997. Brown said MTA plans to build a surface lot at the 3.6-acre site and will re-evaluate the need for more parking.

John Matthias, planning coordinator for the county's transportation planning division, said MTA will look at how the lot is used by commuters to determine if additional space is warranted.

Nancy Philips, MTA spokeswoman, said the Germantown station draws the most riders in



the county, with 425 to 450 passengers daily. Philips said the parking lot is 80 to 90 percent full each day, and plans for another lot are in response to Germantown's steady increase in population.

"It's one of the fastest growing communities," she said.

MTA's move to put garage plans on hiatus came after a decision last July that had the county Planning Board and the state at odds on the location and design of the garage.

Moreover, Matthias said the garage is not an immediate need, but could be in the future as Germantown continues to grow.

"It's an issue that didn't have to be resolved at the time," Matthias said.

MTA favored building a five-level, 780-space garage on the existing lot on the east side of Route 118, next to the historic Pumphrey-Mateney house.

The county suggested building on the west side of Route

118, and recommended a more modest structure of a two- or three-story complex with 768 spaces. The Planning Board also suggested the surface lot be suspended in place of the garage.

Included in the debate was which location would have the least impact on the surrounding community.

In a planning staff report last year, Matthias stated that the MTA proposed east-side site would conflict with the Germantown Master Plan that calls for a structure that is visually and functionally compatible with the neighborhood.

Brown said when the parking garage issue is revisited in the future, officials will consider existing studies while updating information based on the "needs of the community and development of the area."

Matthias said he hopes MTA will "take in consideration the issues the community and our staff have raised."

Mass tree clearing irks local residents

OLNEY GAZETTE

by Susan Singer-Bart
Staff Writer

4/10/86

Throughout the winter, Alice Ortuzar looked out her window and watched in horror as workers cleared the trees and shrubs along New Hampshire Avenue

north of Route 108.

She spoke to friends and neighbors and while no one liked to see the trees and vegetation cut, no one knew what to do about it.

She spoke to the foresters from the De-

partment of Natural Resources and the Baltimore Gas & Electric Co. who were on the job. The trees had to go, she was told.

But she didn't like their answer.

Then last week, Ortuzar called her state representative, Sen. Christopher McCabe (R-14, East County, Howard County) of Elliott City and Del. Pat Faulkner (R-14A, Olney) of Silver Spring. They arranged a meeting last week on New Hampshire Avenue between Ortuzar and representatives of the Department of Natural Resources, BG&E and Russell Dominique.

Dominique is acting president of the Ashton-Sandy Spring Civic Association and a member of the Sandy Spring-Ashton Master Plan Citizens Advisory Committee.

The new master plan declares the importance of the rural character of the area's roads, an element

the trees along New Hampshire Avenue provided, said Dominique.

"All of New Hampshire Avenue is important to the character of Sandy Spring/Ashton," reads the master plan.

The plan recommends preserving the character of New Hampshire Avenue by keeping the current road classification and the number of through-lanes. It offers strategies for preserving and enhancing the road's character.

Since February, from Route 108 north toward Brinklow, contractors have been cutting down the trees that form a canopy over the road and give New Hampshire Avenue its rural character.

"Utilities need to provide electrical service," said Jeff Horan, regional forester with the Maryland Department of Natural Resources. "BG&E hires foresters to figure out the best way to have electricity and preserve resources."

Tree trimming, he said, opens the trees to disease. After several trimmings, trees become unhealthy and need to be cut down before the branches start falling

down.

In the last few years, BG&E has received numerous complaints from customers about power outages, said Kurt Merkel, BG&E's district forester. The utility is responding to customer complaints in doing tree work along New Hampshire Avenue, he said.

"Last time we trimmed through here, we got a lot of complaints on the way it looked," said Merkel. "We're in a no-win situation."

The quality of the branches along New Hampshire Avenue was low, said Rodney Riffle, a forest ranger with the State Department of Natural Resources.

Riffle said BG&E has a state permit for the tree work and the permission of all homeowners whose trees are being cut.

"Usually, there is not a great deal of consensus — people either love or hate trees," said Horan.

He did not know about the new master plan or the general feeling of community concern for the trees.

"We need to come to some agreement about proceeding," said

Horan.

He asked BG&E to stop cutting down trees until all parties can meet with Margaret Rifkin, the county's lead planner for the Sandy Spring-Ashton Master Plan.

Ortuzar asked that someone from the Sierra Club be included in the meeting to explain a better way of controlling tree growth along roadways.

"In areas they're doing a master plan, we need to be part of the process," said Horan.

Rifkin said the Planning Board routinely reviews state and county road projects. State Department of Natural Resources projects do not currently get reported to the Planning Board for review.

"We need to set up a system where we need to be contacted when there's work," said Rifkin.

Horan and Rifkin agree the rural character of New Hampshire Avenue can be preserved if slow growing indigenous plants and shrubs, such as dogwoods and redbuds, are planted along the edge of the road.

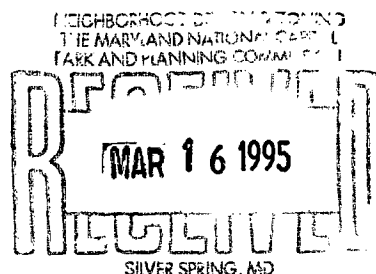
Horan said he hopes to meet with Rifkin this week.

14

Raymond J. Howar
19215 Blunt ave.
Germantown, Maryland 20874

FACSIMILE TRANSMISSION

DATE: 03-16-95
TEL #: 301-217-2170 2118
TO: MR. EDWARD DANIEL
FROM: RAYMOND HOWAR
SUBJECT: GERMANTOWN TRAIN STATION
PAGES: () INCLUDING COVER PAGE



Dear Mr. Daniel;

Many thanks for the letter notifying me of the meeting concerning the proposed changes at the train station. It has been difficult to determine which governmental agency to correspond with as the field changes so quickly. All of the proposed building has caused great concern and will continue to be involved in the changes that are taking place. I will address this letter to you and forward copies to related persons and agencies.

After the Action Committee meeting it was apparent that there are several outstanding issue that need to be resolved and the impact of these changes on the Historic District and residential neighborhood.

- 1.) Acquisition of Water Street property and subsequent building of parking lot
- 2.) Building of commuter shelter in and around station platform
- 3.) Proposed acquisition of additional property lot P-209 by MTA from CSX and future use as a parking lot
- 4.) Train Station attendant moving to the Station in front of my house

The above described issues are of great concern to me, as my house sits directly across from the train station. My position has not changed as it relates to the fundamental issue

of continued commercialization of the Germantown Historical District and the enclosed residential neighborhood by the agencies involved . I am still opposed to any additional parking lot allocated for the District and my reason are as follows;

- 1.) The existing parking lots are not to capacity and have never reached capacity.
- 2.) Incorrect grow analysis for the Germantown area and percentage use of MARC by local area residents.
- 3.) Inadequate landscaping and unacceptable lighting condition on the existing structures and surrounding area.
- 4.) Increased vandalism and crime associated with the present parking lots
- 5.) Trash and other debris that litter the area

I will continue to oppose the changes that you and MTA are proposing until your office and State MTA address the previous issues that I have raised over the last (5) years. Until a comprehensive landscaping plan and lighting plan is adopted and implemented, my support for any further increase in parking, use and any other related items will be minimal. The additional land purchase proposed by MTA and subsequent increase in parking availability further exacerbates my concern that there will never be enough parking for MTA or MC transit until the entire area is paved.

I remind you again, that my house sits directly across the street from the station, as such our residential neighborhood needs to be protected. With this in mind, below are some suggestion that need to be seriously considered. At a minimum, planting of additional trees and installation of timers in and around the train platform and station will need to be a priority. Until a comprehensive landscaping and lighting plan is adopted and implemented, I will oppose the proposed changes and express my views where I can.

It would appear it is inevitable that these changes to the train station are going to occure. With this in mind, my minimal recommendations would be as follows.

PARKING LOT STRUCTURE:

There will need to be adequate, if not excessive planting of large trees to properly screen the new asphalt. Lighting should be minimal and setup on timers. The parking lot itself should incorporate green areas (larger that the existing parking lot) in areas that will be facing the residential neighborhood. A dirt mound would be an alternative in screening the parking lot.

TRAIN STATION PLATFORM & PROPOSED STRUCTURE:

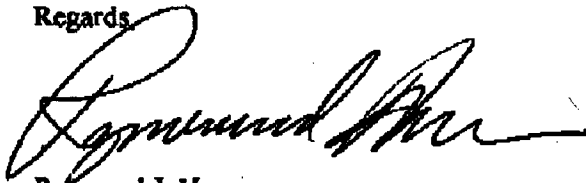
Any additional structure would be opposed until adequate lighting and landscape screening be incorporated into any design. I would recommend that you and MTA abandon the new building concept and look at adding onto the existing Train Station or building covers for the platforms.

ADDITIONAL RECOMMENDATIONS:

- * Train Station attendant stay in the Bank Building. If he is moved to the train station additional landscaping (trees) be planted.
- * Removal or relocation of payphone
- * Reduction of lighting in and around platform (specifically, lighting baffles installed in vapor lights on Train Station door entrances).
- * Installation of lighting timers on platform lights

I hope that this letter clarifies my position on these issues and I look forward in working with all the agencies involved.

Regards



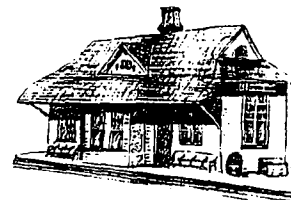
Raymond J. Howar

cc: Jean Roessier
Gwen Marcus
AJ Forst
MTA
CSX
Susan Sodenberg



Germantown Bank

*The Germantown Historical Society
P.O. Box 475
Germantown, M.D. 20875*



Germantown Train Station

Oct., 3, 1995

William Hussman, Chairman
M-NCPPC
8787 Georgia Ave.
Silver Spring, MD 20910-3760

RE: Parking Garage Site
Selection Study for the
Germantown MARC Station

Dear Mr. Hussman,

The Germantown Historical Society is opposed to the construction of a parking garage for the Germantown MARC Station on site A for the following reasons:

1. It would negatively impact the historic Pumphrey/Mateny House and the historic district more than a garage on lot B by being more visible, closer to the historic buildings, and not of a size and construction of buildings at that time.
2. The destruction of the present parking lot and the construction of a multi-level parking garage at that location would create vibrations and air pollution particles which may do harm to the existing nearby historic structures.

We would only approve a parking garage at that location if it was built in the exact same design as the double dairy barns of Walter "Big Train" Johnson's farm which had been located at the top of the hill to the east of the pond -- with silos.

We would support the construction of a parking garage on the west side of Rt. 118 (lot B) if it were to be separated from the road by an earth berm planted with coniferous trees and attractive period (19th cen.) shrubbery, and if it were limited to two stories above the ground and faced with red brick.

We thank you for your consideration of our requests.

Sincerely,

Susan Soderberg, President

cc: John A. Agro, Mass Transit Admin.
John Matthias, Mont. Co. Planning

MFC

Councilmember Nancy Dacek
Senator Jean Roesser
Germantown Citizens Assoc.
Germantown Alliance



March 6, 1995

Mr. John A. Agro, Jr.
Administrator
Maryland Mass Transit Administration
300 West Lexington Street
Baltimore, MD 21202-3415

RE: Germantown MARC Station

Dear Mr. Agro:

On Thursday, February 23, 1995 the Montgomery County Planning Board considered a Mandatory Referral review of the proposed parking lot expansion at the Germantown MARC station. Following its consideration of the staff report of February 17 and the addendum and revision of February 23 to the staff report, and discussion of the issues, the Planning Board voted to approve the staff recommendations as revised with two changes:

1. That the Mass Transit Administration (MTA) be responsible for all of the landscaping along the site's frontage with relocated MD 118, and
2. That the 14 parking spaces adjacent to existing MD 118 be deleted and that the area be added to the landscaped pedestrian area to be developed adjacent to the bridge over the railroad tracks.

Concerning the cost of the double row of trees along the frontage, the Planning Board policy is that developers provide the landscaping along road frontages if it is not already in place. Thus, MTA should provide the funds for all of the double row of trees along relocated MD 118.

The 1989 Germantown Master Plan recommends that the portion of the Town Center where the parking lot is located "be a quiet, green, pedestrian-oriented enclave." To achieve this recommendation, the Planning Board decided that the area of the 14 parking spaces adjacent to existing MD 118 should be added to the landscaped pedestrian area to be developed between Mateney Hill Road and the entrances to the existing and proposed parking lots.

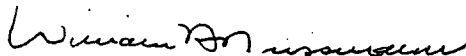
The Planning Board is looking forward to reviewing two additional Mandatory Referrals related to the Germantown MARC

station. One will be on the site selection study for the Germantown parking garage, the second will be on the platform improvements. It is anticipated that these items will be presented to the Planning Board within the next few months.

As noted in the staff report, MTA and the Montgomery County Department of Transportation (MCDOT) should begin developing plans for the landscaped pedestrian/bicycle crossing over the railroad tracks at existing MD 118. These plans will result in a separate project which will be reviewed as the plans near completion.

The Planning Board appreciates the high level of cooperation and communication between the staff of MTA, MCDOT, and the Planning Department. The quality of the proposed parking lot reviewed by the Planning Board reflects this interagency review.

Sincerely,

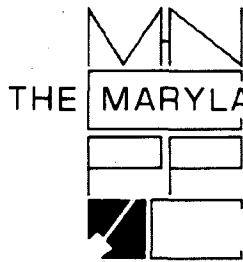


William H. Hussmann
Chairman

WHH:JOM:plb

cc: Graham Norton

David Winstead
Secretary of Transportation



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

8787 Georgia Avenue • Silver Spring, Maryland 20910-3760

(301) 495-4605

Montgomery County Planning Board
Office of the Chairman

July 27, 1995

Mr. John A. Agro, Jr., Administrator
Mass Transit Administration
300 West Lexington Street
Baltimore, MD 21201-3415

RE: Mandatory Referral on the
Parking Garage Site Selection
Study for the Germantown MARC
Station

Dear Mr. Agro:

On Thursday, July 20, the Montgomery County Planning Board reviewed the Mandatory Referral referenced above. Based on the information in the Staff Report and the presentations of the owners of two adjacent properties, the Planning Board strongly recommends that Lot B be selected as the location of the proposed parking garage. A copy of the Staff Report is enclosed for your reference. I understand that you have reviewed the site selection study but have not had the benefit of our position and staff analysis. The Planning Board feels that the conclusions reached by staff are convincing and that the choice is clear in favor of Lot B.

The most significant impact is to the Germantown Historic District and to the Pumphrey-Mateney House immediately adjacent to Lot A. The five-story facade of the parking garage proposed for Lot A would present a significant intrusion into the Historic District and tower over the existing one- and two-story buildings there. The Maryland Historic Trust should be provided an opportunity to review and comment on the relative impacts of the two alternative proposals and the possibility of mitigating the impact of the proposed five-story facade.

The Planning Board requests that the need for a surface parking lot on Lot B in the interim be reconsidered. Both when the staff prepared its review of this study and when they prepared the review of the surface parking lot, the existing surface facility, Lot A, was not more than 60 percent full. If the surface lot is not immediately needed, the cost per added space provided by a parking garage on Lot B will be much less.

Both speakers before the Board expressed their frustration in not being contacted by MTA during the preparation of the study. The

Planning Board and Planning Department staff share their frustration. In addition to the Montgomery County Planning Department, MTA project staff should inform adjacent residents and land owners as well as community associations when a major study or project is underway. Planning Department staff can assist in preparing address lists of appropriate people and organizations.

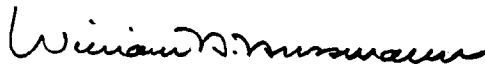
In addition to concerns regarding the visual impacts of a garage on Lot A, the owner of the Pumphrey-Mateney House raised concerns regarding security. There has already been vandalism to the building from people parking in the existing MARC lot, and he is concerned that the occurrences of vandalism will increase if a garage is built immediately behind his building.

Further, the owner has concerns that the construction activity required for a five-story parking garage on Lot A may well damage the rock-and-mortar foundation of his historic building. The movement of the large earth-moving and construction equipment past the building and the actual excavation could cause damaging vibrations.

Again, let me emphasize the Planning Board's support for a parking garage on Lot B, and strong opposition to one on Lot A.

It is important to achieve and maintain a high level of interagency coordination and cooperation in conducting major studies and in the planning, design and engineering of construction projects. We want to work with you and look forward to the review of the future parking garage at the Germantown MARC station.

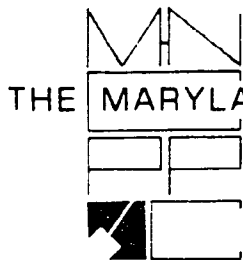
Sincerely,



William H. Hussmann
Chairman

WHH:JOM:plb\agro.724
Enclosure

cc: David L. Winstead, Secretary
Councilmember Nancy Dacek
Senator Jean W. Roesser
Senator Patrick J. Hogan
Delegate Jean Cryor
Delegate Richard LaVay
Delegate Mark K. Shriver
Delegate Raymond Beck
Delegate Barrie S. Ciliberti
Delegate Matthew J. Mossberg
Maryland Historic Trust
Montgomery County Historic Preservation Commission
Robert Albiol
Raymond Hower



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
8787 Georgia Avenue • Silver Spring Maryland 20910-3760

July 17, 1995
(CORRECTIONS AND ADDITIONS: July 21, 1995)

MEMORANDUM

TO: Montgomery County Planning Board

FROM: John Matthias for the Montgomery County Planning Department (495-4569) *jm*

SUBJECT: Mandatory Referral: Site Selection for the MARC Parking Garage at the
Germantown Station

STAFF RECOMMENDATION

Staff recommends that Lot B, rather than the MTA-recommended Lot A, be used for a new parking garage. Lot B is a more appropriate site because it:

- is more compatible with the 1989 Germantown Master Plan and community
- will have less impact on the Germantown Historic District
- will ensure a greater level of pedestrian safety
- will have less impact on the surrounding roadway network
- will cost less per additional parking space
- will have less impact on the surrounding environmental features

OUTSTANDING ISSUES

Planning Department staff and the MTA Administrator do not agree on the most appropriate site for the proposed parking garage at the Germantown MARC station. Staff's determination that Lot B is more appropriate is based on the reasons cited above and elaborated in Section 3 of this Staff Report. The MTA Administrator prefers Lot A as the parking spaces will be closer to the platforms. As there will probably be a charge for the parking spaces, proximity is important. Further, a garage on Lot B would require removing the surface parking lot to be built there.

STAFF REPORT

1. Background

The Germantown MARC station is located southeast of the existing MD 118 crossing over the railroad tracks (Figure 1). The station is currently served by two parking lots, one on each side of the tracks.

A consultant study was completed in June 1994 for the Mass Transit Administration (MTA) of the Maryland Department of Transportation (MDOT). That study compared the two sites indicated on Figure 2. Lot A currently has 231 parking spaces and is the larger of the two existing lots. Lot B is currently an undeveloped wooded area and the location of the proposed 306-space surface parking lot reviewed by the Planning Board on February 23. Figure 3 shows the parking lot approved for Lot B. Copies of the correspondence related to that review are attached. A copy of the consultant study is also attached.

The consultant study and the accompanying recommendations of MTA staff have been presented to the Administrator of the MTA. The study was not referred to the Montgomery County Planning Department; therefore, the only local agency commenting was the Montgomery County Department of Transportation (MCDOT). The need to go through the Mandatory Referral process was brought to the attention of the consultant during the preparation of the study. However, Planning Department staff only became aware of the study during its Mandatory Referral review of the proposed surface parking lot on Lot B.

The Administrator selected Lot A as the site for the parking garage because it is closer to the existing platform and because it would not require tearing up the recently completed parking lot at Lot B. Proximity to the platform is a central criterion since the MTA intends to charge for parking in the garage.

In his response to the Planning Board's comments regarding the proposed surface parking lot on Lot B, the Administrator indicated that the location for the proposed parking garage would be considered in the future. Figure 4 indicates the layout of the proposed parking lot on Lot B.

2. Alternative Proposals

The garage proposed for Lot A would provide 780 spaces on five levels where the 231-space surface parking lot exists today. (See Figure 4.) Two levels would be below-grade and two above-grade. As mentioned earlier, a 306-space surface parking lot is planned for Lot B. The garage studied on lot B would have 768 spaces. Given the addition of a public street between existing MD 118 and relocated MD 118, changes will be needed to the proposed layout shown in the study. These changes will probably necessitate an additional level to accommodate the 768 spaces noted in the study. Table 1 is a comparison of the two sites from the consultant study.

3. Staff Evaluation

Staff recommends that Lot B be the site of the proposed parking garage. That recommendation is based on a review of the following issues:

- (1) Community and master plan compatibility
- (2) Impact on the Germantown Historic District and other adjacent historic resources
- (3) Pedestrian safety
- (4) Traffic impact on adjacent intersections
- (5) Cost per additional parking space
- (6) Environmental considerations

For each issue, staff review indicated that Lot B would be a preferable location for a parking garage.

(1) *Community and Master Plan Compatibility*

The proposed sites are both located in Analysis Area TC-6, the Town Center Historic Area of Germantown. This Analysis Area is recommended for low intensity uses which are compatible with the existing residential character of the area. Specifically, the 1989 Germantown Master Plan states on page 42:

This Plan recommends that this Analysis Area be a quiet, green, pedestrian-oriented enclave that provides relief from the intense development of the Town Center. It should establish a strong visual and functional link to the Germantown Historic District directly across the railroad tracks. (See Analysis Area CL-1.) When Relocated MD 118 is open to traffic, the existing railroad bridge will be either closed to automobile traffic or replaced by a pedestrian bridge. This will serve to limit traffic and further enhance the passive environment.

The structures on the east side of existing MD 118 consist of historic and other older frame residential buildings, some of which have been converted to commercial uses. On the east side few older buildings remain while new buildings have been built and more have been approved. These buildings are of a larger scale than those on the other side of existing MD 118 and are appropriate along relocated MD 118.

A ~~three- or four-story~~ garage would be appropriate for Lot B given its location adjacent to relocated MD 118 and the newer, larger-scale buildings. A multi-story parking garage on Lot A, even one with a facade, would conflict with the recommendations of the Master Plan to provide a strong visual and functional link to the Germantown Historic District.

(2) *Impact on the Germantown Historic District*

The Germantown Historic District is the one area in Germantown with an intact ensemble of historic resources that recall an overall historic ambiance. It is the heart of late 19th Century-

early 20th Century Germantown. The collection of remaining buildings — both commercial and residential — are the physical evidence of where, why, and how Germantown originated. Figure 5 indicates the location of the Historic District and the two alternative sites.

The 1989 Germantown Master Plan recommends (page 233) “that any subdivision or site plan in the areas bordering on or adjacent to the historic district be given careful consideration in terms of its impact on the historic district.” A garage on Lot A would present a significant intrusion into the Germantown Historic District and tower over the existing one- and two-story buildings there. Figure 6 indicates the approximate scale of the facade as viewed from the Historic District. The five-story facade would directly face several historic resources in the District. Although the study indicates that there could be a special facade treatment, the scale of the five-story facade of the parking garage would still be out of character with the historic area.

Concerns regarding the location and height of the proposed parking garage on Lot B as well as its visual impact on the Historic District were raised by staff to the consultants during the preparation of the study.

The Maryland Historic Trust has reviewed only the proposed surface parking lot on Lot B and found that it would have no effect on historic properties. MTA typically solicits comments from the Maryland Historic Trust after the completion of construction drawings for the facility on the selected site. At that point in the process, all alternatives have been reviewed and one has been chosen. This procedure does not provide the Maryland Historic Trust an opportunity to comment on the relative merits of the alternative proposals and on the compatibility of a five-story parking garage adjacent to the Germantown Historic District.

Staff finds that the impact on the Historic District of a five-story parking garage on Lot A would be significantly greater than the impact of a garage on Lot B. Therefore, Lot B is recommended to be the location of the proposed parking garage.

(3) *Pedestrian Safety*

Although a garage on Lot A would be closer to the platforms, one on Lot B would provide increased pedestrian safety. A garage on Lot A would be closer to the station platform than one on Lot B (approximately 300 feet on average). The distance between the lobby entrance to MRO to the entrance of the parking garage is nearly 400 feet and the horizontal distance to the middle of the staff parking area is approximately 600 feet. The distance from the center of the proposed garage on Lot A to the center of the southbound platform is approximately 360 feet. The corresponding distance from Lot B is 650 feet. However, all of those who park in Lot A must cross the tracks at-grade to get to the inbound side of the platform each morning. As the existing MD 118 bridge over the railroad tracks will be restricted to pedestrian- and bicycle-only use once relocated MD 118 is open to traffic, people who park in Lot B can use the bridge to safely cross the tracks to reach the inbound platform.

While the Germantown MARC station patrons have been safely crossing the tracks for the last several years, the risk of serious injury still exists. Earlier this year a woman was struck and

killed in a similar situation at the Laurel MARC station. The growing number of CSX and Amtrak trains will increase the risk.

Because of the reduced potential for personal injury and fatality, staff strongly recommends that Lot B be the location of the proposed parking garage.

(4) *Traffic Impact*

Traffic patterns from a parking garage on Lot B would create less congestion than one on Lot A. Autos can reach a garage on Lot B directly from relocated MD 118, a six-lane divided highway. Cars entering and exiting a garage on Lot A would be more likely to use the intersection of existing MD 118 and Wisteria Drive, or to drive past the proposed surface parking lot on Lot B. Both of these traffic movements would cause congestion as they conflict with other dominant vehicular movements during the morning and evening peak periods. This traffic would also conflict with pedestrian connections to the community services and retail shops across relocated MD 118.

(5) *Cost per Additional Parking Space*

According to the MTA study, the proposed garage on Lot A will cost more than one on Lot B. Further, a garage on Lot A would remove existing spaces. Although more spaces are proposed for a garage on Lot A versus one on Lot B (780 versus 768), the net loss of spaces would be greater. (See Table 2.) Currently, there are 231 spaces at Lot A. MTA's plan would add a surface lot with 306 spaces to Lot B. Building the MTA-planned garage on Lot A would eliminate the 231 spaces and add 780 more, a net increase of 549 spaces, at a cost of \$16,472,000. Therefore, the cost per additional space would be the cost of building the garage on Lot A would be approximately \$30,000

Alternatively, building a parking garage on Lot B would add 462 net spaces (768 in the garage less the 306 in the surface parking lot) at a cost of \$14,062,000 (see Table 1). The approximate cost per additional space would therefore be \$30,400. The above figures are for a garage without a facade. If a facade is added, the cost per additional space is approximately \$32,100 for Lot A and \$34,300 for Lot B. However, since Lot B would not have the impact of Lot A on historic properties, a facade would most likely not be necessary, making Lot B approximately \$1,700 less expensive per space. This is five percent less per added space than for one on Lot A with a facade.

Given the public road through Lot B, the land available for the parking lot is reduced. A three-story garage would provide for the 768 spaces. The additional height may well reduce the cost differential between the two options.

Another option would be to not construct the currently planned surface parking lot on Lot B and only build the parking garage there. The primary benefit would be the cost per additional space, which would fall from \$30,400 to \$18,300. However, the surface parking lot

is scheduled to begin construction this year; the timetable for the parking garage is less certain as funding is not currently available.

(6) *Environmental Considerations*

As both sites already have parking lots, there would be few additional impacts on the natural environment no matter which site is ultimately chosen. The issue is the proximity of the proposed garage on Lot A to the existing stormwater management pond.

The proposed garage as delineated violates the 25-foot minimum building setback from a stormwater management pond. A Flood Plain District Permit will be required if Lot A is selected as the location of the parking garage.

Consideration of each of the relevant issues resulted in the staff recommendation that the proposed parking garage be located on Lot B.

4. Design Guidelines for the Design and Engineering of the Proposed Garage

If Lot A is selected as the site for the parking garage, the following design guidelines should be followed:

- Provide a park-like area south of the Pumphrey/Mateney House with landscaping, special paving, lighting, seating, and a focal point such as a fountain, sculpture, and/or historic interpretive display. This area should be designed to enhance the historic theme of the area and to relate to the Historic District and the Pumphrey/Mateney House. (See Figure 7 for sketch illustration.) This guideline responds to the recommendations of the Germantown Town Center Design Study regarding "Old MD 118."
- Provide lighting and special paving consistent with the historic theme of the Germantown MARC station.
- Provide street trees spaced 30 feet apart along existing MD 118 for the frontage of the site not covered by the parking lot on Lot B.
- Provide pedestrian and bicycle connections to and through the garage.

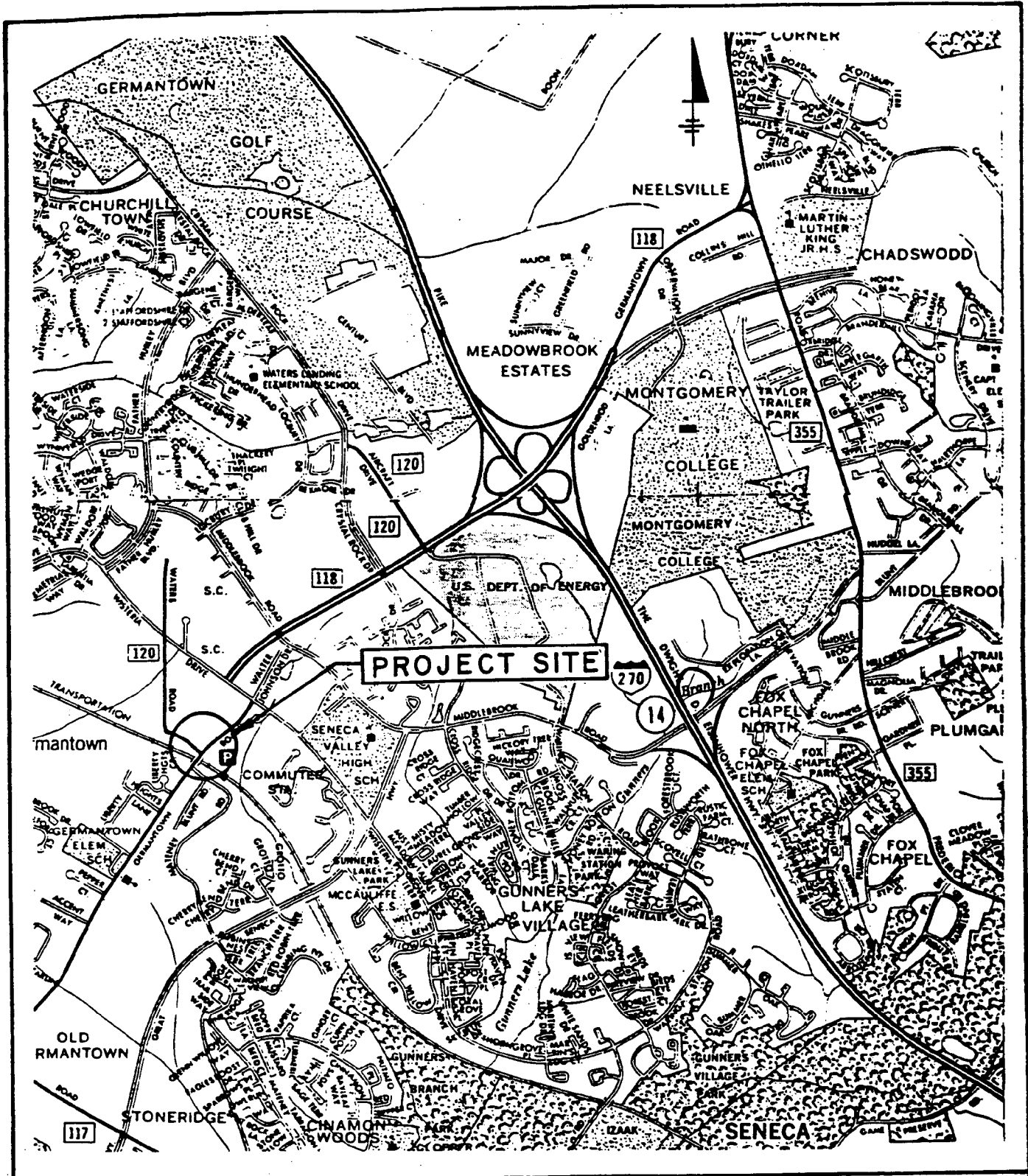
If Lot B is selected, then the following design guidelines should be followed:

- Provide lighting and special paving consistent with the historic theme of the Germantown MARC station.
- Provide a low hedge or wall along the edge of the parking garage abutting roadways.

In either case, Planning Department staff should be involved in the development and review of the construction plans for the proposed garage.

marcpark.mr

Figure 1

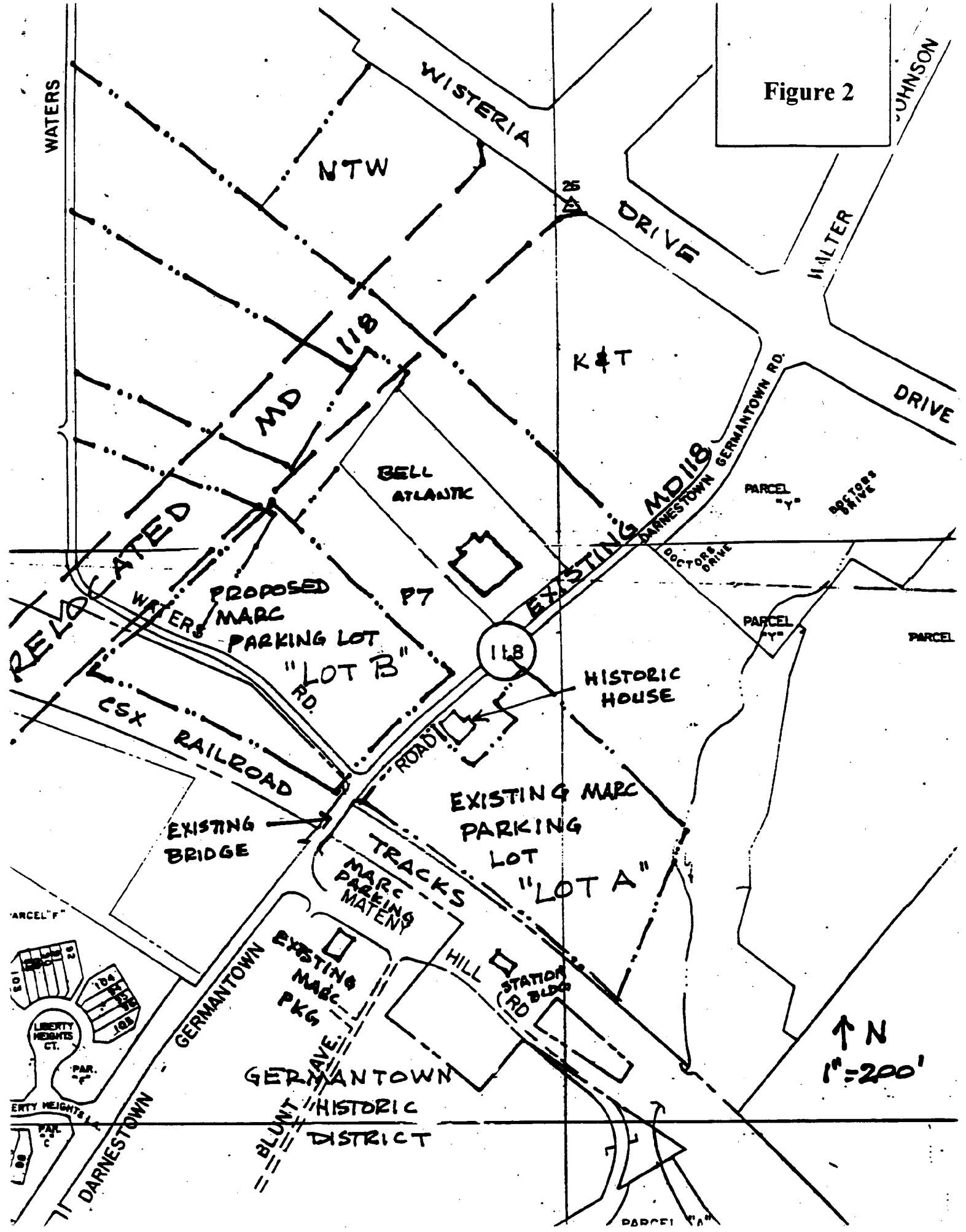


WHITNEY
BAILEY
COX
MAGNANI

LOCATION MAP
GERMANTOWN PARK-N-RIDE

FIGURE 1

Figure 2



MD ROUTE 118 (RELOCATED)

DESIGNED BY: RELCO, MD 118 S.R.C. PLAT NO. 31179

Figure 3

FUTURE 12' DECELERATION LANE

127.5' = 15 PARKING SPACES

118' = 14 PARKING SPACES

SEE DETAIL 3, DISC. CE-10 (TYP.)

5' = 8 PARKING SPACES

38' = 8 PARKING SPACES

93.5' = 11 PARKING SPACES

155' = 18 PARKING SPACES

153' = 18 PARKING SPACES

153' = 18 PARKING SPACES

153' = 18 PARKING SPACES

153' = 18 PARKING SPACES

153' = 18 PARKING SPACES

181' = 18 PARKING SPACES

93.5' = 11 PARKING SPACES

118' = 14 PARKING SPACES

127.5' = 15 PARKING SPACES

136' = 18 PARKING SPACES

146.5' = 17 PARKING SPACES

144.5' = 17 PARKING SPACES

144.5' = 17 PARKING SPACES

153' = 18 PARKING SPACES

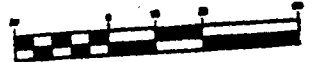
65' = 10 PARKING SPACES

34' = 4 PARKING SPACES

14 spaces deleted

SEE DETAIL 3, DISC. CE-10 (TYP.)

GRAPHIC SCALE



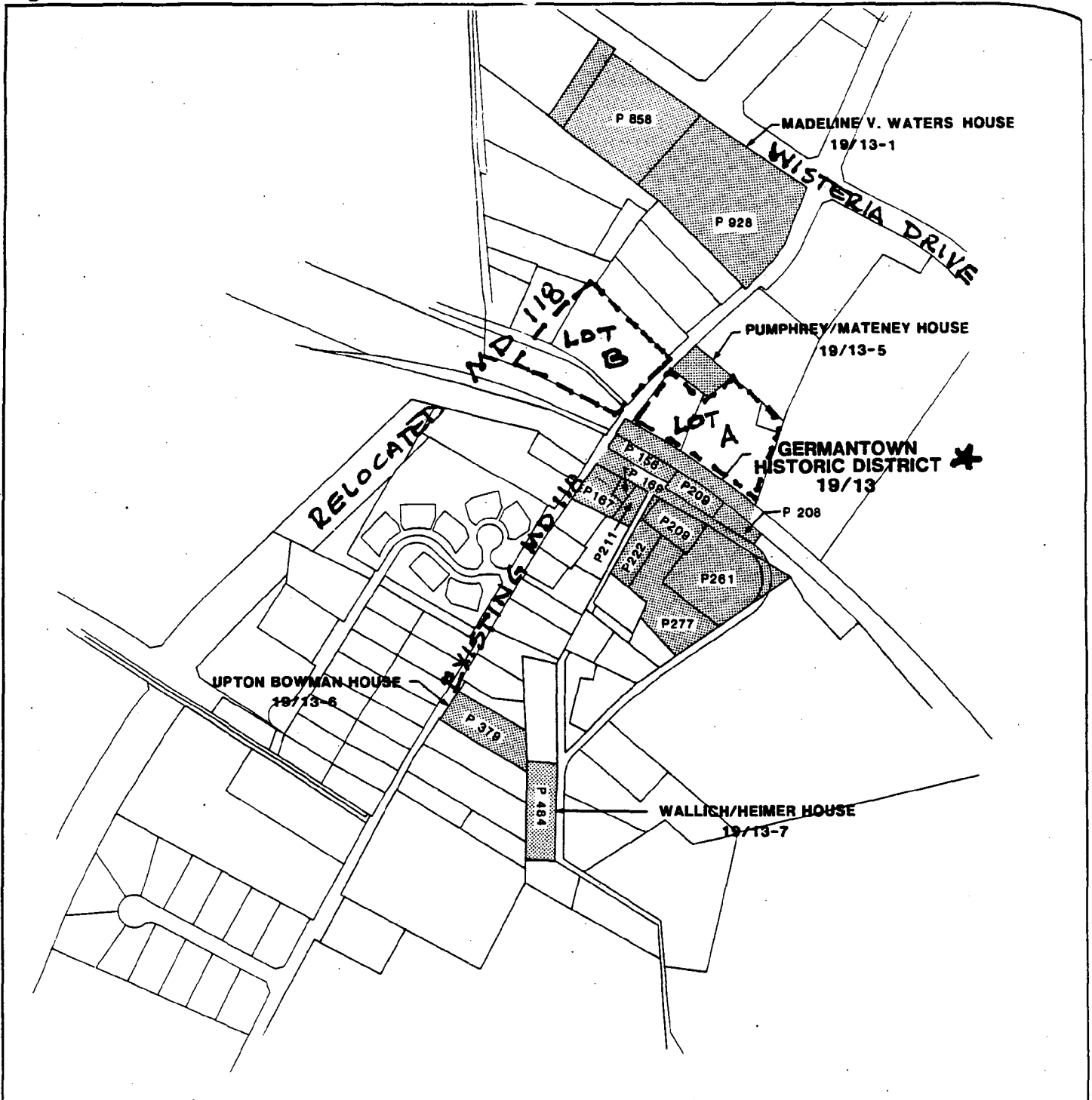
EXIST. MD. 118

TO EXISTING MAJOR PARKING

CONCRETE WALL

CONCRETE WALL

CONCRETE WALL



Germantown Historic District
 And Adjacent Historic Resources

 **Comprehensive Amendment**
to the Master Plan for Germantown
 Montgomery County, Maryland
 The Maryland-National Capital Park and Planning Commission

View from Germantown Historic District

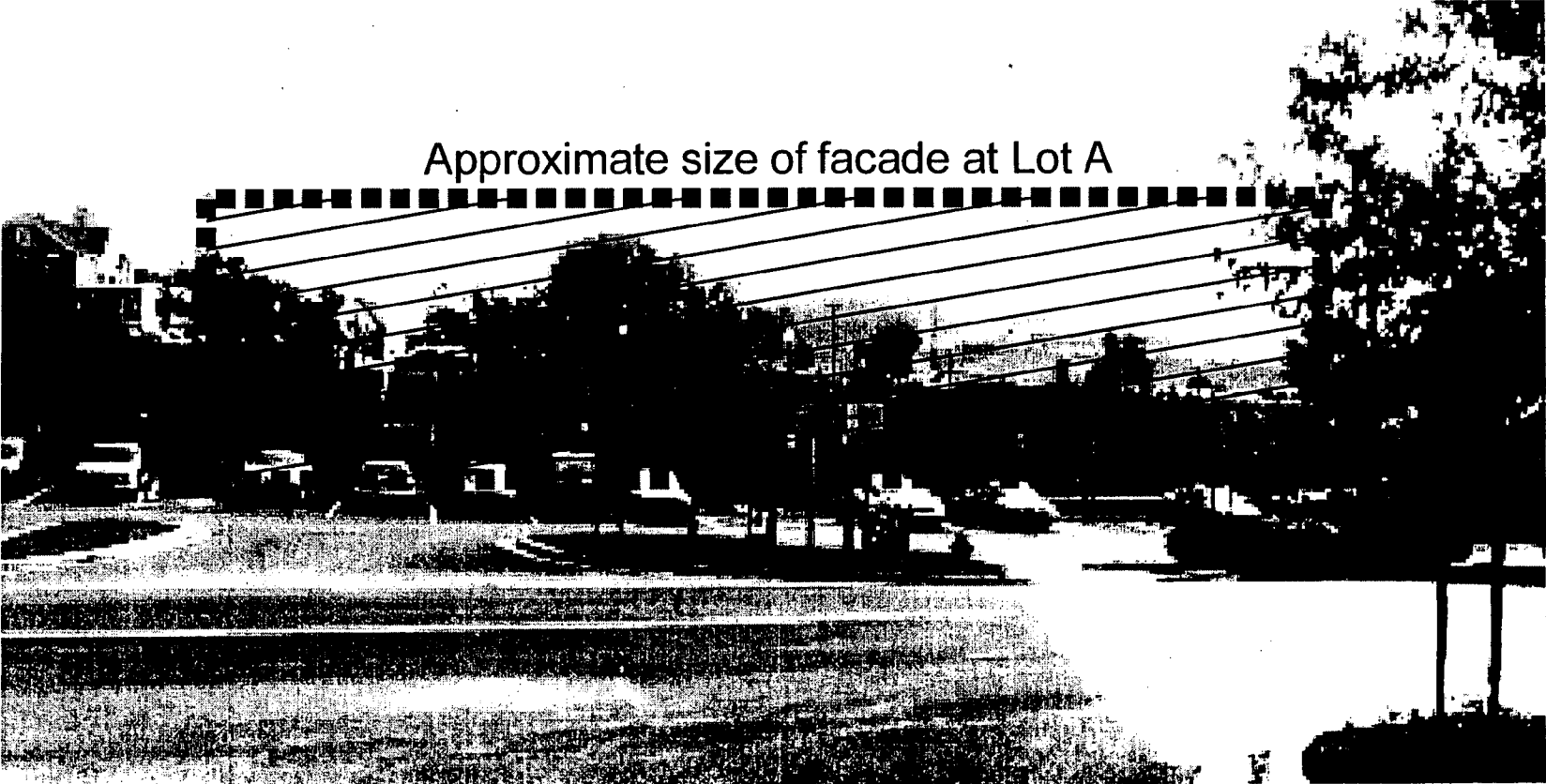


Figure 6

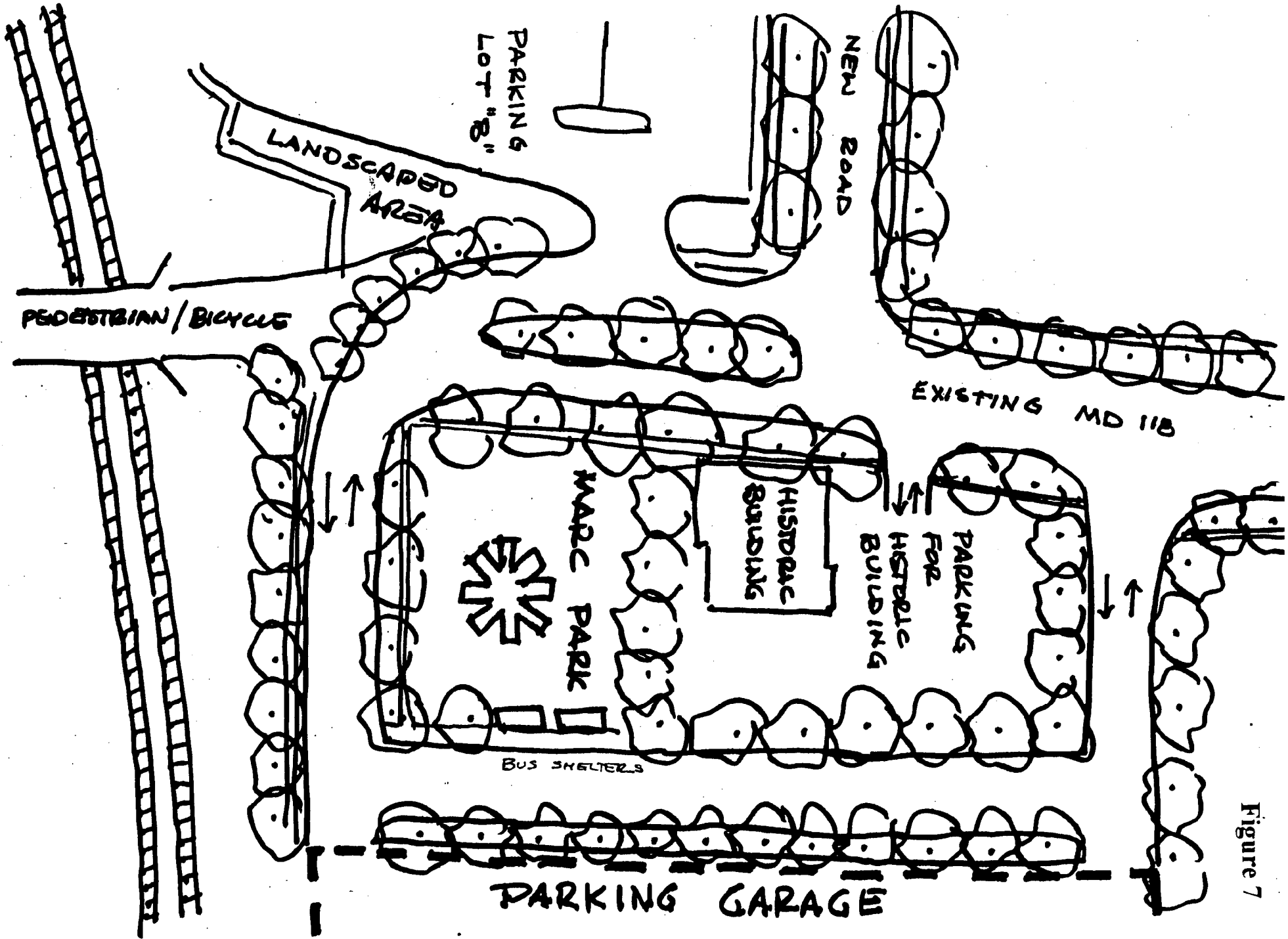


Figure 7

Table 1

**ALTERNATIVE SITE ANALYSIS
GERMANTOWN PARKING GARAGE**

SITE FACTORS	LOT 'A'	LOT 'B'
Vehicular Access	From existing MD Rte 118	May be able to have access from relocated MD 118
Pedestrian Access to Station Platform	Direct access from bottom level or elevator	Access via stairs to extended station platform (450' walk distance to boarding area)
Historic Impacts	Proposed garage located directly across from the historic district. Also historic building on same site (80' from proposed garage structure)	Visual impact to historic district a concern.
Displacement of Existing Parking	All existing parking (375 spaces will be displaced)	Existing parking on proposed surface lot will be displaced (300 spaces)
Stormwater Management Quantity	Existing regional SWM pond (no quantity management)	Modifications to existing regional pond or new pond required for quantity management
Stormwater Management Quality	Sand filter water quality structure and oil interceptor	Sand filter water quality structure and oil interceptor
Cost Comparison w/o Facade	\$16,472,000	\$14,062,000
Cost Comparison w/ Facade	\$17,599,000	\$15,858,000

Table 2

	Lot A	Lot B	
		with surface lot	without surface lot
Number of Spaces			
A Proposed Spaces	780	768	768
B Existing Spaces	231	306	0
C Net Additional Spaces (A-B)	549	462	768
Estimated Cost			
D without facade	\$16,472,000	\$14,062,000	\$14,062,000
E with facade	\$17,599,000	\$15,858,000	\$15,858,000
Estimated Cost per Additional Space			
F without facade (D+C)	\$30,000	\$30,400	\$18,300
G with facade (E+C)	\$32,100	\$34,300	\$20,600

7/5/95
Owen - I talked w/
Beth Hammell about
Georgetown parking -

She's being told by MTA
that there's no pressure to
build the parking garage -
& MTA is aware that
MTA would have to review
parking garage proposals.

Robin

Q: Is there pressure on now?

6/26/95 9/10/95

Owen -

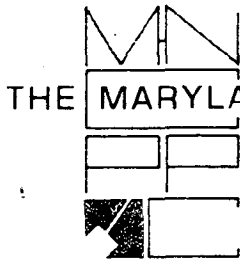
Beth Harold says MHT
only reviewed surface parking
proposal. They haven't seen
either parking garage -

She hadn't been aware of our
local HD! - only a district
along I-8 - so I faxed over
the local HD maps.

I told John Mathias all
this.

Beth spoke with her contact @
DET - she got the impression that this
was not a hot priority -

Robin




THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
8787 Georgia Avenue • Silver Spring Maryland 20910-3760

July 17, 1995

MEMORANDUM

TO: Montgomery County Planning Board

FROM: John Matthias for the Montgomery County Planning Department (495-4569) 

SUBJECT: Mandatory Referral: Site Selection for the MARC Parking Garage at the Germantown Station

STAFF RECOMMENDATION

Staff recommends that Lot B, rather than the MTA-recommended Lot A, be used for a new parking garage. Lot B is a more appropriate site because it:

- is more compatible with the 1989 Germantown Master Plan and community
- will have less impact on the Germantown Historic District
- will ensure a greater level of pedestrian safety
- will have less impact on the surrounding roadway network
- will cost less per additional parking space
- will have less impact on the surrounding environmental features

OUTSTANDING ISSUES

Planning Department staff and the MTA Administrator do not agree on the most appropriate site for the proposed parking garage at the Germantown MARC station. Staff's determination that Lot B is more appropriate is based on the reasons cited above and elaborated in Section 3 of this Staff Report. The MTA Administrator prefers Lot A as the parking spaces will be closer to the platforms. As there will probably be a charge for the parking spaces, proximity is important. Further, a garage on Lot B would require removing the surface parking lot to be built there.

STAFF REPORT

1. Background

The Germantown MARC station is located southeast of the existing MD 118 crossing over the railroad tracks (Figure 1). The station is currently served by two parking lots, one on each side of the tracks.

A consultant study was completed in June 1994 for the Mass Transit Administration (MTA) of the Maryland Department of Transportation (MDOT). That study compared the two sites indicated on Figure 2. Lot A currently has 231 parking spaces and is the larger of the two existing lots. Lot B is currently an undeveloped wooded area and the location of the proposed 306-space surface parking lot reviewed by the Planning Board on February 23. Figure 3 shows the parking lot approved for Lot B. Copies of the correspondence related to that review are attached. A copy of the consultant study is also attached.

The consultant study and the accompanying recommendations of MTA staff have been presented to the Administrator of the MTA. The study was not referred to the Montgomery County Planning Department; therefore, the only local agency commenting was the Montgomery County Department of Transportation (MCDOT). The need to go through the Mandatory Referral process was brought to the attention of the consultant during the preparation of the study. However, Planning Department staff only became aware of the study during its Mandatory Referral review of the proposed surface parking lot on Lot B.

The Administrator selected Lot A as the site for the parking garage because it is closer to the existing platform and because it would not require tearing up the recently completed parking lot at Lot B. Proximity to the platform is a central criterion since the MTA intends to charge for parking in the garage.

In his response to the Planning Board's comments regarding the proposed surface parking lot on Lot B, the Administrator indicated that the location for the proposed parking garage would be considered in the future. Figure 4 indicates the layout of the proposed parking lot on Lot B.

2. Alternative Proposals

The garage proposed for Lot A would provide 780 spaces on five levels where the 231-space surface parking lot exists today. (See Figure 4.) Two levels would be below-grade and two above-grade. As mentioned earlier, a 306-space surface parking lot is planned for Lot B. The garage studied on lot B would have 768 spaces. Given the addition of a public street between existing MD 118 and relocated MD 118, changes will be needed to the proposed layout shown in the study. These changes will probably necessitate an additional level to accommodate the 768 spaces noted in the study. Table 1 is a comparison of the two sites from the consultant study.

3. Staff Evaluation

Staff recommends that Lot B be the site of the proposed parking garage. That recommendation is based on a review of the following issues:

- (1) Community and master plan compatibility
- (2) Impact on the Germantown Historic District and other adjacent historic resources
- (3) Pedestrian safety
- (4) Traffic impact on adjacent intersections
- (5) Cost per additional parking space

(6) Environmental considerations

For each issue, staff review indicated that Lot B would be a preferable location for a parking garage.

(1) *Community and Master Plan Compatibility*

The proposed sites are both located in Analysis Area TC-6, the Town Center Historic Area of Germantown. This Analysis Area is recommended for low intensity uses which are compatible with the existing residential character of the area. Specifically, the 1989 Germantown Master Plan states on page 42:

This Plan recommends that this Analysis Area be a quiet, green, pedestrian-oriented enclave that provides relief from the intense development of the Town Center. It should establish a strong visual and functional link to the Germantown Historic District directly across the railroad tracks. (See Analysis Area CL-1.) When Relocated MD 118 is open to traffic, the existing railroad bridge will be either closed to automobile traffic or replaced by a pedestrian bridge. This will serve to limit traffic and further enhance the passive environment.

The structures on the east side of existing MD 118 consist of historic and other older frame residential buildings, some of which have been converted to commercial uses. On the east side few older buildings remain while new buildings have been built and more have been approved. These buildings are of a larger scale than those on the other side of existing MD 118 and are appropriate along relocated MD 118.

A two- or three-story garage would be appropriate for Lot B given its location adjacent to relocated MD 118 and the newer, larger-scale buildings. A multi-story parking garage on Lot A, even one with a facade, would conflict with the recommendations of the Master Plan to provide a strong visual and functional link to the Germantown Historic District.

(2) *Impact on the Germantown Historic District*

The Germantown Historic District is the one area in Germantown with an intact ensemble of historic resources that recall an overall historic ambiance. It is the heart of late 19th Century-early 20th Century Germantown. The collection of remaining buildings — both commercial and residential — are the physical evidence of where, why, and how Germantown originated. Figure 5 indicates the location of the Historic District and the two alternative sites.

The 1989 Germantown Master Plan recommends (page 233) “that any subdivision or site plan in the areas bordering on or adjacent to the historic district be given careful consideration in terms of its impact on the historic district.” A garage on Lot A would present a significant intrusion into the Germantown Historic District and tower over the existing one- and two-story buildings there. Figure 6 indicates the approximate scale of the facade as viewed from the Historic District. The five-story facade would directly face several historic resources in the District. Although the study indicates that there could be a special facade treatment, the

scale of the five-story facade of the parking garage would still be out of character with the historic area.

Concerns regarding the location and height of the proposed parking garage on Lot B as well as its visual impact on the Historic District were raised by staff to the consultants during the preparation of the study.

The Maryland Historic Trust has reviewed only the proposed surface parking lot on Lot B and found that it would have no effect on historic properties. MTA typically solicits comments from the Maryland Historic Trust after the completion of construction drawings for the facility on the selected site. At that point in the process, all alternatives have been reviewed and one has been chosen. This procedure does not provide the Maryland Historic Trust an opportunity to comment on the relative merits of the alternative proposals and on the compatibility of a five-story parking garage adjacent to the Germantown Historic District.

Staff finds that the impact on the Historic District of a five-story parking garage on Lot A would be significantly greater than the impact of a garage on Lot B. Therefore, Lot B is recommended to be the location of the proposed parking garage.

(3) *Pedestrian Safety*

Although a garage on Lot A would be closer to the platforms, one on Lot B would provide increased pedestrian safety. A garage on Lot A would be closer to the station platform than one on Lot B (approximately 150 feet on average, roughly the distance from the parking garage stair tower to the lobby entrance). However, all of those who park in Lot A must cross the tracks at-grade to get to the inbound side of the platform each morning. As the existing MD 118 bridge over the railroad tracks will be restricted to pedestrian- and bicycle-only use once relocated MD 118 is open to traffic, people who park in Lot B can use the bridge to safely cross the tracks to reach the inbound platform.

While the Germantown MARC station patrons have been safely crossing the tracks for the last several years, the risk of serious injury still exists. Earlier this year a woman was struck and killed in a similar situation at the Laurel MARC station. The growing number of CSX and Amtrak trains will increase the risk.

Because of the reduced potential for personal injury and fatality, staff strongly recommends that Lot B be the location of the proposed parking garage.

(4) *Traffic Impact*

Traffic patterns from a parking garage on Lot B would create less congestion than one on Lot A. Autos can reach a garage on Lot B directly from relocated MD 118, a six-lane divided highway. Cars entering and exiting a garage on Lot A would be more likely to use the intersection of existing MD 118 and Wisteria Drive, or to drive past the proposed surface parking lot on Lot B. Both of these traffic movements would cause congestion as they conflict with other dominant vehicular movements during the morning and evening peak periods. This

traffic would also conflict with pedestrian connections to the community services and retail shops across relocated MD 118.

(5) *Cost per Additional Parking Space*

According to the MTA study, the proposed garage on Lot A will cost more than one on Lot B. Further, a garage on Lot A would remove existing spaces. Although more spaces are proposed for a garage on Lot A versus one on Lot B (780 versus 768), the net loss of spaces would be greater. Currently, there are 231 spaces at Lot A. MTA's plan would add a surface lot with 306 spaces to Lot B. Building the MTA-planned garage on Lot A would eliminate the 231 spaces and add 780 more, a net increase of 549 spaces, at a cost of \$16,472,000. Therefore, the cost per additional space would be the cost of building the garage on Lot A would be approximately \$30,000

Alternatively, building a parking garage on Lot B would add 462 net spaces (768 in the garage less the 306 in the surface parking lot) at a cost of \$14,062,000 (see Table 1). The approximate cost per additional space would therefore be \$30,400. The above figures are for a garage without a facade. If a facade is added, the cost per additional space is approximately \$32,100 for Lot A and \$34,300 for Lot B. However, since Lot B would not have the impact of Lot A on historic properties, a facade would most likely not be necessary, making Lot B approximately \$1,700 less expensive per space. This is five percent less per added space than for one on Lot A with a facade.

Given the public road through Lot B, the land available for the parking lot is reduced. A three-story garage would provide for the 768 spaces. The additional height may well reduce the cost differential between the two options.

Another option would be to not construct the currently planned surface parking lot on Lot B and only build the parking garage there. The primary benefit would be the cost per additional space, which would fall from \$30,400 to \$18,300. However, the surface parking lot is scheduled to begin construction this year; the timetable for the parking garage is less certain as funding is not currently available.

(6) *Environmental Considerations*

As both sites already have parking lots, there would be few additional impacts on the natural environment no matter which site is ultimately chosen. The issue is the proximity of the proposed garage on Lot A to the existing stormwater management pond.

The proposed garage as delineated violates the 25-foot minimum building setback from a stormwater management pond. A Flood Plain District Permit will be required if Lot A is selected as the location of the parking garage.

Consideration of each of the relevant issues resulted in the staff recommendation that the proposed parking garage be located on Lot B.

4. Design Guidelines for the Design and Engineering of the Proposed Garage

If Lot A is selected as the site for the parking garage, the following design guidelines should be followed:

- Provide a park-like area south of the Pumphrey/Mateney House with landscaping, special paving, lighting, seating, and a focal point such as a fountain, sculpture, and/or historic interpretive display. This area should be designed to enhance the historic theme of the area and to relate to the Historic District and the Pumphrey/Mateney House. (See Figure 7 for sketch illustration.) This guideline responds to the recommendations of the Germantown Town Center Design Study regarding "Old MD 118."
- Provide lighting and special paving consistent with the historic theme of the Germantown MARC station.
- Provide street trees spaced 30 feet apart along existing MD 118 for the frontage of the site not covered by the parking lot on Lot B.
- Provide pedestrian and bicycle connections to and through the garage.

If Lot B is selected, then the following design guidelines should be followed:

- Provide lighting and special paving consistent with the historic theme of the Germantown MARC station.
- Provide a low hedge or wall along the edge of the parking garage abutting roadways.

In either case, Planning Department staff should be involved in the development and review of the construction plans for the proposed garage.

g:\marcpark.mr

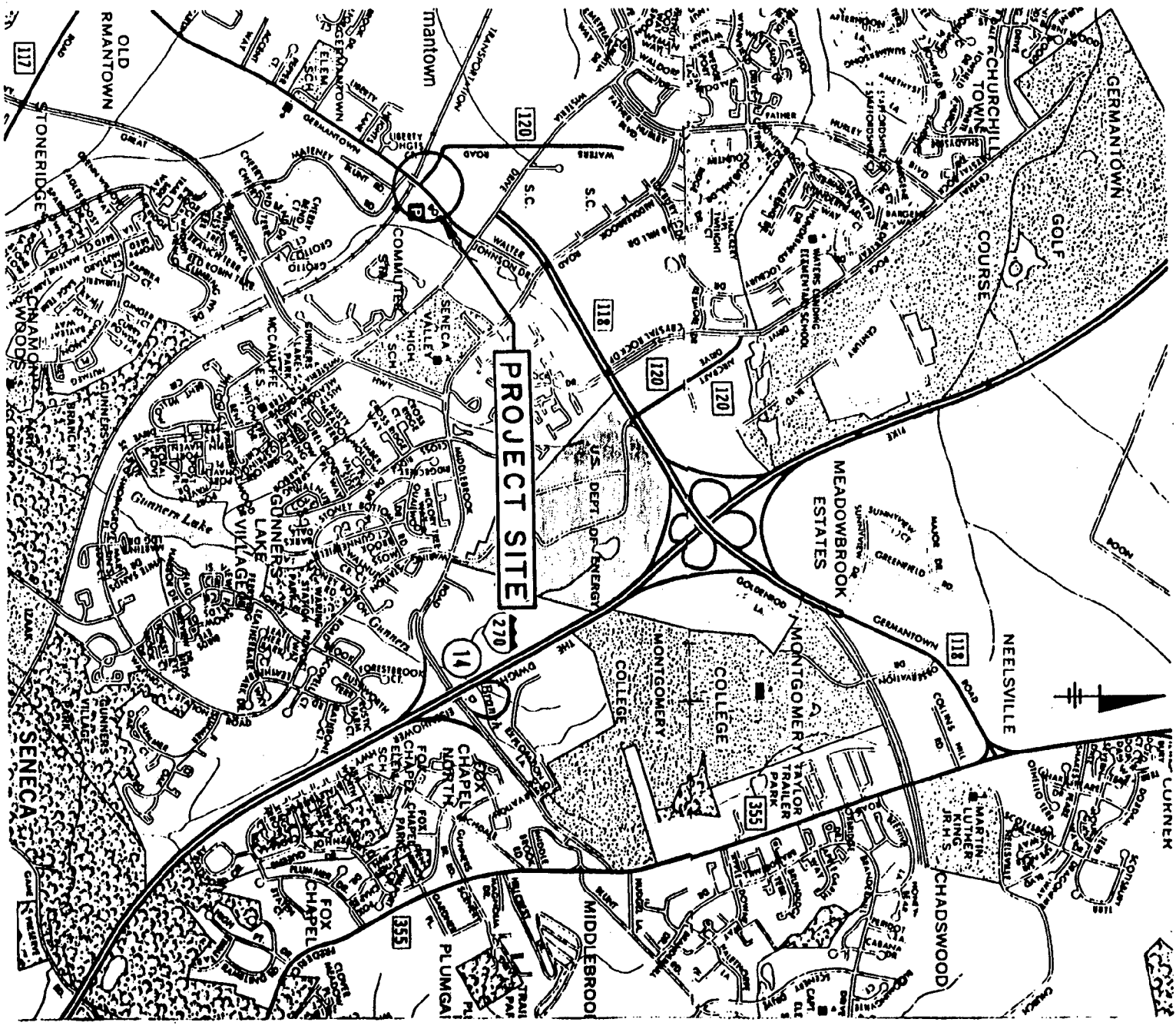


Figure 1



WHITNEY
BAILEY
COX
MAGNANI

LOCATION MAP
GERMANTOWN PARK - N - RIDE

FIGURE 1

Figure 2

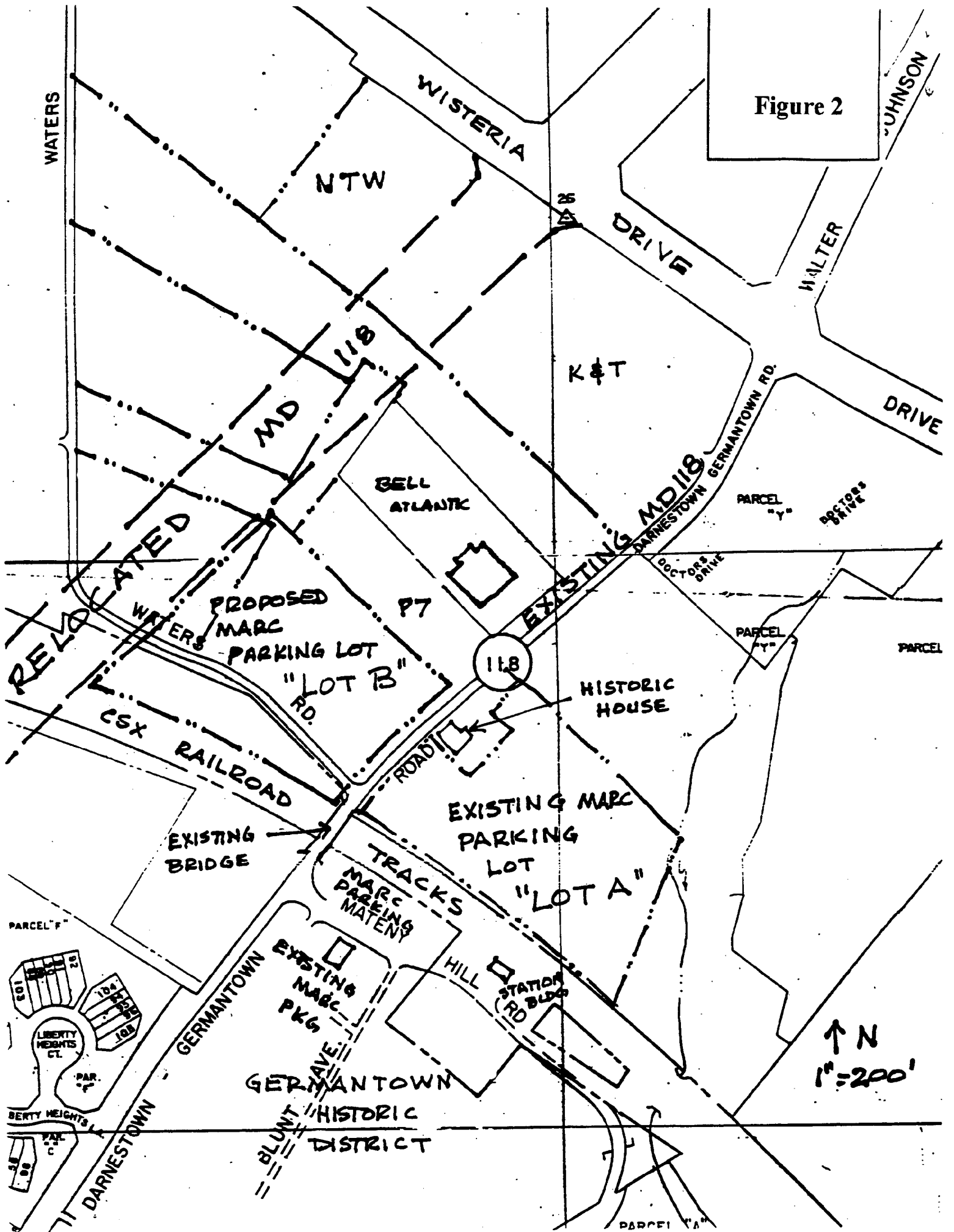
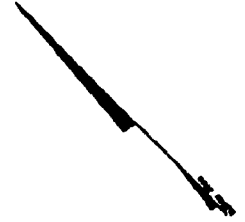
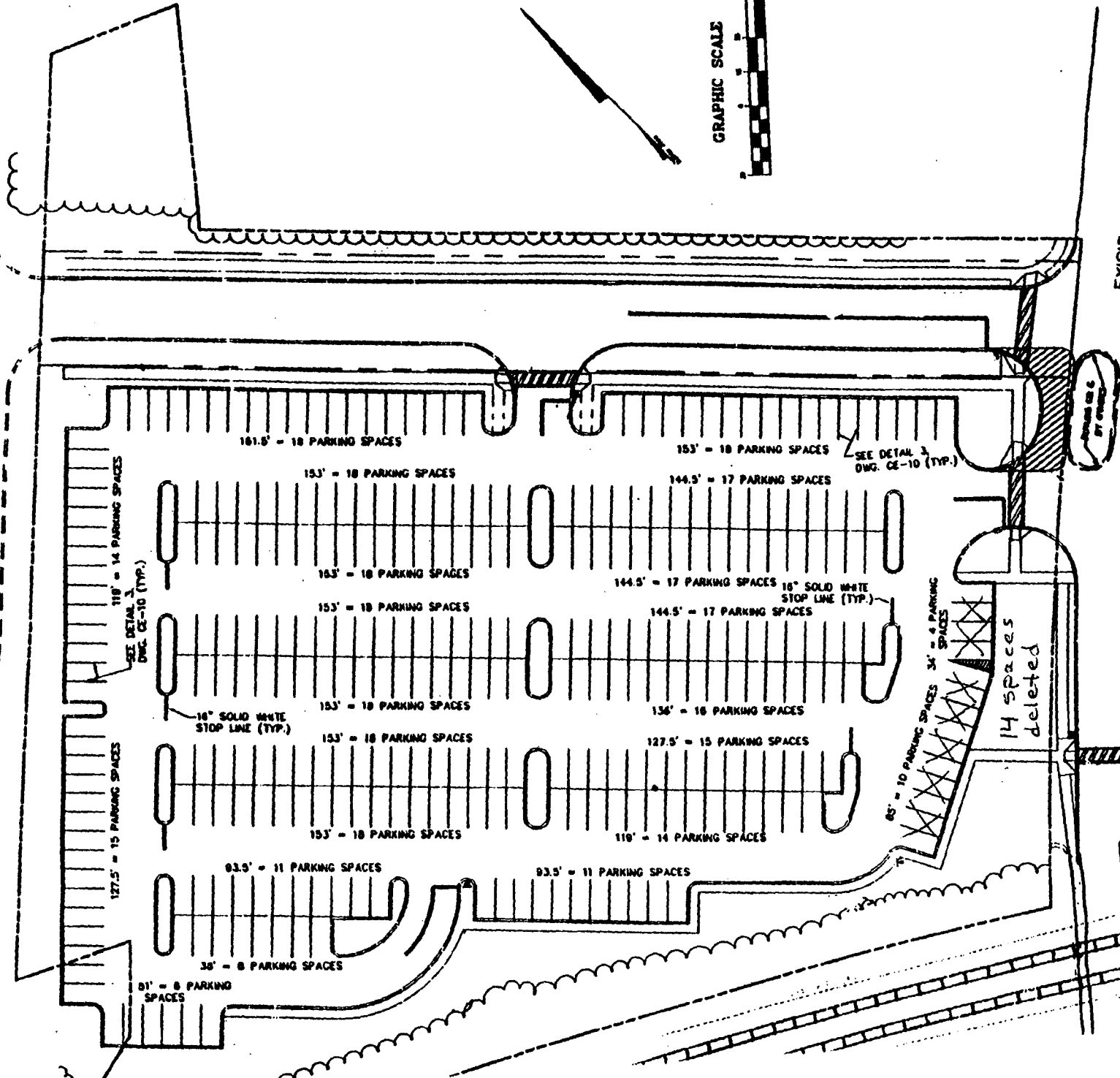


Figure 3

MD ROUTE 118 (RELOCATED)
BASELINE 7/7/80, MD 118 SEC 3, AT MD 3179

FUTURE 12' DECELERATION LANE



EXIST. MD. 118

CONCRETE WALK

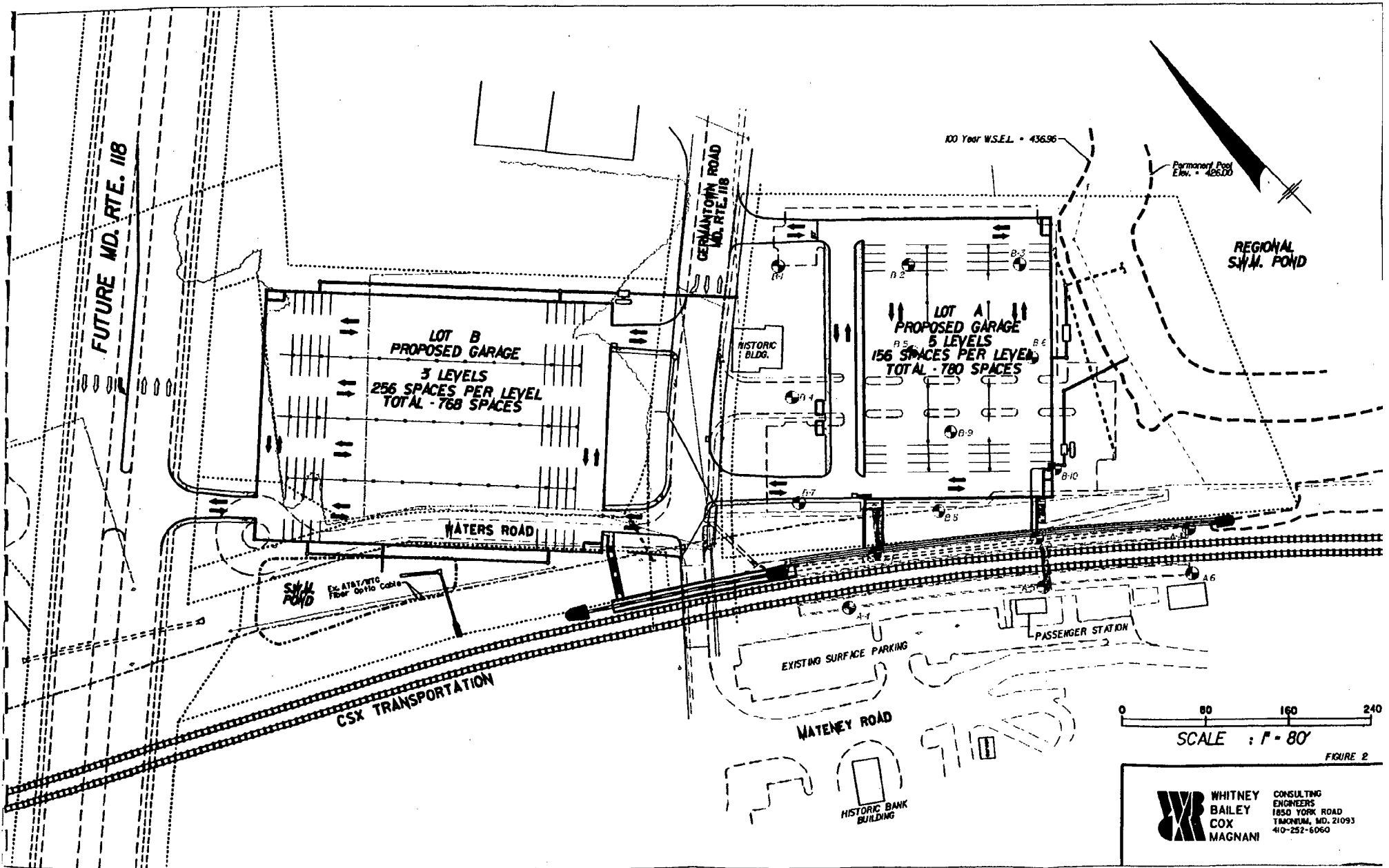
SECURED BANK

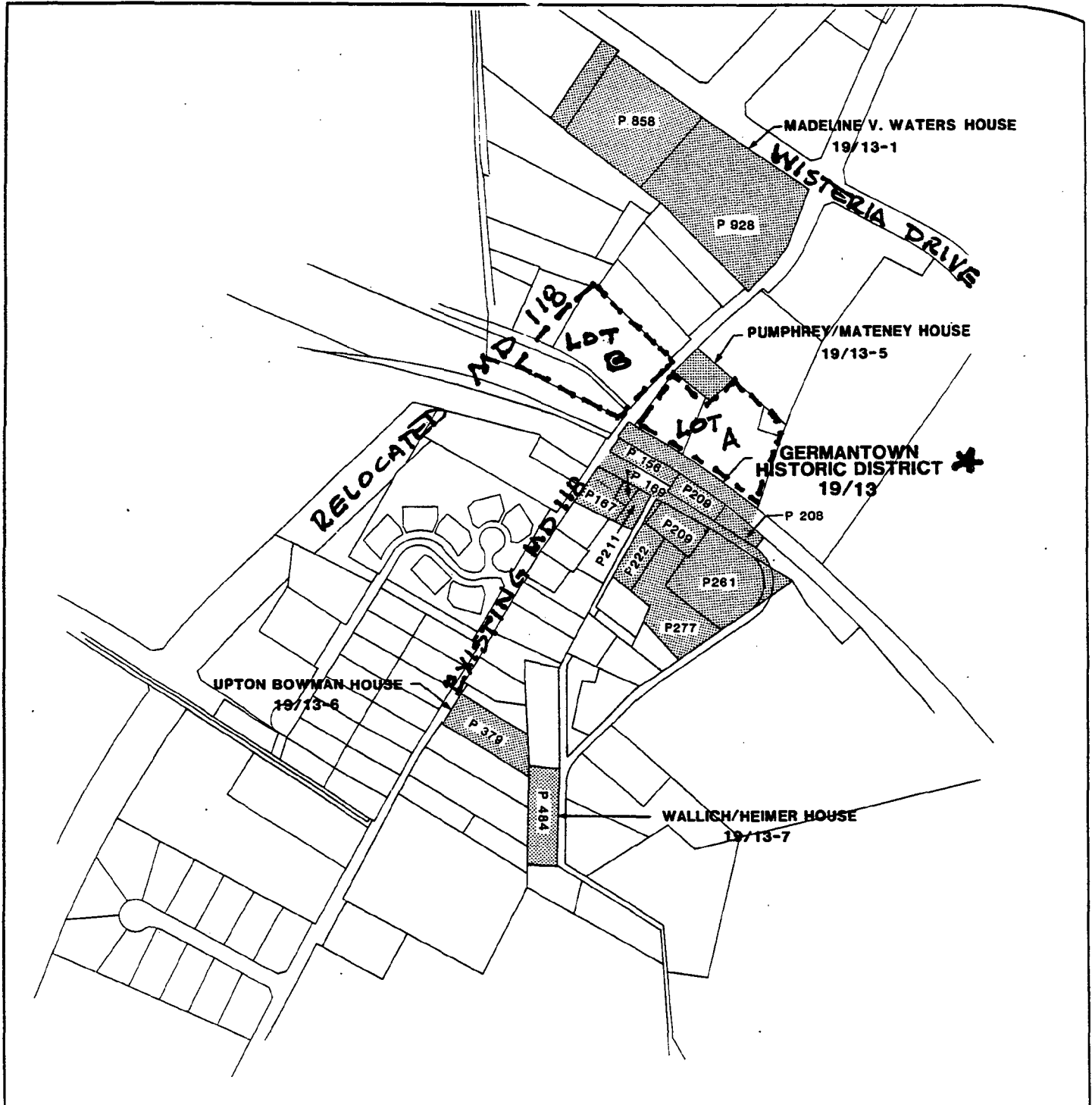
TO EXISTING
MAZE PARKING

SECURED BANK

CONCRETE WALK

Figure 4





Germantown Historic District
And Adjacent Historic Resources

 **Comprehensive Amendment
to the Master Plan for Germantown**
Montgomery County, Maryland
The Maryland-National Capital Park and Planning Commission

View from Germantown Historic District

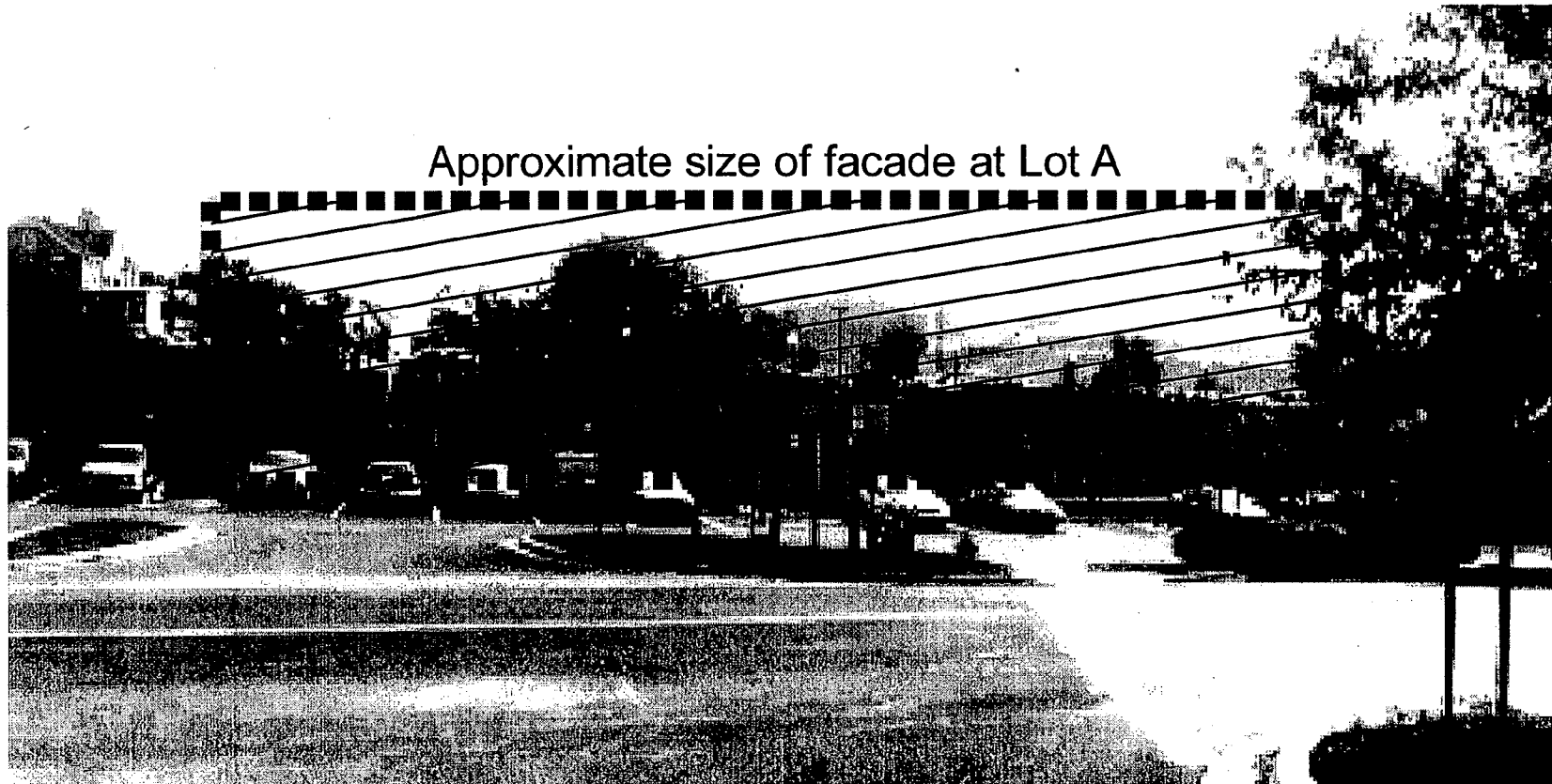


Figure 6

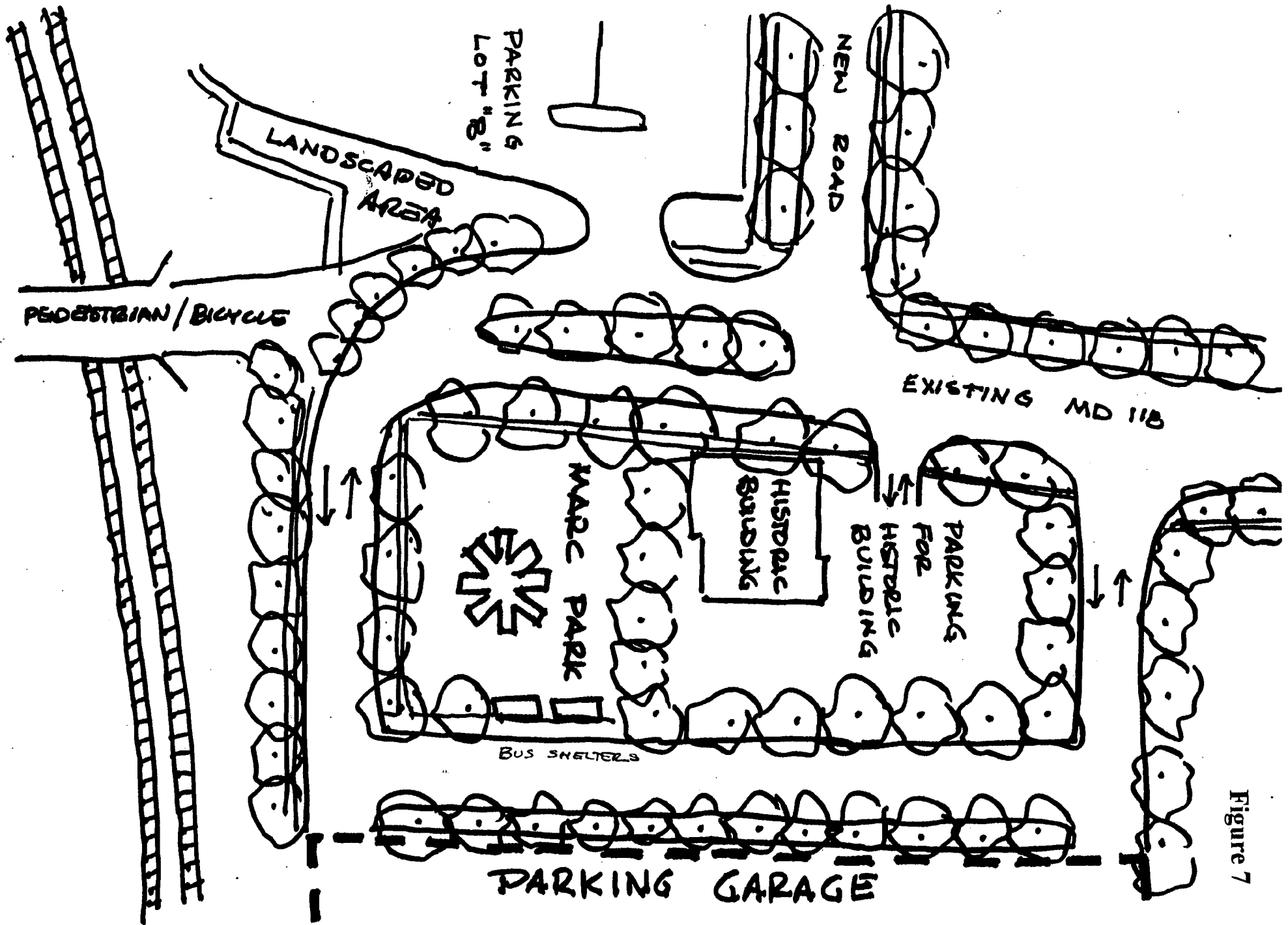


Figure 7

Table 1

ALTERNATIVE SITE ANALYSIS
GERMANTOWN PARKING GARAGE

SITE FACTORS	LOT 'A'	LOT 'B'
Vehicular Access	From existing MD Rte 118	May be able to have access from relocated MD 118
Pedestrian Access to Station Platform	Direct access from bottom level or elevator	Access via stairs to extended station platform (450' walk distance to boarding area)
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Stormwater Management Quantity	Existing regional SWM pond (no quantity management)	Modifications to existing regional pond or new pond required for quantity management
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Cost Comparison w/o Facade	\$16,472,000	\$14,062,000
Cost Comparison w/ Facade	\$17,599,000	\$15,858,000

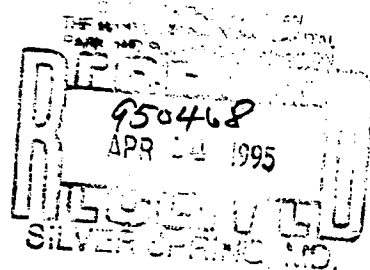


MARYLAND DEPARTMENT OF TRANSPORTATION

MASS TRANSIT ADMINISTRATION

300 West Lexington Street • Baltimore, Maryland 21201-3415

April 10, 1995



Mr. William H. Hussmann, Chairman
Maryland-National Capital Park and
Planning Commission
8787 Georgia Avenue
Silver Spring, MD 20910-3760

RE: GERMANTOWN PARKING LOT EXPANSION
CONTRACT NO. MTA-4316-103

Dear Mr. Hussmann:

The Secretary of Transportation, David Winstead and I would like to thank you for your letter dated March 6, 1995 concerning the above mentioned MARC Station's parking lot expansion and the commission's timely review of the project.

The Mass Transit Administration's (MTA) Office of Engineering has reviewed the recommendations and will:

- 1.) Incorporate within the plans a double row of trees along the site's frontage of Relocated MD. 118.
- 2.) Eliminate the fourteen (14) parking spaces adjacent to existing MD. 118 and add the area to the landscaped pedestrian area to be developed adjacent to the bridge over the railroad line.

The site selection for the Germantown parking garage and the platform improvements, which are two (2) independent projects, will be presented to the Planning Board at a future date.

The MTA will support and coordinate with the Montgomery County Department of Transportation as they develop their plans for the landscaped pedestrian/bicycle crossing over the railroad line at the existing MD. 118.

My phone number (410) 333-3885

FAX number (410) 333-3279

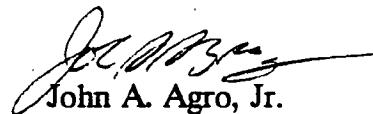
TDD for people who are hearing and/or speech impaired 539-3497



Mr. William H. Hussmann
April 10, 1995
Page Two

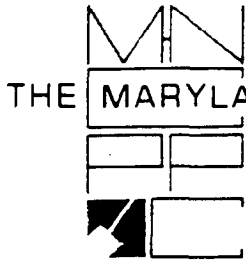
It has been a pleasure to work with the Planning Board and the MTA looks forward to working with the Planning Board on the future projects. If you have any questions, please do not hesitate to contact me.

Sincerely,


John A. Agro, Jr.
Administrator

JAA

cc: David L. Winstead
Secretary



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
8787 Georgia Avenue • Silver Spring, Maryland 20910-3760

March 6, 1995

Mr. John A. Agro, Jr.
Administrator
Maryland Mass Transit Administration
300 West Lexington Street
Baltimore, MD 21202-3415

RE: Germantown MARC Station

Dear Mr. Agro:

On Thursday, February 23, 1995 the Montgomery County Planning Board considered a Mandatory Referral review of the proposed parking lot expansion at the Germantown MARC station. Following its consideration of the staff report of February 17 and the addendum and revision of February 23 to the staff report, and discussion of the issues, the Planning Board voted to approve the staff recommendations as revised with two changes:

1. That the Mass Transit Administration (MTA) be responsible for all of the landscaping along the site's frontage with relocated MD 118, and
2. That the 14 parking spaces adjacent to existing MD 118 be deleted and that the area be added to the landscaped pedestrian area to be developed adjacent to the bridge over the railroad tracks.

Concerning the cost of the double row of trees along the frontage, the Planning Board policy is that developers provide the landscaping along road frontages if it is not already in place. Thus, MTA should provide the funds for all of the double row of trees along relocated MD 118.

The 1989 Germantown Master Plan recommends that the portion of the Town Center where the parking lot is located "be a quiet, green, pedestrian-oriented enclave." To achieve this recommendation, the Planning Board decided that the area of the 14 parking spaces adjacent to existing MD 118 should be added to the landscaped pedestrian area to be developed between Mateney Hill Road and the entrances to the existing and proposed parking lots.

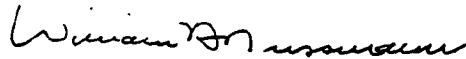
The Planning Board is looking forward to reviewing two additional Mandatory Referrals related to the Germantown MARC

station. One will be on the site selection study for the Germantown parking garage, the second will be on the platform improvements. It is anticipated that these items will be presented to the Planning Board within the next few months.

As noted in the staff report, MTA and the Montgomery County Department of Transportation (MCDOT) should begin developing plans for the landscaped pedestrian/bicycle crossing over the railroad tracks at existing MD 118. These plans will result in a separate project which will be reviewed as the plans near completion.

The Planning Board appreciates the high level of cooperation and communication between the staff of MTA, MCDOT, and the Planning Department. The quality of the proposed parking lot reviewed by the Planning Board reflects this interagency review.

Sincerely,

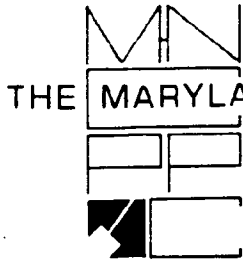


William H. Hussmann
Chairman

WHH:JOM:plb

cc: Graham Norton

David Winstead
Secretary of Transportation



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
8787 Georgia Avenue • Silver Spring, Maryland 20910-3760

February 17, 1995
REVISED: February 23, 1995

MEMORANDUM

TO: Montgomery County Planning Board

FROM: John Matthias for the Planning Department, 495-4569 *jm*

SUBJECT: Addendum and Revision to Staff Report on the Mandatory Referral of the Germantown MARC Station Parking Expansion

Staff met with staff of the Mass Transit Administration (MTA) and the Montgomery County Department of Transportation (MCDOT) and their consultants on Tuesday, February 21 to discuss issues related to this Mandatory Referral. Based on that discussion, staff has amended Condition 1 and deleted Condition 4 of the February 17 Staff Report.

Condition 1 as revised is as follows:

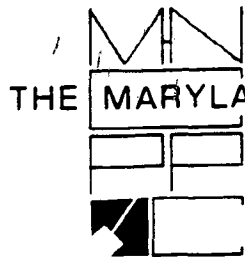
1. Coordinate with the State Highway Administration and the Montgomery County Department of Transportation (MCDOT) regarding grading and landscaping along relocated MD 118.

The proposed parking lot is anticipate to be completed one year before relocated MD 118 will be open to traffic. Therefore, the conversion of the portion of existing MD 118 crossing the railroad tracks into a pedestrian/bicycle path will need to be a separate project. This project will extend along the right-of-way of existing MD 118 from Mateney Hill Road to the entrances to the existing and proposed parking lots and be jointly funded by MTA and MCDOT. It will address the design and installation of appropriate paving materials, lighting, landscaping, and street furniture. The discussion of this project in Section 5 of the February 17 Staff Report is, therefore, not relevant to this Mandatory Referral. It does, however, provide initial guidance for the design plans for the separate project. The preparation of these plans should be initiated in the near future.

The closing of the bridge to motor vehicle traffic in that area should be done by the State Highway Administration as soon as relocated MD 118 is open to traffic. The bridge structure is deteriorating and should be closed to motor vehicle traffic as soon as possible. It is strong enough to accommodate pedestrian and bicycle traffic.

In regard to Condition 4 of the February 17 Staff Report, the provision of bicycle storage facilities was agreed to be addressed as part of the project on improvements to the station platforms. The plans for this project are being developed and will be brought to the Planning Board as a Mandatory Referral in approximately six months. The discussion regarding the bicycle storage facilities in Section 3 of the February 17 Staff Report should be used as guidance in the preparation of the plans for this future project.

JM:kcw



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
8787 Georgia Avenue • Silver Spring, Maryland 20910-3760

February 17, 1995

MEMORANDUM

TO: Montgomery County Planning Board
FROM: John Matthias for the Planning Department, 495-4569
SUBJECT: Mandatory Referral: Germantown MARC Station Parking Expansion

STAFF RECOMMENDATION

APPROVAL, subject to the following conditions:

1. Coordinate with the State Highway Administration and the Montgomery County Department of Transportation (MCDOT) regarding grading and landscaping along relocated MD 118 and the conversion into a pedestrian/bicycle path of a portion of existing MD 118.
2. Receive approval for the design and location of the public street along the northeast side of the site between existing MD 118 and relocated MD 118.
3. Abandon Waters Road (MD 120A) through the site.
4. Provide inverted "U" racks to accommodate 24 bicycles and lockers to accommodate 12 bicycles near the station platform with sufficient room for expansion.
5. Receive approval of the forest conservation plan by the Department of Natural Resources and identify and secure the off-site planting location prior to clearing and grading of the parking lot site.
6. Provide landscaping, lighting, and pedestrian amenities as noted in Section 5 of the Staff Report.

STAFF REPORT

Background

The Maryland Mass Transit Administration (MTA) plans to construct an additional surface parking lot at the Germantown MARC station. This facility is intended to provide adequate parking capacity until a parking garage is needed.

The proposed location is shown on Map 1. The site is a 3.6-acre area between existing MD 118 and relocated MD 118 just east of the CSX railroad tracks. Also indicated on this map are the existing parking lots on both sides of the station platforms. There are currently 348 spaces; the proposed lot will add 320, for a total of 668. Germantown is one of the most active stations on the MARC Brunswick line. Table 1 indicates the boardings and alightings as of September 29, 1994.

MTA intends using double-decker coaches on the Brunswick Line in 1997. These coaches will increase the capacity of the trains serving the Germantown station. Four additional trains in each direction will serve the Germantown station once service to Frederick, via Point-of-Rocks, is initiated. This service is anticipated to start in 1998.

The site is located in the TC-6 Analysis Area of the 1989 Germantown Master Plan. As stated on page 42 of the Plan, this portion of the Town Center of Germantown is recommended to "be a quiet, green, pedestrian-oriented enclave ... It should establish a strong visual and functional link to the Germantown Historic District directly across the railroad tracks."

In April 1992, the Germantown Town Center Design Study was approved by the Montgomery County Planning Board as the policy guide for staff in its review of development proposals in the Town Center. The Design Study establishes guidelines for landscaping, vehicular access, pedestrian movement, and other design elements.

Analysis of Proposal

1. Coordination With Relocated MD 118

The subject site abuts the southern edge of relocated MD 118. The grading and landscaping for these two projects need to be coordinated. In addition, the public road along the side of the site needs to be coordinated with MD 118 in terms of its location, the median break with left-turn storage lane for west-bound traffic, and the deceleration and, if necessary, acceleration lanes for east-bound traffic.

Both projects anticipate starting construction this year. The relative timing of the construction of both projects relates to the completion of the proposed public roadway and the work on existing MD 118. Once relocated MD 118 is open to traffic, existing MD 118

will be transferred to county control, and the bridge over the railroad tracks will be closed to motor vehicle traffic. The construction drawings of the proposed parking lot indicate a curb being installed along existing MD 118. This curb will not be necessary, as that portion of existing MD 118 will no longer carry motorized traffic. When that portion of existing MD 118 is closed to motor vehicle traffic, a curb connecting the existing and proposed parking lots should be installed. Additional work regarding paving, landscaping, and lighting will be needed when the portion of existing MD 118 is changed from a roadway to a pedestrian/bicycle path. Coordination is needed to determine which agency is responsible for these work items and when they will occur. Staff intends to continue to work toward a resolution of this issue prior to the Planning Board's consideration of this item.

2. Vehicular Circulation

In response to staff review and comments, the proposed site plan has addressed the recommendation of the Town Center Design Study to provide a public street connecting existing MD 118 to relocated MD 118 and extend across into Analysis Area TC-5. This connection is indicated on Map 2.

The new road's location along the eastern side of the site, farthest from the railroad tracks, maximizes sight-distance at its intersection with relocated MD 118. In addition, sufficient length is provided for the stacking of eastbound vehicles turning right to get to the existing and proposed parking facilities on the east side of the tracks. The location of the intersection of the proposed road with relocated MD 118 is required by the State Highway Administration (SHA).

Another advantage of shifting access between existing and relocated MD 118 to a location at the eastern edge of the site is to allow the site to be a possible location for the proposed parking garage. The parking garage is a separate project and the site selection study for this project will be presented to the Planning Board as a mandatory referral.

The proposed road requires approval of the MCDOT as it will be a public roadway. In order to accommodate the roadway and still provide the needed number of parking spaces, a roadway with a narrower right-of-way and paving width than normally required is needed. Staff recommends approval by MCDOT of the proposed two-lane roadway without parking. This roadway, with 26 feet of pavement in a 50-foot right-of-way, is appropriate in this instance, but a waiver of the right-of-way and cross section is required from MCDOT in addition to the standard review of grade, profile, and traffic control. Once the proposed roadway is approved, the design plans for relocated MD 118 will need to be revised to include:

- the new roadway intersection and median break,
- left turn lane for west-bound traffic,

- right-turn deceleration lane for east-bound traffic, and
- an acceleration lane from the proposed street onto east-bound relocated MD 118, if necessary.

There will be two entrances into the proposed parking lot -- one from the new street and one from existing MD 118 as shown on Map 2. Commuters will be entering and leaving the parking areas in peaks related to the departure and arrival of MARC trains. However, the heaviest traffic demand will occur earlier in the morning and later in the evening than for those commuting to work in Germantown

The portion of Waters Road (MD 120A) through the site will need to be abandoned. The abandonment is proposed to be done by SHA when it abandons the portion of the right-of-way across relocated MD 118. MTA will need to petition for the abandonment if the relocated MD 118 project is not initiated in time.

3. Pedestrian and Bicycle Circulation

When relocated MD 118 is open to traffic, the existing bridge across the railroad tracks will be closed to motor vehicle traffic in accordance with the recommendations of the 1989 Germantown Master Plan. The existing bridge will only be used for pedestrian and bicycle traffic; bollards or other barriers will be placed on either side of the existing bridge to prevent motor vehicle traffic. When the bridge is closed to motor vehicle traffic, those who park in the proposed parking lot can reach the platforms by two routes:

- walk across the abandoned portion of existing MD 118 and the existing parking lot, and down the stairs or ramp; or
- walk across the bridge and down Mateney Hill Road.

The handicap parking requirement for the proposed parking lot will be provided in the existing west side lot as it is closest to the platforms and is at the same grade level.

MTA is preparing plans to enlarge the covered waiting area at the MARC station. This may well be a covered canopy along the southbound platform. This expansion is in conjunction with the relocation of the ticket agent from the historic bank building to the station building. This project will also be presented to the Planning Board as a Mandatory Referral.

No bicycle racks or lockers currently exist at the Germantown MARC station. MTA should provide inverted "U" racks for 24 bicycles as well as lockers for 12 bicycles at the Germantown MARC station for those commuters who travel to and from the station by bicycle. These should be near the platforms at a location with sufficient room for expansion. Staff has noticed on several occasions bicyclists chaining their bicycles to the hand rail along the platform.

4. Forest Conservation

As noted in the memorandum of February 7 from Cathy Conlon of the Environmental Planning Division (Attachment 1), the forest conservation plan for this facility will be reviewed and approved by the Maryland Department of Natural Resources (DNR). The proposed forest conservation plan meets the requirements of the county forest conservation law, but a revision to the forest conservation worksheet is recommended as noted in the memorandum.

The plan proposes clearing of the existing forest and reforestation off-site at a location approved by the DNR. Staff recommends that the location of the reforestation should be in Germantown on a site as close as possible to the proposed parking lot.

5. Landscaping, Lighting and Pedestrian Amenities

The Germantown Town Center Design Study recommends the following guidelines:

- Existing MD 118 and the proposed new road: Both of these streets are indicated as being "special streets" in the Design Study. On page 90, the Design Study notes that "old MD 118 will incorporate street trees, lighting, and other street furniture that extend the historic character of the recently constructed MARC Commuter Rail Station and the Historic District."

- Relocated MD 118: The Design Study recommends that a landscaped gateway be established on relocated MD 118 just east of the railroad tracks. The character of the gateway is established by a double row of ornamental trees on each side of the roadway and a single row in the median. The landscaped trees are to extend 100 feet back from the end of the bridge.

For that portion of the site further than 100 feet from the end of the bridge, the proposed streetscape for relocated MD 118 has a formal pattern of street trees, sidewalks, and bike paths. Specific guidelines regarding the spacing and locations of the street trees and sidewalk/bike path are on page 92 of the Design Study.

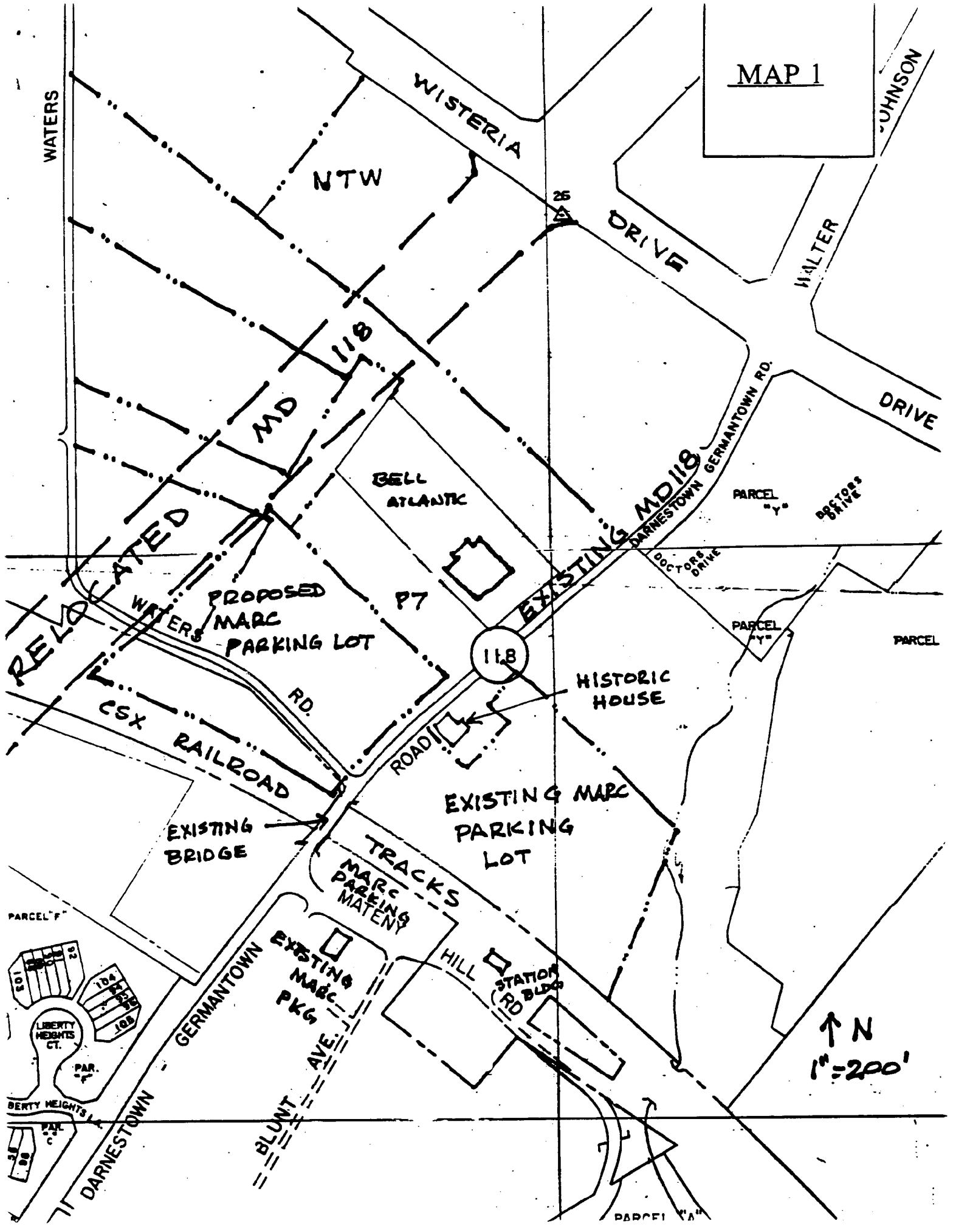
Based on these guidelines, staff recommends:

- provide Washington Globe or similar lighting throughout the proposed parking lot and along existing MD 118 and the proposed new road; this lighting is consistent with that at the other parking area as well as the station area,
- contribute one-half the cost of providing a double row of ornamental trees extending 100 feet east from the end of the bridge of relocated MD 118; the species, size, and spacing of these trees to be consistent with the landscaping plan for relocated MD 118,

- contribute one-half the cost of providing a double row of 3- to 4-inch caliper Red Oaks along the remaining portion of relocated MD 118 adjacent to the site,
- provide a low hedge or wall along the edge of the proposed parking lot abutting MD 118 and existing MD 118,
- provide street trees spaced 30 feet apart on both sides of existing MD 118, (the size and species of which are to be approved by staff),
- provide special paving such as brick or concrete pavers on a suitable concrete base on sidewalks along both sides of existing MD 118, including the frontage of the historic house, as well as between the proposed and existing parking lots,
- replace the portion of existing MD 118 which will be closed to vehicular traffic with special paving,
- provide shade trees in the parking lot islands and around the perimeter of the proposed parking lot (the size and species of which are to be approved by staff).

JM:kcw/marcstn.mmo

MAP 1



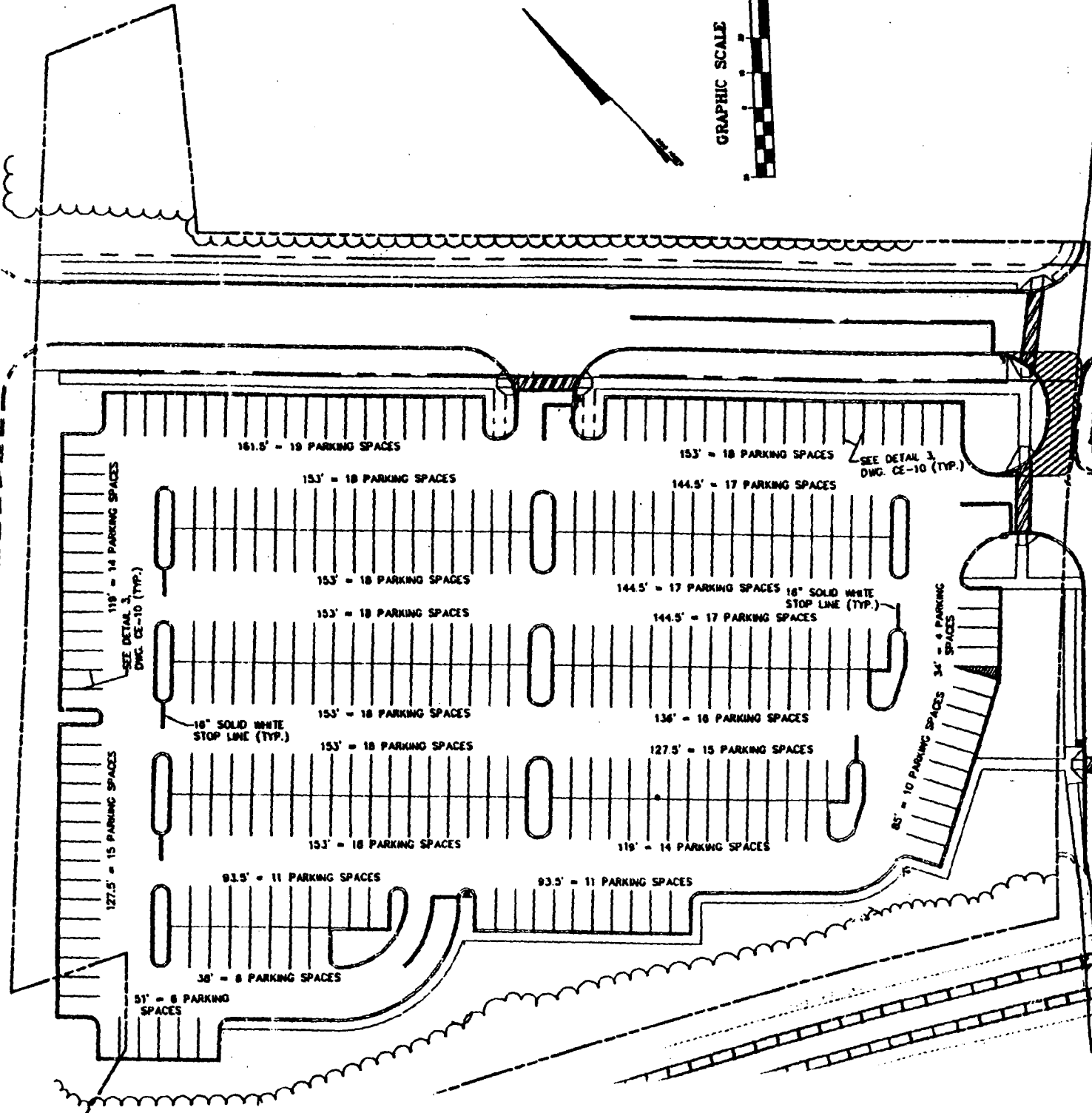
↑ N
1" = 200'

MAP 2

MD ROUTE 118 (RELOCATED)

BASELINE P/W REL. TO 118 S.R.C. PLAT NO. 3179

FUTURE 12' DECELERATION LAKE



EXIST. MD. 118

CONCRETE WALL

CONCRETE WALL

TO EXISTING
M&E PARKING

CONCRETE WALL

CONCRETE WALL



16" SOLID WHITE
STOP LINE (TYP.)

85' = 10 PARKING SPACES
34' = 4 PARKING SPACES

SEE DETAIL 3
DWG. CE-10 (TYP.)

16" SOLID WHITE
STOP LINE (TYP.)

51' = 8 PARKING SPACES

127.5' = 15 PARKING SPACES

93.5' = 11 PARKING SPACES

93.5' = 11 PARKING SPACES

153' = 18 PARKING SPACES

119' = 14 PARKING SPACES

153' = 18 PARKING SPACES

127.5' = 15 PARKING SPACES

153' = 18 PARKING SPACES

136' = 16 PARKING SPACES

153' = 18 PARKING SPACES

144.5' = 17 PARKING SPACES

153' = 18 PARKING SPACES

153' = 18 PARKING SPACES

144.5' = 17 PARKING SPACES

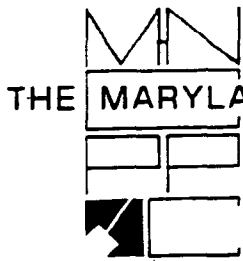
161.5' = 19 PARKING SPACES

144.5' = 17 PARKING SPACES

TABLE 1

MARC STATIONS IN MONTGOMERY COUNTY
(September 29, 1994)

STATION	AM BOARDINGS AND ALIGHTINGS
DICKERSON	10
BARNESVILLE	58
BOYDS	7
GERMANTOWN	472
METROPOLITAN GROVE	178
GAITHERSBURG	342
WASHINGTON GROVE	25
ROCKVILLE	314
GARRETT PARK	55
KENSINGTON	107
SILVER SPRING	323




THE MARYLAND-NATIONAL CAPITAL PARK AND PL

8787 Georgia Avenue • Silver Spring, Maryland 20910-3760

February 7, 1995

MEMORANDUM

TO: John Matthias
Transportation Planning Division

FROM: Cathy Conlon 
Environmental Planning Division

SUBJECT: Mandatory Referral on Germantown MARC Station Parking Lot Expansion

Environmental Planning Division staff have reviewed the above-referenced plan. The property is a 3.6 acre wooded site located on the ridgeline between the Little Seneca Creek (Use III-P) and Great Seneca Creek (Use I-P) watersheds.

Forest conservation is the primary environmental issue related to development of this site. Since MTA is part of a state agency, the forest conservation plan will be reviewed and approved by the Maryland Department of Natural Resources. Approximately two-thirds of the site is currently forested. The trees include black cherry, black walnut, mulberry and silver maple. Most of them are in the 6-10" size class, but there are also some scattered specimen size trees (> 30"). Preservation of the specimen trees is not possible given their location in the middle of the proposed parking lot and within the right-of-way for relocated MD Route 118. There are no environmentally sensitive areas on this site which would make the forest high priority for preservation.

The forest conservation plan proposes clearing of the existing forest and reforestation off-site. This proposal meets the requirements of the county forest conservation law, but staff feels that a revision to the forest conservation worksheet is necessary. According to the applicant's calculations credit has been given for retention of 0.16 acres of forest on-site, however, the area of trees which would be left between the parking lot and the railroad tracks is not forest. The area is both too small and too narrow to meet the definition of a forest. Staff recommends that the calculations be revised to reflect the loss of all of the existing forest on-site. This would result in a total of 1.8 acres of off-site reforestation rather than the 1.5 acres proposed. We will be recommending to the state that they require this change.

An off-site planting location must be identified and secured by MTA prior to the start of clearing and grading for the parking lot. The planting site should be in Germantown, preferably on a site as close as possible to the proposed parking lot.

Proposed grading associated with the parking lot will route all stormwater runoff to

Great Seneca Creek via an existing stormwater management facility. Water quality pre-treatment is provided by a proposed sand filter.

CAC:cc

***Germantown Parking Garage
Alternative Site Analysis Report***

Submitted to:
Maryland Department of Transportation
Mass Transit Administration

June 30, 1994

Prepared by:

Whitney, Bailey, Cox & Magnani
1850 York Road
Timonium, Maryland 21093

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(1)

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ALTERNATIVE SITE ANALYSIS REPORT

GERMANTOWN PARKING GARAGE

A. INTRODUCTION

The purpose of this Alternative Site Analysis is to document the preliminary analysis conducted for the potential construction of a parking garage along existing MD Route 118 at the CSX Railroad crossing. This area is the site of an existing MARC train station (see Figure 1). Two potential sites were analyzed. The first site is in the northeast quadrant of the intersection between the CSX Railroad and MD Route 118 and the second site is in the northwest quadrant. The first site will hereafter be referred to as Lot 'A' and the second site as Lot 'B' (see Figure 2). The report addresses the following topics:

- Existing Site Conditions
- Zoning and Historic Requirements
- Site Options & Utility Impacts
- Structural Options
- Geotechnical Data
- Traffic Analysis
- Construction Cost

Through discussions with Mr. Edward Daniel, Special Assistant to the Director of the Montgomery County Department of Transportation, it is apparent that only two parking levels above ground will be acceptable on either lot due to historic considerations and community acceptance. Therefore, this study focused on maximizing the number of spaces that can be obtained based on this requirement. This report addresses the findings of that study.

B. EXISTING SITE CONDITIONS

Lot 'A' is the site of an existing park-n-ride facility. The lot slopes downward from MD Route 118 at approximately a 5.5% grade and is situated 15 to 20 feet higher than the railroad tracks. The park-n-ride is a 375-car facility with approximate dimensions of 220 feet by 290 feet. In the northwest corner of the lot is a historic house which is currently being used as a commercial office. When the lot for the park-n-ride facility was purchased by Montgomery County, this house was restored by the County and leased for 99 years. East of the park-n-ride facility is an existing regional stormwater management pond owned by Montgomery County.

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Lot 'B' is a wooded tract of land approximately 230 feet by 260 feet. The land slopes at approximately 4% from north to south. Immediately to the south of the wooded lot, between the lot and the railroad is an existing road (Waters Road). This road is owned and maintained by the State Highway Administration. Between Waters Road and the railroad is a tract of land varying in width from 35 feet to 105 feet which is currently owned by the CSX railroad. Montgomery County is in the process of purchasing the wooded lot. The tract of land between Waters Road and the railroad is to be purchased by the MTA, and the two sites together with the Waters Road right-of-way will be the site of a 300-space MARC parking lot to be constructed by the MTA. MD Route 118 is proposed to be relocated west of the wooded lot. Since the MD Route 118 relocation will sever Waters Road, this section of roadway will not serve any properties. SHA District Engineer, Creston Mills confirmed that this portion of roadway can be abandoned. Projected advertisement date for MD Route 118 is May, 1995.

The Germantown Master Plan, prepared by the Maryland-National Capital Park and Planning Commission (MNCPPC), approved and adopted in July 1989, states, "When Relocated MD Route 118 is open to traffic, the existing bridge will be either closed to automobile traffic or replaced by a pedestrian bridge." Conversations with SHA indicated that, part of the overall plan is to transfer the Old MD Route 118 to the County when the relocation of MD Route 118 is complete. Therefore, it will be a County decision on closing the bridge.

Access to Lot 'B' from Relocated MD Route 118 is currently under review by SHA Access Permits Division. Approval for a median break in Relocated MD Route 118 must be approved by the Chief Engineer.

C. ZONING AND HISTORIC REQUIREMENTS

There is a locally-designated historic district in Germantown (County-designated) located southeast of the CSX Railroad and MD Route 118 crossing. It is the one area in Germantown with an intact ensemble of historic resources that recall an overall historic ambiance. It is the heart of the late 19th Century-early 20th Century Germantown, and the Germantown Master Plan recommends that the area be preserved.

The Germantown Master Plan also recommends that

"...any subdivision or site plan in the areas bordering on and adjacent to the historic district be given careful consideration in terms of its impact on the historic district. In addition, more detailed consideration of the buffering issue is needed, and the development of a "buffer" zoning

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classification or overlay zone, to provide the necessary design guidelines and review, may be warranted in the future."

In addition to the Germantown Historic District, there are individually designated historic structures (resources) outside the historic district, yet still in the Germantown area. A number of these structures are located along the Maryland Route 118 corridor, and any future development adjacent to these historic houses should respect these individually-designated houses.

Both lots 'A' and 'B' are zoned R-200. Normally, a parking garage requires a special exception for the use on property zoned R-200. However, since the facility will be owned and operated by a public entity, no special exception is required. The use still must be reviewed by the appropriate departments via the mandatory referral process. There is no checklist per se for this review process. The requirement is that plans be submitted to the Chairman of the Montgomery County Planning Board, William Hussman. In sixty days or less, plans will be reviewed by the Planning Board at a public hearing. The review of plans will be supervised by the Transportation Division in the MNCPPC. This department will distribute the plans to other departments for comment, including the County's Historic Preservation Commission.

With respect to zoning restrictions, the minimum building setback from the street is forty feet. Setbacks from adjoining lots areas follows: twelve feet on one side (provided the sum of both sides is not less than 25 feet), and the rear is 30 feet (see Figure 3). Since the garage will only be two levels above grade, the minimum setback requirements stated above apply.

The maximum percentage of net lot area that may be covered by buildings, including accessory buildings, is 25 percent. Mr. Edward Daniel does not know if this criteria can be waived by the County; but if so, the County would waive the maximum 25-percent lot coverage, but would want to adhere to the setback criteria. In a telephone conversation with Ms. Denise Boswell of MNCPPC, she stated that MTA would need to petition the Board of Appeals for a variance from the maximum 25-percent lot coverage requirement. This applies even if the garage is publicly owned and operated. The MTA must prove that the existing zoning imposes a hardship.

D. SITE OPTIONS

1. Lot 'A'

To maximize the number of spaces based on the maximum two levels above ground restriction, the option of constructing two levels below ground was studied. The lower level would be at the same approximate elevation (Elev. 430) as the railroad station platform providing direct access to the station platform. This elevation is above the permanent water surface elevation (Elev. 426) of the existing regional stormwater management pond. This option would provide a five-level garage with two of the levels below ground. The maximum garage size that can be constructed on this lot based on minimum setback requirement is 182 feet by 272 feet (see Figure 4).

Construction of the parking garage on Lot 'A' would displace the entire existing park-n-ride facility. Handicap parking would need to be provided in the garage. A kiss-n-ride and bus stop area could be provided along the west side of the garage adjacent to the historic building. Access to the stormwater management facility would be provided by access around the north and east sides of the parking garage. Two fiber optic lines are located along the southwest side of the lot adjacent to the railroad. These lines would require relocation.

This garage would provide 156 spaces per level, or a total of 780 parking spaces.

2. Lot 'B'

Using minimum setback requirements, a garage size of 242 feet by 326 feet can be obtained using the wooded lot, Waters Road, and a portion of the lot that MTA is purchasing from the CSX railroad. Based on only two levels above ground and one at grade, a total of 768 parking spaces can be provided in this garage, 256 spaces per level (see Figure 5).

On this site, the train platforms may be extended to the west under the existing MD Route 118 bridge with new stairs from the garage site for more direct access. Without the platform extension, the walking distance to the existing platforms would be 450 feet.

The lot adjacent to the railroad is much wider on the west end (105 feet) versus 35 feet at the east end.

The option of using an additional portion of this lot at the west end was studied, however, was found not to be cost-effective because very few additional spaces would be provided.

The construction of a garage on Lot 'B' would require the relocation of several utility poles along MD Route 118. There are two also fiber optic cables parallel to the railroad. Although it appears that the garage itself would not impact the cables, construction of the stairs and stormwater management pond would affect the cables.

The top floor elevations for either garage (Lot 'A' or Lot 'B'), with only two levels above ground, would be lower than the height of the existing historic house (see Figure 6).

E. Stormwater Management/Environmental Requirements

Environmental design requirements, as outlined by the Montgomery County Department of Environmental Protection, generally apply to parking garage facilities constructed within Montgomery County. The proposed garage locations considered in this feasibility study will require that the following environmental systems be incorporated in the design of the parking garage facility, regardless of locations.

1. Top Parking Deck

The top deck of the parking garage is considered analogous to an on-grade parking lot, having the same surface area, subject to stormwater quality control. Therefore, all stormwater drainage associated with the uppermost or top deck must be conveyed to a sandfilter water quality structure (SFWQ) with outfall connections to the project's stormwater management facility. Design criteria for design of the SFWQ is based on a peak discharge for a 15-year storm event, and storage for the first flush runoff of 0.5 inches over the top deck surface.

2. Below Top Deck Parking Levels

Parking lot levels below the top deck are not subject to drainage from stormwater events. These parking levels will have their drainage systems piped from floor drains, with an outfall connection to the nearest available public sanitary sewer system preceded by an on-site oil interceptor with separate waste oil holding

tank. Vehicular access to the oil interceptor and the underground holding tank should be provided for routine maintenance services. Preliminary sizing of the oil interceptor is based on 35 gpm, which should adequate for either structure.

For Lot 'A', quantity stormwater management would not be required since this site drains to the existing regional stormwater management pond today.

The stormwater management requirements for Lot 'B' were studied. The existing runoff flows toward the railroad tracks and under the northeast platform via a storm drain, and does not enter the existing regional stormwater management pond. Using computations for the existing stormwater management pond obtained from Montgomery County, the proposed lot drainage was routed into the pond. Adding this additional drainage into the pond increased the release rate for the 2-year storm by 7.66 cfs, or a 36 percent increase. The 10-year storm release rate increased by 23.80 cfs, or 7 percent increase, and the 100-year storm release rate increased 21.12 cfs, or 3 percent. During a telephone conversation with Mr. Mike Geier of Montgomery County Environmental Protection, he indicated that the increase in the 2-year storm release rate was unacceptable. Modifications to the release structure, which could lower the 2-year release rate, should be examined during final design; however, Mr. Geier indicated that on-site stormwater management of Lot 'B' may be preferable. Analysis of this site indicated that a surface stormwater management pond could be constructed on the southwest corner of the lot; however, this location of the pond would require relocation of the fiber optic cables owned by AT&T and Wiltel Communications. In lieu of the surface lot, an underground stormwater management structure could be constructed to avoid relocation of the fiber optic cables.

F. STRUCTURAL OPTIONS

Structural systems that were considered include precast concrete and cast-in-place concrete. We recommend a precast concrete system based on the following advantages over cast-in-place concrete:

- Lower cost per square foot
- Minimize user inconvenience
- Better quality control of finished product
- More suitable to expansion or modification.

Even though neither site is within the Historic District, the garage will be subject to review by the Montgomery

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County Planning Board, which will consider the impacts of the garage on the historic district. Ms. Nancy Witherall, of the Design Zoning and Preservation Division of the Maryland National Capital Parks and Planning Commission, has indicated that the garage may not need to be enclosed; however it should be attractive and appropriate. Therefore, the options of an open structure or enclosed structure were studied.

The open garage would consist of a structure with parapet/barriers designed as part of the spandrel beam system. The open structure will maximize the benefits of natural lighting and ventilation. The spandrel beams could be designed to accommodate a brick veneer system. Using a brick veneer on the spandrels and at the stair towers, in combination with sloped roofs over the stair towers, may be adequate to provide an attractive facade for the reviewing agencies. Should the reviewing agencies require one or two completely closed facades, the code will allow two closed sides and still consider the structure an open garage. Should the reviewing agencies require that all sides of the garage be enclosed by the facade, then the structure would be classified as a public garage. This classification has more stringent requirements for fire protection, ventilation, lighting and life safety.

Two-way and one-way traffic patterns were considered within the garages. We recommend a two-way pattern for the following reasons:

- Allows more options for the driver and quicker parking access.
- Does not require interconnections between ramps.
- Allows simpler, less costly structural system.

Parking stalls will be perpendicular since diagonal parking is a less effective parking configuration.

The structural system would consist of precast, prestressed concrete double tees spanning between precast concrete girders that are supported by precast concrete columns. The foundations and ground floor would be cast-in-place concrete. The typical framing bay would be 60 feet by 27 feet. This bay can accommodate three 8½-foot wide parking spaces at each end with a 20 foot wide lane in the middle. The garage sizes will be based upon multiples of this bay size. The depth of the structural system would be approximately 3 feet and the recommended clearance is 7 feet. Therefore, the floor-to-floor or story height used in this study is 10 feet. The at-grade floor on Lot 'A' would provide a 9-foot clearance to accommodate handicap vans.

Based upon the lot sizes and the set-back requirements, the garage footprint at Lot 'A' would be 182 feet wide and 272 feet long (see Figure 7). At Lot 'B', the garage footprint would be 242 feet wide by 326 feet long (see Figure 8).

The garage footprint at Lot 'A' will accommodate 156 parking spaces per floor. With five parking levels, 780 spaces would be provided. It was decided to place two levels below grade to take advantage of the grade differential adjacent to the railroad tracks. The remaining three levels would be at or above grade and the building height would be approximately 23 feet, except that the stair and elevator towers would project above the uppermost parking deck. The lowest parking level would be nearly the same elevation as the station platform level and gives the owner the opportunity to create a plaza area with landscaping, kiosks and seating adjacent to the tracks.

The garage footprint at Lot 'B' will accommodate 256 parking spaces per floor. With three parking levels, 768 spaces would be provided. The entire parking structure would be at or above grade and the building height would be approximately 23 feet, except that the stair towers would project above the uppermost parking deck.

The building code for open parking structures requires provision of a minimal amount of lighting. Fire protection requirements would consist of fire hose stations at certain intervals within the garage. The underground parking levels at Lot 'A' require sprinklers, ventilation and higher lighting levels.

Should the reviewing agencies require a completely enclosed structure, the garage would need complete sprinkler protection, higher lighting levels and be completely ventilated.

Life safety requirements in the building code for open garages prescribe a minimum of two exits with a maximum travel distance of 300 feet. The minimum of two stair towers will satisfy this criteria for both garages. Additionally, we are recommending that an elevator be provided for the garage on Lot 'A' due to the number of stories of the structure and to access the van handicap parking on the at-grade level.

Should an enclosed structure be required, the life safety requirements may be more stringent than that required for an open parking garage. These requirements would require a thorough code review during the final design, after the determination of the building classification is made.

The ADA requirements for parking structures of this size (501 to 1,000 parking spaces) mandates that 2 percent of the total number of spaces be handicap accessible. Based on the total number of spaces provided (780 or 768), 16 spaces would be required. The code does allow all of the spaces to be clustered in one area. The garage at Lot 'A' would accommodate the handicapped parking on the lowest parking level, which would permit easy access from the northbound platform without a major ramp system, except that the handicap vans would be restricted to the at-grade level. However, the handicap vans could be allowed through the lower parking levels if the story heights were increased by two feet. This change would require adjusting the grades around the building for the at-grade parking level. The costs to increase the story heights would be minimal. On Lot 'B', handicapped parking may not be required since an adequate number of spaces can be provided at the existing park-and-ride lot, which would be closer to the station platform. However, handicapped parking, except handicap vans, could easily be accommodated within the garage on Lot 'B'.

G. GEOTECHNICAL DATA

The sites are located in the Piedmont region of the State. This area is generally characterized by residual soils overlaying rock strata. The rock can generally be found 10 to 30 feet below the surface. The garages considered in this study will have column loads in the range of 800 to 1200 kips. The residual soils will generally have low (2 to 4 KSF) bearing capacities. The low capacities would require large spread footings should a shallow foundation system be selected. We recommend that a deep foundation system be utilized for the garages. This would consist of caissons or drilled piers that would bear on the rock strata. The rock would generally provide bearing capacities in the 10 to 12 KSF range.

Review of the shallow borings (5 to 10 feet) taken for the MD Route 118 extension indicate that the soils are generally sandy silt and silty clay. At one location, the rock strata was encountered at elevation 448.0 feet; however, this elevation is approximately 12 feet below the finished grade of the garage on Lot 'B'. This boring was approximately 600 feet northeast of Lot 'B'.

Examinations of the shallow borings (10 feet) taken for construction of the existing parking lot (Lot 'A') show that no rock was encountered. Rock excavation is not anticipated at the southwest end of the garage; however, there is a possibility for rock excavation at the northeast end of the

garage since the existing borings ended 10 feet above the proposed bottom floor.

Further geotechnical investigations will be required at the specific site locations during final design.

H. TRAFFIC ANALYSIS

A formal Traffic Impact Study was prepared by WBCM under separate cover for assessing traffic impacts associated with the various siting options of the proposed parking structure. Data presented herein represents a summary of the key findings.

Analyses of the siting options were based on non-site-generated traffic data developed by the Maryland National Capital Park and Planning Commission (MNCPPC). The projections reflect design-year 2003 conditions assuming MD 118 relocated complete and open to traffic as shown in Figure 9. The new roadway will attract through trips from existing MD 118 and reduce turning movements along Wisteria Drive. As a result, existing MD 118 north of Clopper Road would serve only local traffic. Included as components of the projected non-site traffic data developed by MNCPPC are regional traffic growth and site-specific local development activity. Trips destined for the proposed parking structure and the existing park-n-ride lots represent site-generated trips and were not included in the MNCPPC computations.

The year 2003 projections were reduced to year 2001 figures (the assumed design year of the parking structure) by WBCM for subsequent analyses.

Anticipated trip generation of the proposed parking structure was based on hourly traffic volume counts conducted at the driveway of existing Lot A. Based on current operations, local trip generation per parking space of the park-n-ride is as follows:

Trip Generation Rates

Germantown Park-n-Ride

PERIOD	ENTER	EXIT
AM Peak Hour	0.47	0.14
PM Peak Hour	0.16	0.58

14

14

Applying the trip generation factors to the proposed 800-space facility yields the following trip generation.

Trip Generation Summary
800-Space Parking Structure

PERIOD	TRIP GENERATION	
	ENTER	EXIT
A.M. Peak Hour	376	112
P.M. Peak Hour	128	464

Based on existing traffic dispersion, approximately 67 percent of the site-generated trips will be oriented to/from residential development south of the site. Trip attractions to the north along MD 118 account for 16 percent of the distribution; 11 percent is oriented to/from the west on Wisteria Drive and 6 percent to/from the east on Wisteria Drive. Design-year 2001 total traffic volumes were derived by adding the adjusted non-site traffic to the trips generated by the new facility and the existing park-n-ride lots.

Capacity analyses were conducted to determine impacts to the intersections of Wisteria Drive with relocated and existing MD 118 caused by the projected non-site traffic (traffic which would be on the roadway network even if the parking structure and other MTA lots were not present). Once potential capacity constraints were identified and measures determined to accommodate the non-site traffic, a second set of studies were conducted to assess the impacts of site-generated trips plus non-site traffic. Analyses were based on the design-year lane geometry with MD 118 relocated in place and open to traffic.

Non-site analyses indicate that the relocation of MD 118 and corresponding redistribution of through-traffic significantly reduces the turning movements along Wisteria Drive. As a result, projected increases in non-site through-traffic along Wisteria Drive can be accommodated by a reassignment of lane use within the existing four-lane section. However, improvements would be required along northbound, relocated MD 118 to provide a double left-turn lane at Wisteria Drive given anticipated queuing of non-site traffic in the evening peak hour. The additional turn lane

15

would be required even if the parking structure was not constructed.

Four access scenarios for the new parking structure were considered in assessing impacts with total traffic volumes (non-site plus MTA-generated trips). In each scenario, it was assumed that the new facility would be constructed to the west of existing MD 118 (Lot 'B'). A western siting offers the advantage of potential access to relocated MD 118. The additional access point would serve to alleviate potential congestion at local intersections by redistributing site-generated traffic directly to relocated MD 118. Access to relocated MD 118 could be provided with a Lot 'A' siting; however, it would be difficult to restrict the access from existing MD 118 to MTA patrons only. The surface lot would become a short-cut for non-site traffic accessing relocated MD 118--a major disadvantage.

The access scenarios considered in the analyses for Lot 'B' are noted below:

1. Existing MD 118 would remain open to vehicular traffic. Access to the new facility would be provided across from the existing northern park-n-ride lot on existing MD 118.
2. Existing MD 118 would be closed to vehicular traffic at the CSX railroad bridge between the existing park-n-ride lots. The existing CSX bridge would only serve pedestrian traffic. Access to the new facility would be provided across from the existing northern park-n-ride on existing MD 118.
3. Existing MD 118 would remain open to vehicular traffic. Site access would be provided on existing MD 118 across from northern park-n-ride lot and from relocated MD 118.
4. Existing MD 118 would be closed to vehicular traffic at the CSX bridge between the existing park-n-ride lots. Full access from relocated MD 118 and across from existing northern park-n-ride on existing MD 118 would be provided.

Results of the analyses for each scenario are noted below:

Scenario 1: Trips generated by the proposed facility can be accommodated by the design-year lane geometry assuming the northbound approach of relocated MD 118 at Wisteria Drive is improved to provide a double left-turn for non-site traffic as

noted above. The additional lane would be required even if the parking structure was not constructed. Widening of the narrow CSX bridge along existing MD 118, between the park-n-ride lots, and a widening of existing MD 118 would be required to provide a left-turn lane into the new facility, assuming a Lot 'B' siting, thus ensuring efficient traffic flow along northbound MD 118. Currently, the roadway widens north of the CSX bridge to provide a right-turn lane into the park-n-ride. The limited widening would not be sufficient to accommodate a left-turn lane; thus, the additional improvements would be required.

Scenario 2: As noted above, 67 percent of the site-generated trips are oriented to/from the south; therefore truncating existing MD 118 at the CSX bridge would cause redistribution of a major component of the site-generated trips. Turning movements would balloon to over 600 vehicles at the Wisteria Drive intersections necessitating the construction of a double left-turn lane westbound and an additional northbound right-turn lane to accommodate the potential queuing of site generated traffic. Also, an additional left-turn lane along northbound relocated MD 118 would be required at Wisteria Drive to accommodate projected non-site traffic. The additional lane would be required even if the parking structure was not constructed. An advantage of Scenario 2 is that the northbound left-turn lane and associated bridge repairs and roadway widening along existing MD 118 would not be required.

Scenario 3: Access to the proposed parking structure from relocated MD 118 would divert traffic from existing MD 118 and eliminate the need for constructing turn lanes along existing MD 118 at Wisteria Drive. All site generated traffic could be accommodated by the improved non-site roadway network. Widening along existing MD 118 for a left turn lane would be required to accommodate site traffic from the south; however, the length of widening would be reduced over that of Scenario 1. Also, the additional left-turn lane along northbound relocated MD 118 would be required at Wisteria Drive to accommodate projected non-site traffic. The additional lane would be required even if the proposed parking facility was not constructed.

Scenario 4: The proposed facility would be the primary trip generator in the MD 118 corridor;

therefore, access to relocated MD 118 would mitigate the majority of turning movement demands created by the discontinuity of existing MD 118. Widening of the CSX bridge and existing MD 118 would not be required. However, additional widening could be required along westbound Wisteria Drive to accommodate the right turn movement at relocated MD 118. Also, the additional left-turn lane along northbound relocated MD 118 would be required at Wisteria Drive to accommodate projected non-site traffic.

The preferred alternate is Scenario 4 given it provides for the dispersion of site-generated traffic, thus reducing impacts to individual intersections. Elimination of bridge and roadway widening along existing MD 118 to provide a left-turn lane at the site access is another advantage of this alternate. A summary of the traffic impacts of each access scenario are noted below.

Traffic Analysis Summary

SCENARIO NO.	TRAFFIC IMPACTS
1. Existing MD 118 open. No access to relocated MD 118.	<ul style="list-style-type: none"> • Left-turn lane required on existing MD 118 at site access • No off-site improvements required for site trips
2. Existing MD 118 closed. No access to relocated MD 118.	<ul style="list-style-type: none"> • No improvement at site access • Off-site improvements required for site trips
3. Existing MD 118 open. Access to relocated MD 118.	<ul style="list-style-type: none"> • Site access improvements reduced • No off-site improvements required for site trips
4. Existing MD 118 closed. Access to relocated MD 118	<ul style="list-style-type: none"> • No improvements at site access • No off-site improvements required for site trips

18

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I. CONSTRUCTION COST

The total estimated probable cost for Lot 'A' without a facade is \$16,472,000; and with a facade is \$17,599,000. For Lot 'B', the total estimated probable cost is \$14,062,000 without a facade; and \$15,858,000 with a facade. See Appendix 'A' for the cost breakdowns.

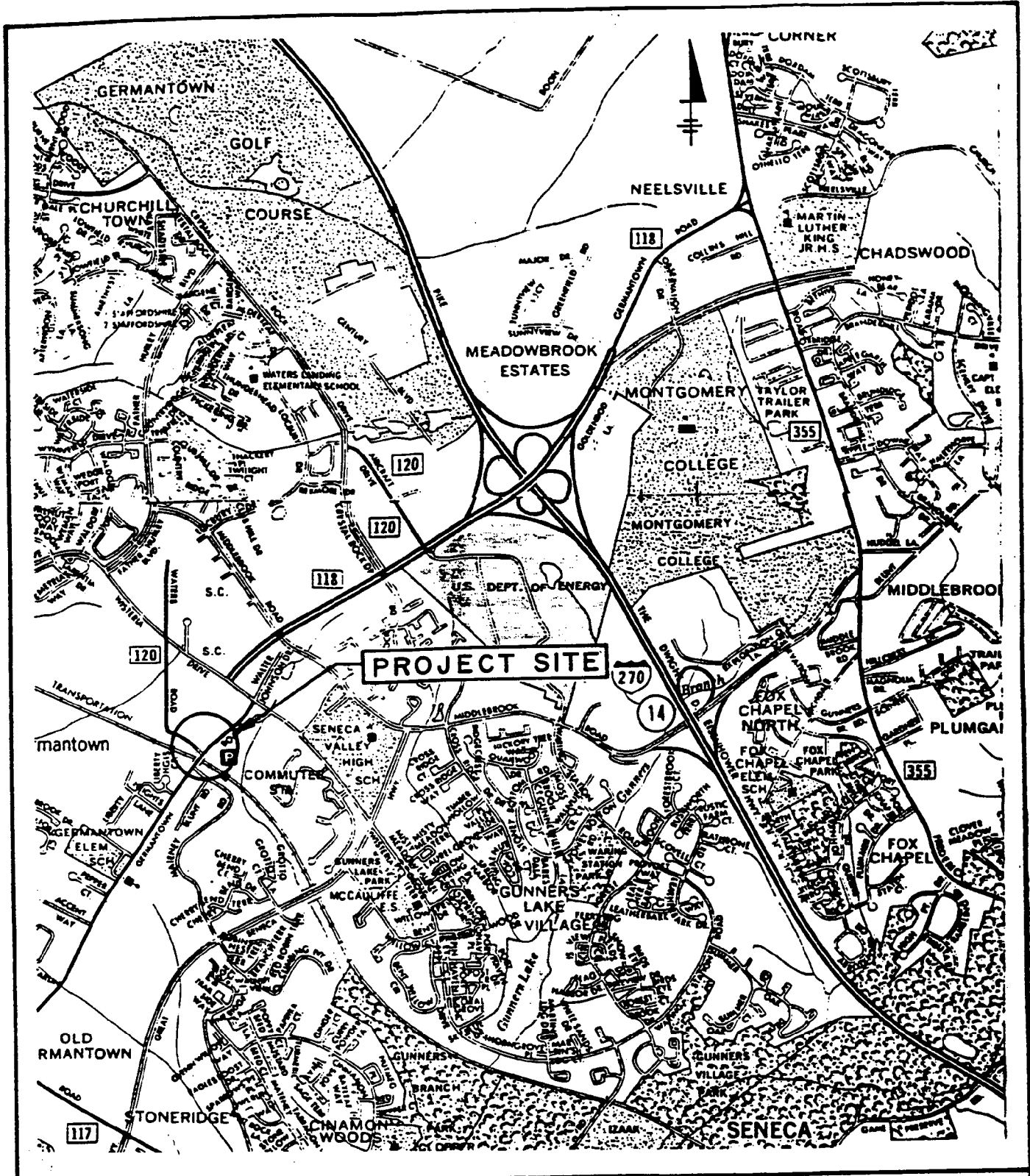
9310008R.001

**ALTERNATIVE SITE ANALYSIS
GERMANTOWN PARKING GARAGE**

SITE FACTORS	LOT 'A'	LOT 'B'
Vehicular Access	From existing MD Rte 118	May be able to have access from relocated MD 118
Pedestrian Access to Station Platform	Direct access from bottom level or elevator	Access via stairs to extended station platform (450' walk distance to boarding area)
Historic Impacts	Proposed garage located directly across from the historic district. Also historic building on same site (80' from proposed garage structure)	Visual impact to historic district a concern.
Displacement of Existing Parking	All existing parking (375 spaces will be displaced)	Existing parking on proposed surface lot will be displaced (300 spaces)
Stormwater Management Quantity	Existing regional SWM pond (no quantity management)	Modifications to existing regional pond or new pond required for quantity management
Stormwater Management Quality	Sand filter water quality structure and oil interceptor	Sand filter water quality structure and oil interceptor
Cost Comparison w/o Facade	\$16,472,000	\$14,062,000
Cost Comparison w/ Facade	\$17,599,000	\$15,858,000

(20)

(20)



WHITNEY
 BAILEY
 COX
 MAGNANI

LOCATION MAP (21)
 GERMANTOWN PARK-N-RIDE

FIGURE 1

22

22

FUTURE MD. RTE. 118

CSX TRANSPORTATION

SMH
R&M
REPLACEMENT

WATERS ROAD

LOT B
PROPOSED GARAGE
3 LEVELS
296 SPACES PER LEVEL
TOTAL - 768 SPACES

GERMANTOWN ROAD
MD. RTE. 118

HISTORIC
BLDG.

LOT A
PROPOSED GARAGE
5 LEVELS PER LEVEL
156 SPACES PER LEVEL
TOTAL - 780 SPACES

100 Year WSEL - 458.98

WATERS ROAD
EXISTING SURFACE PARKING
TOWER BANK
HIS BUILDING

PASSENGER STATION

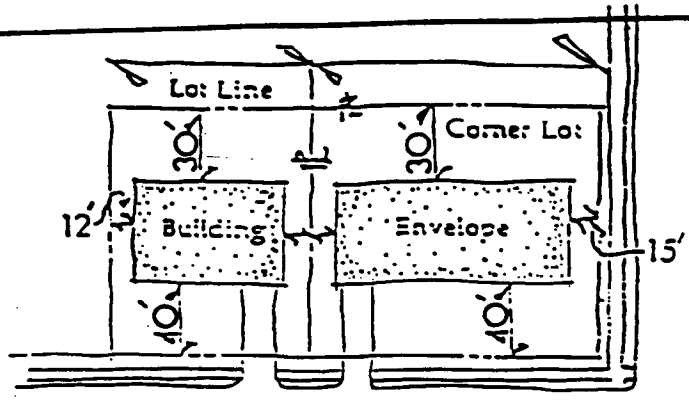
REGULATED
SM.M. POND

SCALE : 1" = 80'

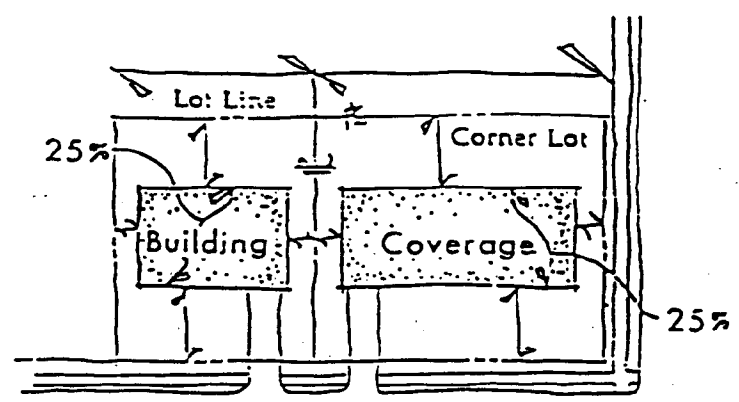
WR
WHITNEY
BAILEY
COX
MAGNANI

CONSULTING ENGINEERS
1800 YORK ROAD
TOWSON, MD. 21286
410-251-8060

FIGURE 3



Zoning Envelope



Zoning Envelope

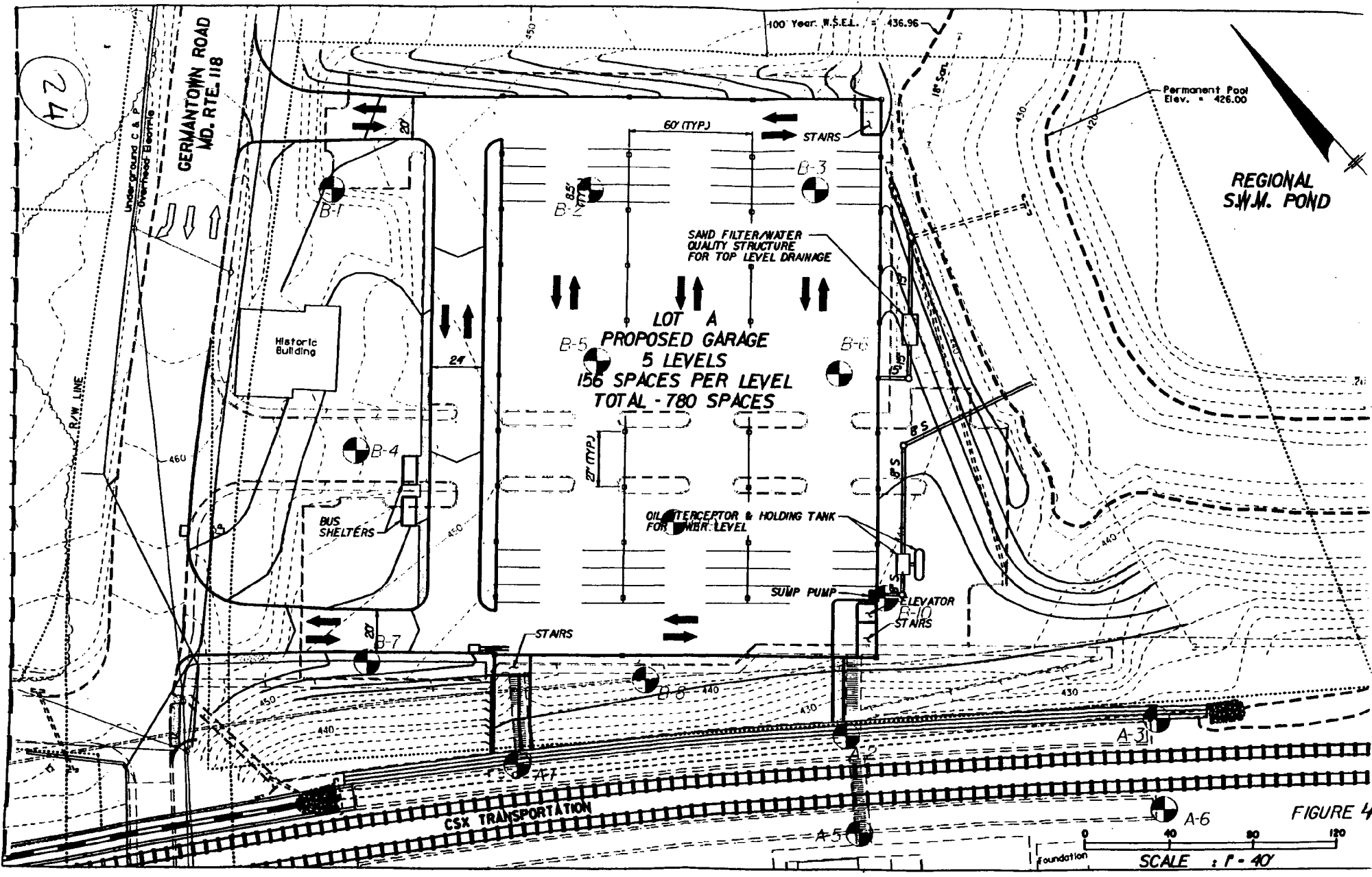


WHITNEY
BAILEY
COX
MAGNANI

ZONING REQUIREMENTS

23

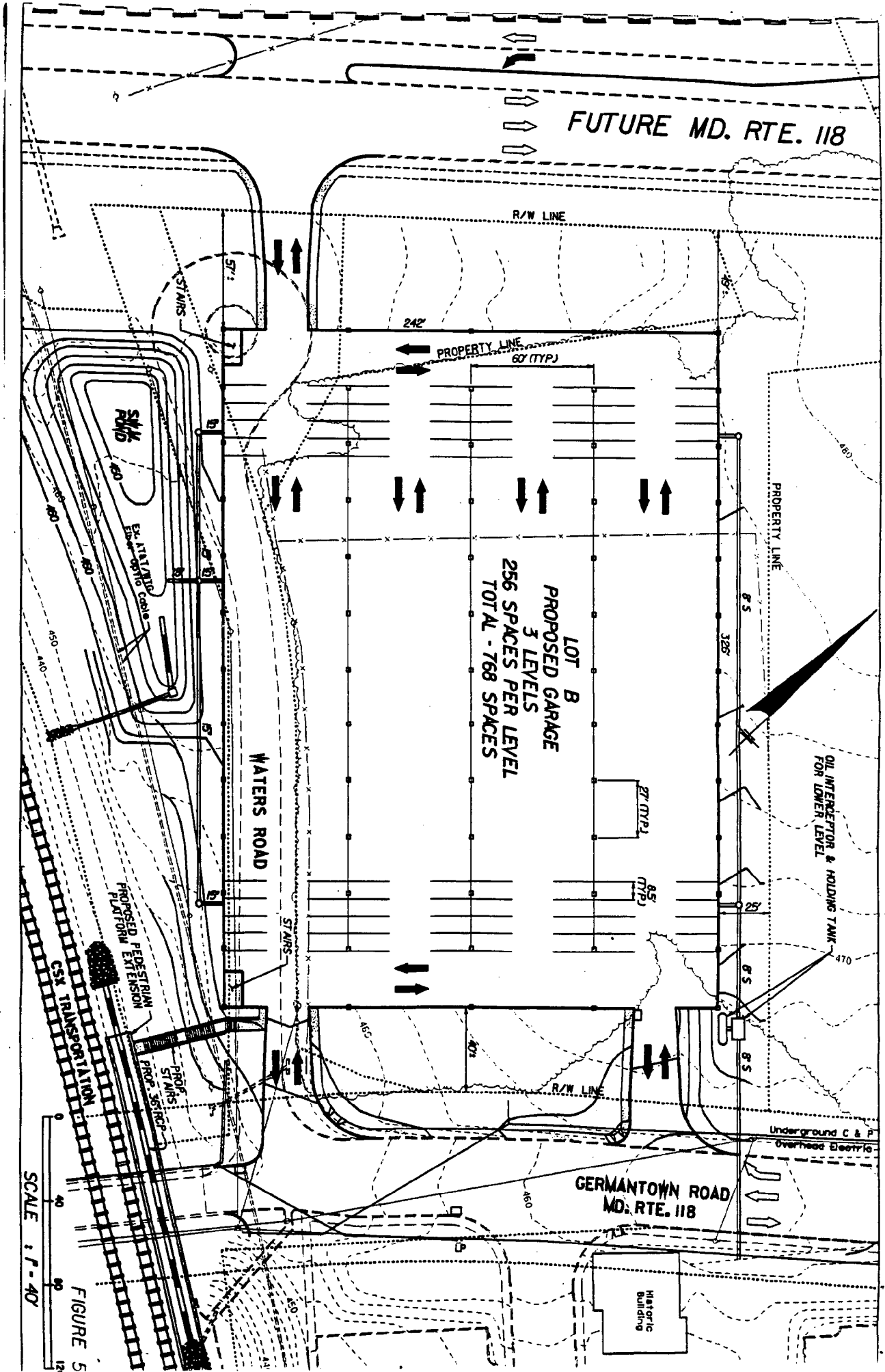
FIGURE 3



74

74

52



SCALE : 1" = 40'

FIGURE 5

26

PROP. PARKING GARAGE LOT 'B'

HISTORICAL HOUSE

GERMANTOWN ROAD
MD. RTE. 118

BUS & KISS-N-RIDE ISLE

PROP. PARKING GARAGE LOT 'A'

DATUM ELEV. 430.0

CROSS SECTION THROUGH HISTORICAL HOUSE AND BOTH PARKING GARAGES

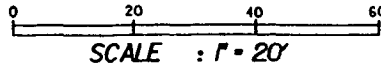


FIGURE 6



GERMANTOWN COMMUTER RAIL STATION IMPROVEMENTS
PARKING GARAGE CONCEPTUAL DESIGN PLAN
MONTGOMERY COUNTY, MARYLAND

REVISIONS	DATE	DESIGN	D.E.G.
		DRAFT	R.J.S.
		APPROVED	L.W.R.
		DATE	6-28-94
		SCALE	1" = 20'



26

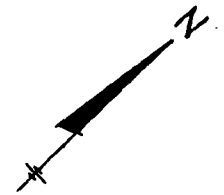
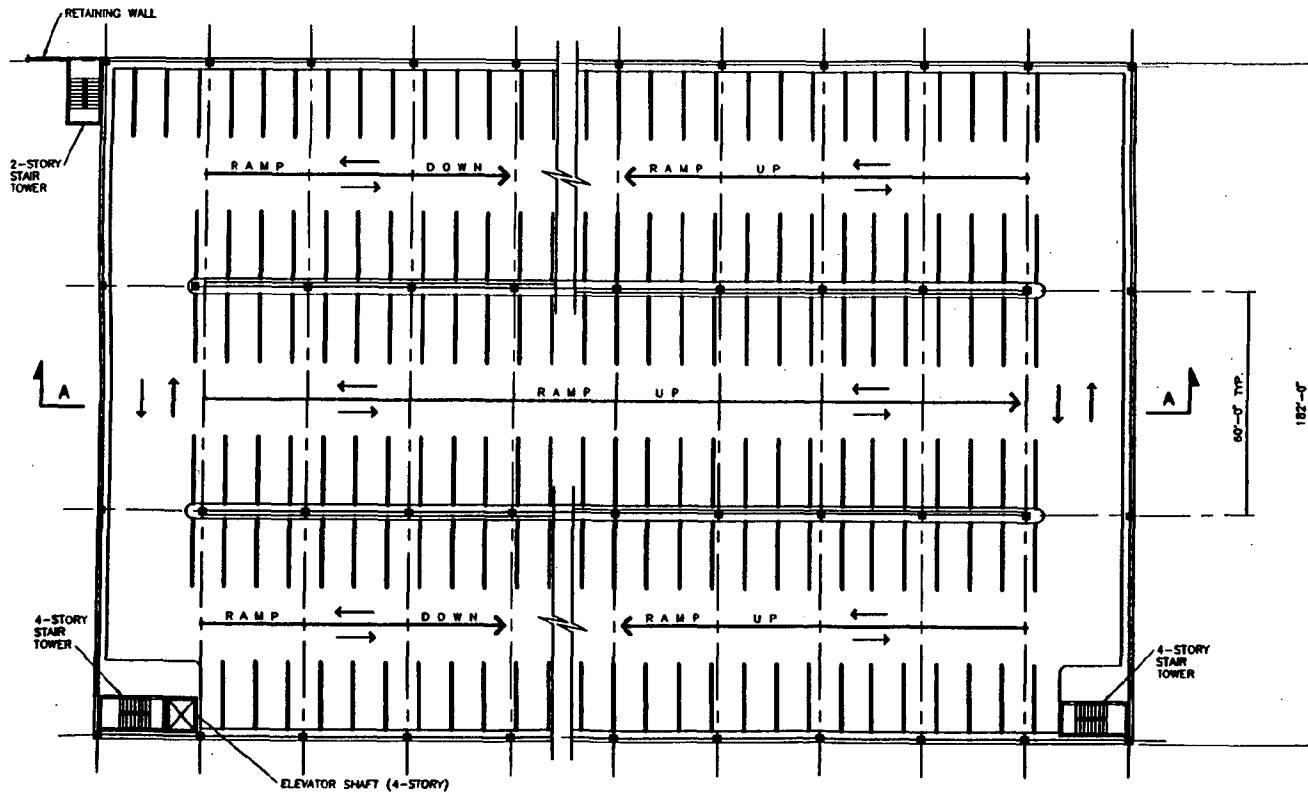
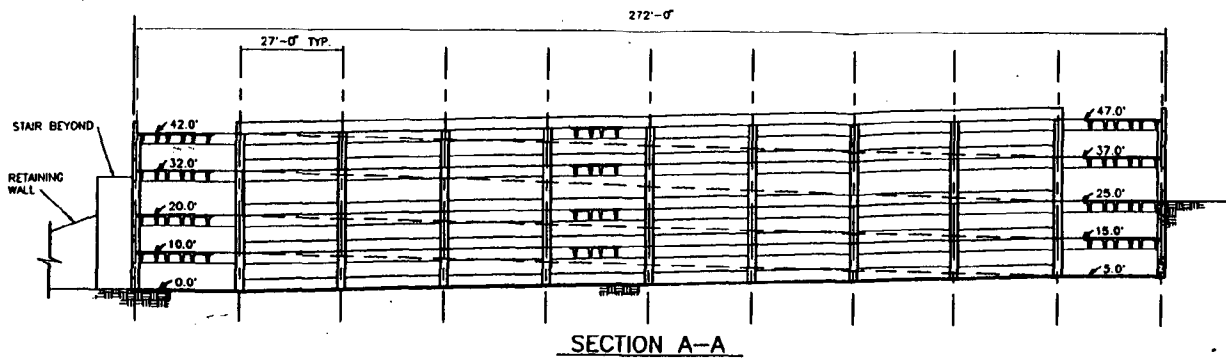


FIGURE 7
TYPICAL FLOOR PLAN - LOT 1

WB WHITNEY
BAILEY
COX
MAGNANI
CONSULTING
ENGINEERS
1850 YORK ROAD
TIMONIUM, MD. 21093
410-252-6060

27

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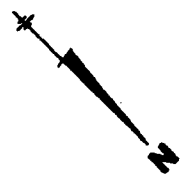
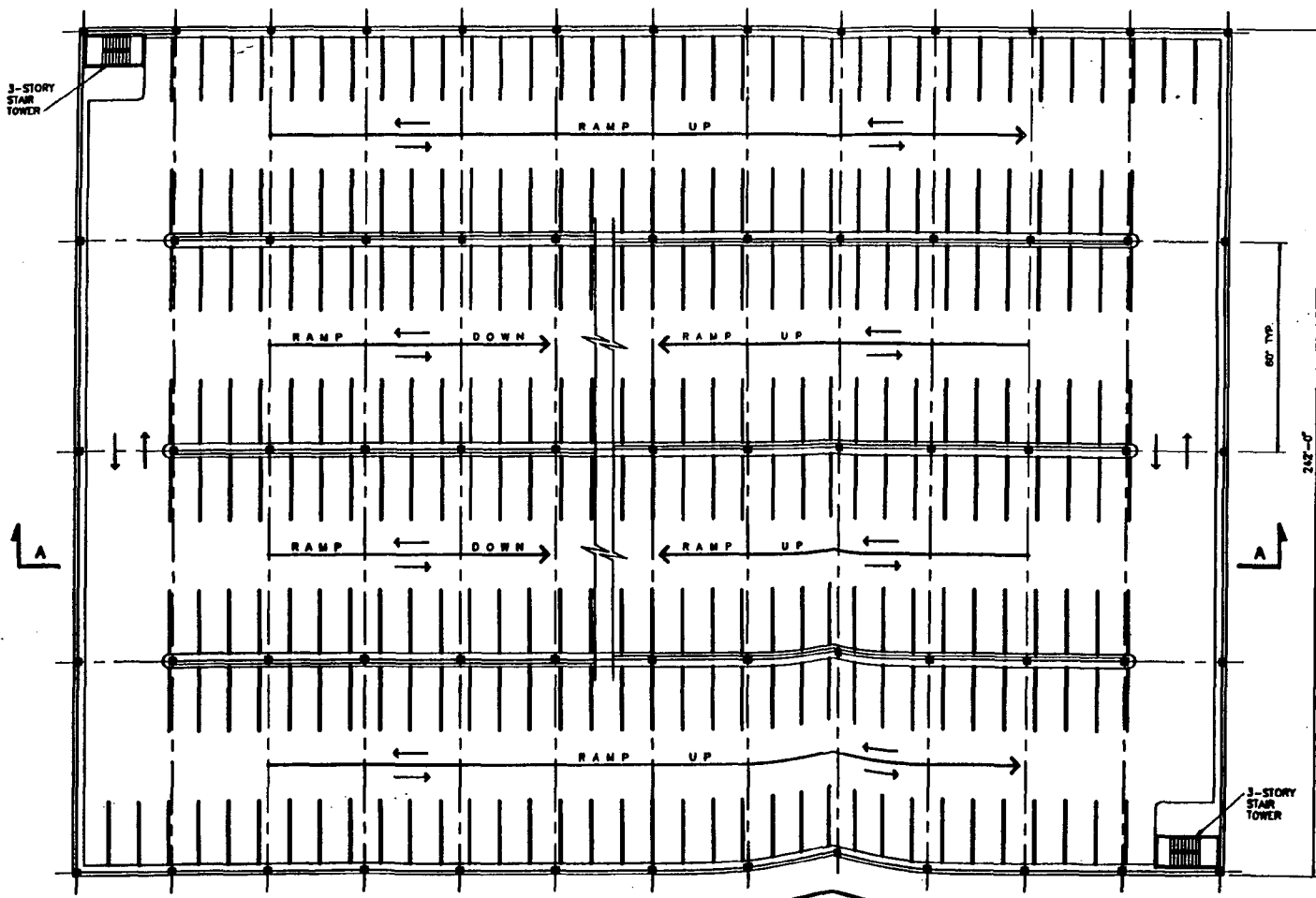
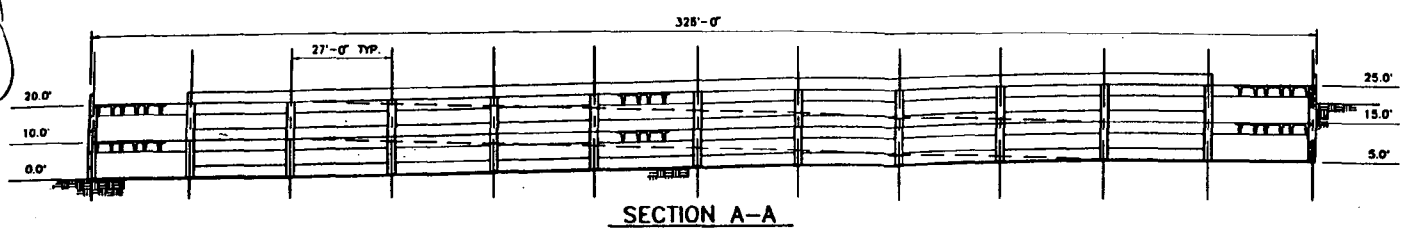
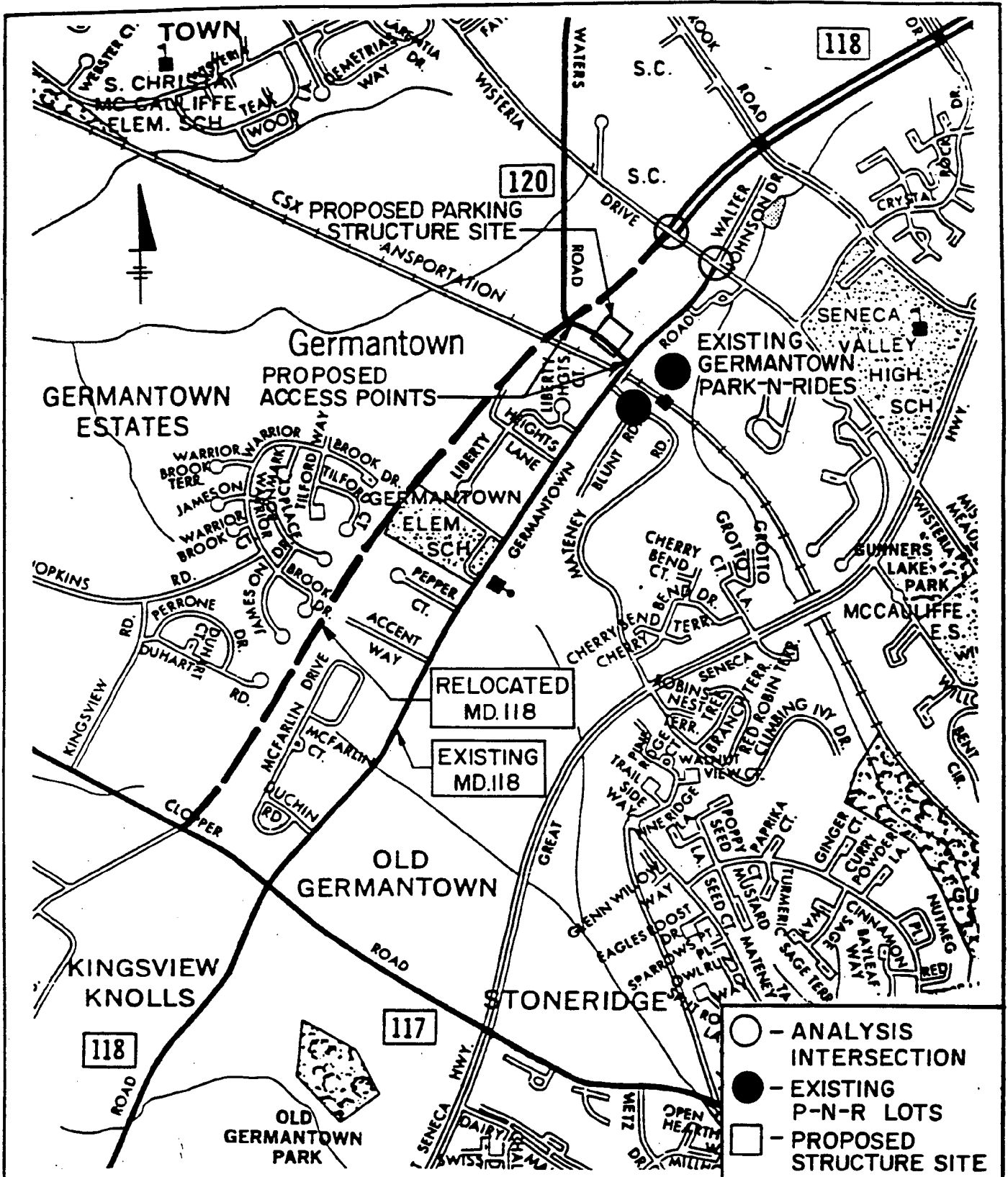


FIGURE 8
TYPICAL FLOOR PLAN - LOT 1

WB WHITNEY
BAILEY
COX
MAGNANI

CONSULTING
ENGINEERS
1850 YORK ROAD
TIMONUM, MD. 21093
410-252-6060

28



WHITNEY
BAILEY
COX
MAGNANI

PROJECT AREA MAP 29
GERMANTOWN PARK-N-RIDE

FIGURE 9

APPENDIX 'A'

ESTIMATED CONSTRUCTION COST BREAKDOWN

COST ESTIMATE BREAKDOWN

LOT 'A'

<u>Description</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Cost</u>	<u>Total</u>
Preliminary				
Mobilization (5% of total)	1	LS	\$395,000	\$395,000
Construction Stakeout	1	LS	\$40,000	\$40,000
Main. of Traffic	1	LS	\$5,000	\$5,000
Clear & Grub	1	LS	\$5,000	\$5,000
Eng. Office	1	LS	\$30,000	\$30,000
Grading				
Excavation	32,500	CY	\$11	\$357,500
Sheeting & Shoring (20' High)	450	LF	\$600	\$270,400
Sawcut	225	LF	\$6	\$1,350
Drainage (Includes Inlets, Pipes, Etc.)	1	LS	\$10,000	\$10,000
Paving				
Hot Mix Asphalt (1½") Surface	125	TON	\$45	\$5,625
Hot Mix Asphalt (4½") Base	360	TON	\$35	\$12,600
6" Subbase	1,400	SY	\$10	\$14,000
Pavement Marking	1	LS	\$1,000	\$1,000
Shoulder				
Type 'A' Curb & Gutter	1,070	LF	\$11	\$11,770
5' Sidewalk	2,700	SF	\$5	\$13,500
Retaining Wall (Includes Stairwell to At-Grade Parking)	1	LS	\$28,000	\$28,000
Utilities				
Relocate Ex. U.G. Fiber Optics Cable	700	LF	\$50	\$35,000
Relocate O.H. Lines (C&P)	1	LS	\$50,000	\$50,000
Possible Reloc. 24" W	150	LF	\$100	\$15,000
Landscaping	1	LS	\$25,000	\$25,000

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Parking Garage: 182' x 272' x 5 (Levels)

Structure (Includes Brick Veneer on Spandrels)	247,520	SF	\$23*	\$5,692,960
Fire Protection	247,520	SF	\$0.60*	\$148,512
Electric Service	247,520	SF	\$2.50	\$618,800
Mechanical	247,520	SF	\$0.20	\$49,504
Elevator	1	LS	\$120,000	\$120,000

(* Unit Prices are higher due to below-grade parking levels)

Upper Deck Drainage Sys. (Includes Drains, Pipes, Manholes, Etc.)	1	LS	\$91,000	\$91,000
Lower Decks Drainage Sys. (Includes Drains, Pipes, Manholes, San. Sewer, Oil Interceptor, Holding Tank, Etc.)	1	LS	\$93,000	\$93,000
Contingent Costs for Facade Above Ground				
Structure	148,510	SF	\$3.00	\$445,530
Fire Protection	148,510	SF	\$0.25	\$37,128
Mechanical	148,510	SF	\$0.50	\$74,255

TOTAL WITHOUT FACADE	\$ 8,139,121
	(\$10,435/space)
TOTAL WITH FACADE	\$ 8,696,034
	(\$11,148/space)

CONSTRUCTION COST ESTIMATE

LOT 'A'

	<u>Without Facade</u>	<u>With Facade</u>
A. Base Estimate	\$ 8,139,121	\$ 8,696,034
B. Contingency 25%	\$ 2,034,780	\$ 2,174,009
C. Escalation 5% (1994 to 2001)	\$ 3,560,865	\$ 3,804,515
D. Design Costs 8% (A & B)	\$ 813,912	\$ 869,603
E. Construction Mgmt/Insp. 8% (A+B+C)	\$ 1,098,781	\$ 1,173,965
F. Project Administration 6% (A+B+C)	\$ 824,086	\$ 880,473
	<hr/>	<hr/>
TOTAL COST	\$16,471,545	\$17,598,599

34

34

CONSTRUCTION ESTIMATE BREAKDOWN

LOT 'B'

<u>Description</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Cost</u>	<u>Total</u>
Preliminary				
Mobilization (5% of total)	1	LS	\$375,000	\$375,000
Construction Stakeout	1	LS	\$40,000	\$40,000
Main. of Traffic	1	LS	\$7,500	\$7,500
Clear & Grub	1	LS	\$30,000	\$30,000
Eng. Office	1	LS	\$30,000	\$30,000
Grading				
Excavation	20,900	CY	\$11	\$229,900
Sheeting & Shoring (10' High)	160	LF	\$210	\$33,600
Sawcut	130	LF	\$6	\$780
Drainage (Includes Inlets, Pipes, Etc.)				
	1	LS	\$10,000	\$10,000
Paving				
Hot Mix Asphalt (1½") Surface	115	TON	\$45	\$5,175
Hot Mix Asphalt (4½") Base	345	TON	\$35	\$12,075
6" Subbase	1,300	SY	\$10	\$13,000
Pavement Marking	1	LS	\$750	\$750
Shoulder				
Type 'A' Curb & Gutter	950	LF	\$11	\$10,450
5' Conc. Sidewalk	2,300	SF	\$5	\$11,500
Stairs (Includes Ret. Walls Around Stairs, Excavation, Stairs, Etc.)				
	1	LS	\$20,000	\$20,000
Platform Extension (Includes Pipe Extension Hdwall, Conc., Brick, Etc.)				
	1	LS	\$21,000	\$21,000
Utilities				
Relocate Ex. U.G. Fiber Optics Cable	950	LF	\$50	\$47,500
Relocate O.H. Lines	1	LS	\$75,000	\$75,000
Possible Reloc. 24" W	300	LF	\$100	\$30,000
Landscaping				
	1	LS	\$25,000	\$25,000

35

SWM Pond (Includes Class I Exc., Pipes, Riser Structure, Riprap, Fence, Gate, Etc.)				
	1	LS	\$30,000	\$30,000

Parking Garage: 326' x 242' x 3 (Levels)

Structure (Includes Brick Veneer on Spandrels)				
	236,676	SF	\$21	\$4,970,196
Fire Protection	236,676	SF	\$0.50	\$118,338
Electric Service	236,676	SF	\$2.50	\$591,690
Upper Deck Drainage System (Includes Drains, Pipes, Manholes, Etc.)				
	1	LS	\$106,000*	\$106,000
Lower Decks Drainage System (Includes Drains, Pipes, Manholes, San. Sewer, Oil Interceptor, Holding Tank, Etc.)				
	1	LS	\$104,000†	\$104,000
Contingent Costs for Facade				
Structure	236,676	SF	\$3.00	\$710,028
Fire Protection	236,676	SF	\$0.25	\$59,169
Mechanical	236,676	SF	\$0.50	\$118,338

TOTAL WITHOUT FACADE \$ 6,948,454
(\$ 9,047/space)

TOTAL WITH FACADE \$ 7,835,989
(\$10,203/space)

* Cost higher than Lot 'A' due to more trench drains.

† Cost higher than Lot 'A' due to more piping and longer connection to sanitary sewer.

CONSTRUCTION COST ESTIMATE

LOT 'B'

	<u>Without Facade</u>	<u>With Facade</u>
A. Base Estimate	\$ 6,948,454	\$ 7,835,989
B. Contingency 25%	\$ 1,737,114	\$ 1,958,997
C. Escalation 5% (1994 to 2001)	\$ 3,039,949	\$ 3,428,245
D. Design Costs 8% (A & B)	\$ 694,845	\$ 783,599
E. Construction Mgmt/Insp. 8% (A+B+C)	\$ 938,041	\$ 1,057,858
F. Project Administration 6% (A+B+C)	\$ 703,531	\$ 793,394
TOTAL COST	\$14,062,025	\$15,858,082

APPENDIX 'B'

**TELEPHONE CONVERSATION RECORDS
AND CORRESPONDENCE**

Baker

Michael Baker Jr., Inc.
c/o AT&T
11820 Leesburg Pike
Herndon, VA 22070

(703) 430-5086
FAX 430-5306

March 25, 1994

Whitney, Baily, Cox & Magnani
ATTN: Mr. Leon J. Kriebel, P.E.
1850 York Road
Timonium, MD 21093

RE: Parking Garage Study
MD Route 118 at CSX Railroad
Montgomery County, Maryland

Dear Mr. Kriebel:

Attached is a map showing AT&T cables in the vicinity of MD Route 118 and the CSX railroad.

The locations of these cables are as follows:

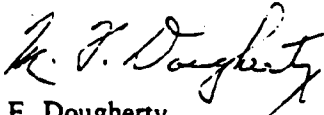
- The lightguide cable is located within the CSX railroad right-of-way on the north side of the tracks.
- The wire cable is located on the south side of Wisteria Drive on private right-of-way to Old Route 118, then, also in private right-of-way, north along Old Route 118 on the west side of the road.

Should you wish to have the cables marked in the field you may contact AT&T at Monrovia, Maryland on (301) 865-3803.

If there are any questions or additional information is needed, please call me on (703) 430-5088.

Sincerely,

MICHAEL BAKER JR., INC.



M. F. Dougherty
Project Engineer

MFD/skt

Enclosure

40

40

1350 York Road
Towson, Maryland 21043
Tel: 410-281-1100
Fax: 410-281-1101

WHITNEY

BAILEY



COX

MAGNANI

Associates
John D. Jones PE
Richard E. Moskowitz PE
Leon J. Knebel PE
W. Howard Garner ASA
John J. Knight PE
John D. Wayne PLS
Albert W. Cocco
Michael J. Simms PE

April 28, 1994

Mr. Ernest M. Baisden, P.E.
Mass Transit Administration
300 West Lexington Street
Baltimore, Maryland 21201-3415

Re: Contract No. MTA-23-91-518
Design of Suburban Transit Facilities
Germantown Parking Garage Study
WBCM Job No. 93-100-08

Dear Mr. Baisden:

On April 25, 1994, a meeting was held at MTA between WBCM and you, with the primary purpose of discussing the alternates that have been proposed to this point. The following is a list of questions and concerns compiled during the feasibility study of said project that require answers to finalize the feasibility study. Please address these issues so that WBCM can continue with the study.

- Since the State Highway Administration is proposing to relocate MD Route 118 northwest of the existing wooded lot, should it be assumed that access will be allowed from proposed MD 118 to the proposed garage on either lot?
- For the studies, can we make an assumption that the existing MD 118 bridge will be closed to vehicular traffic and used for pedestrian traffic? If not, will studies need to be done for both alternatives?
- Based on zoning requirements, the maximum percentage of net lot area that may be covered by buildings is 25%. Can the County waive this criteria?
- What are the landscaping requirements?
- How many levels in a garage would be acceptable in this area? To obtain approximately 1,000 spaces, a garage at the wooded lot would have 4 floors (240 spaces per floor). On the existing park-n-ride lot, 7 floors would be needed (144 spaces per floor). For a garage at the wooded lot, would 3 levels/720 spaces be sufficient? For the garage at the existing park-n-ride, would 5 or 6 levels/720 or 864 spaces be sufficient?

41



Member ACEC



Attn: Mr. Ernest M. Baisden
April 28, 1994
Page 2

- In the study of the wooded lot, it was assumed that Waters Street could be used to expand for usable building area. Since this is a major factor to our design layout, can we receive confirmation on this from the County?
- Are you aware of any requirement for a minimum distance to the historic building on the lot from a proposed sidewalk or garage?
- Based on our research, both sites are out of the historic district. Therefore, will the County require any special exterior building treatment?

Sincerely,

WHITNEY, BAILEY, COX & MAGNANI

Leon J. Kriebel, P.E.
Associate

LJK:GEG:tjs
9310008L.002

2275 New River
 Columbia, Maryland 21046
 Phone (301) 251-1144
 Fax (301) 251-1144

WHITNEY

BAILEY



COX

MAGNANI

2275 New River
 Columbia, Maryland 21046
 Phone (301) 251-1144
 Fax (301) 251-1144

May 17, 1994

Mr. Mike Geier
 Permit and Plan Review
 Environmental Protection
 250 Hungerford Drive
 Rockville, Maryland 208

Reference: Germantown Parking Garage Study
 MD Route 118 and CSX Railroad
 WBCM Job No. 93-100-08

Dear Mr. Geier:

As we discussed by phone, attached is a summary of the results of our storm water management analysis for the above referenced project. We assumed that the entire proposed site would be impervious and would be conveyed to the existing regional storm water management pond. The existing site does not drain to this facility today. Re-running the TR-20 program supplied to us with the additional drainage area resulted in the following:

SUMMARY OF ADDITIONAL D.A. TO EXISTING REGIONAL POND

	EX. POND	POND W/ ADDIT. AREA	INCREASE
Q2 WSEL	435.04	435.11	+0.07'
Q2 Discharge	21.21 cfs	28.87 cfs	+7.66 cfs
Q10 WSEL	436.28	436.33	+0.05'
Q10 Discharge	332.65 cfs	356.45 cfs	+23.80 cfs
Q100 WSEL	436.96	437.00	+0.04'
Q100 Discharge	623.75 cfs	644.87 cfs	+21.12 cfs
SPLWY HYDRO WSEL	437.37	437.41	+0.04'
SPLWY Q Discharge	884.98 cfs	910.66 cfs	+25.68 cfs
PROB. MAX. WSEL	439.64	439.78	+0.14'
PROB. MAX. Q Discharge	2228.73 cfs	2283.72 cfs	+54.99 cfs



Attn: Mr. Mike Geier
May 17, 1994
Page 2

A quick observation of the riser layout indicates that reducing the 7" orifice at the gate valve for the 8" ductile iron pipe would decrease the discharges during extended detention thereby raising the extended detention water surface elevation. As a result, the 2-year storm water surface elevation would also be raised, increasing the 2-year storm discharge. (The riser crest elevation for this pond was set at an elevation slightly below the 2-year storm water surface elevation.) Conversely, opening the gate valve to its maximum orifice size of 8" would have the opposite effect - lowering the extended detention water surface elevation, increasing the extended detention discharges, lowering the 2-year storm water surface elevation and decreasing the 2-year discharge. Increasing the extended detention discharges (thereby reducing the effectiveness of extended detention) would not be advised unless the pond had been over-designed for extended detention. Until the original extended detention computations are obtained and analyzed, further study of this option (i.e., opening the valve to its maximum orifice size) is not warranted.

As you are aware, we are only in a study phase for the proposed garages; therefore, we are requesting an opinion on whether the higher water surface elevations and increased release rates would be permitted. If not, would modification of the existing pond release structures be permitted or would a separate storm water management facility be required?

We thank you for your time. If you have any questions or need additional information, please contact me.

Sincerely,

WHITNEY, BAILEY, COX & MAGNANI

Leon J. Kriebel, P.E.
Associate

LJK:tjs
9310008L.003

cc: Mr. Ernest Baisden

44

44

WHITNEY. BAILEY



COX MAGNANI

May 27, 1994

Mr. David Ramsey
Acting Chief
Access Permits Division
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21202

Attn: Mr. Greg Cook

Re: Germantown Parking Garage Study
Montgomery County
WBCM Job No. 9310008

Gentlemen:

Reference is made to our telephone conversation of May 17, 1994, regarding access from Relocated MD Route 118 to a potential parking garage facility. We are currently conducting a study for the Maryland Mass Transit Administration for potential sites for a parking garage to serve the MARC train station.

Attached is a print of a concept site plan for the garage on a site located adjacent to the CSX Railroad and Relocated MD Route 118. We have marked in red on the print a potential access point from MD Route 118 to the garage. Please note that this is only a preliminary layout and turning lanes are not based on traffic numbers. We are requesting a determination on whether access would be permitted from the Relocated MD Route 118. As an additional note, please be aware that the MTA plans to construct a surface lot on this site prior to constructing any parking garage.

We would appreciate a reply at your earliest convenience. If you have any questions, please do not hesitate to call. I can be reached at 561-8388.

Sincerely,

WHITNEY, BAILEY, COX & MAGNANI

Leon J. Kriebel, P.E.
Associate

LJK:mml
9310008L.004

CC: Mr. Ernest Baisden, MTA

45





**Maryland Department of Transportation
State Highway Administration**

O. James Lighthizer
Secretary
Hal Kassoff
Administrator

June 10, 1994

Mr. Leon J. Kriebel
Whitney, Bailey, Cox & Magnani
1850 York Road
Timonium, Maryland 21093

Re: Montgomery County
MD 118 Relocated
Germantown Parking
Garage Study

Dear Mr. Kriebel: .

Thank you for your transmittal of May 27th, which we received on May 31st concerning the above referenced project.

We are presently circulating this information to the Design Division within the State Highway Administration for their review and comment. We will advise you of our findings and recommendations within the next few weeks.

In the interim, if you have any questions, or need additional information, please contact Greg Cooke of this office at (410) 333-1350.

Very truly yours,

David Ramsey, Acting Chief
Engineering Access Permits
Division

GC/maw

RECEIVED

JUN 14 1994

WBCM

My telephone number is 410-333-1350 (Fax# 333-1041)

Maryland Relay Service for Impaired Hearing or Speech
1-800-735-2258 Statewide Toll Free

Mailing Address: P.O. Box 717 • Baltimore, MD 21203-0717
Street Address: 707 North Calvert Street • Baltimore, Maryland 21202

46

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CONVERSATION RECORD

DATE: 4/4/94

TIME: 12:15 p.m.

REFERENCE: GERMANTOWN PARKING GARAGE STUDY

INDIVIDUAL: Ed Daniel

CORPORATION: Montgomery County

PHONE NO. (301)217-2976

RE: Questions Sent to Mr. Daniel; Refer to April 28, 1994, letter to Mr. Baisden

NOTES:

1. Montgomery County has always assumed that the future parking lot and/or garage will have access from the new MD 118. This access point has not been approved yet by SHA.
2. Montgomery County has not entered into discussion with the SHA about closing the bridge, but this has been in the County Master Planning Plan for a long time. Mr. Daniel is not aware of any community opposition to closing the bridge since there will be an arterial connection between the new MD 118 and old MD 118, 1000' west of the railroad.
3. Mr. Daniel was not aware of the 25% criteria and does not know if it can be waived by the County. If so, the County would certainly waive it. He seems to think that the zoning has been changed in the most recent zoning plan. He also said that the County would not want to waive any of the building set-back criteria.
4. Mr. Daniel does not know what the landscaping criteria is. However, the County would want a lot of landscaping to blend the garage into the area as much as possible.
5. In the County's conceptual work, they had 3 levels, one at grade and two above ground. Their criteria was for 800 vehicles; however, there has never been a demand study. Their feelings have been that a garage was never feasible on the existing park-n-ride lot due to the smaller space available. Since a major portion of the historic district is directly across the tracks from the existing park-n-ride, Mr. Daniel feels that a 7-level garage would never be accepted by the community.

6. Waters Street is a SHA highway. The County has always assumed that the MTA will work out closure of the road with SHA.
7. Mr. Daniel thinks the sites are within the historic district. He is not aware of a minimum distance that the garage must be from the existing historic house; however, he would not want it too close. The County owns the historic house and has leased it out for 99 years.
8. Even if the garage is not in the historic district, the County would want the garage compatible or old-looking to match the community. Something like a brick facing would be preferred on all four sides.

CONVERSATION RECORD

DATE: 4/13/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Creston Mills

CORPORATION: SHA - District #3

PHONE NO. (301)513-7311

RE: Study

NOTES:

I talked to Creston Mills about the following items:

1. Waters Road is an SHA highway. SHA has no problem with abandonment of the roadway when it is not longer needed.
2. Closing of the bridge on Old MD Route 118--it has been part of the overall plan to transfer the existing MD Route 118 to the County when the new MD Route 118 is complete. Therefore, the County can do whatever they want to.
3. Access to the parking lot or garage from the new MD Route 118 - SHA has not looked at this access issue. It has never been discussed with Mr. Mills. He would not have the final say on breaking limited access. This would have to go through Access Permits. He would be asked to comment on it. Mr. Mills suggested that I talk to Mr. Greg Cook in Access Permits about this issue.

CONVERSATION RECORD

DATE: 4/17/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Greg Cook

CORPORATION: SHA - Access Permits

PHONE NO. (410)333-1350

RE: Access from Relocated MD Route 118

NOTES:

I talked to Greg Cook about access from the new MD Route 118 to the future parking garage site. He said they have received several requests for access points for that project. He suggested that we send a concept plan and request comment on access from the new MD Route 118.

CONVERSATION RECORD

DATE: 5/26/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE STUDY

INDIVIDUAL: Nancy Witherall

CORPORATION: MNCPP

PHONE NO. (301)495-4570

RE: Architectural Considerations

NOTES:

Ms. Witherall is with the Design Zoning & Preservation Division of MNCPPC. The purpose of the call was to discuss requirements for the parking garage with respect to impacts on the historic district. I asked her if the garage would need a facade or if it would need to be completely enclosed. She said that she would be mostly concerned about the location and height of the structure and what visual impacts that the garage would have on the historic district. She said that it may not need to be enclosed; however, it should be attractive and appropriate.

cc: Mr. Ernest Baisden

CONVERSATION RECORD

DATE: 6/2/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Mike Geier

CORPORATION: Montgomery County Dept. of Environment

PHONE NO. (301)217-6324

RE: Existing Stormwater Management Pond

NOTES:

Mr. Geier called regarding our May 17, 1994, letter on our analysis of the existing stormwater management pond if the additional runoff from Lot 'B' was to be conveyed to the pond. Mr. Geier stated that he did not like the 2-year discharges based on our analysis. He said we may be able to modify the discharges by opening the gate valve further; however, this would affect the extended detention and he did not know the designed extended detention duration. The way the discharges are based on the numbers that we submitted to him, he is not willing to entertain a waiver.

He raised the possibility of raising the weir crest; however, he would really prefer to keep the drainage flowing the same direction that it does today.

cc: Mr. Ernest Baisden

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CONVERSATION RECORD

DATE: 6/8/94

TIME: 10:00 A.M.

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Mr. Ed Daniel

CORPORATION: Montgomery County

PHONE NO. (301)217-2976

RE: Questions on Parking Garage

NOTES:

1. I asked Mr. Daniel if the County had any plans on the historic house. He checked and was not able to locate any.
2. I asked Mr. Daniel if he was aware of any previous history on this project (i.e., any previous study for a parking garage). He was not aware of any previous studies.
3. I asked Mr. Daniel about the need for an elevator with a 2-story garage. He said that they did not put one in the Shady Grove garage, which was only two stories; however, he does not know if the code did not require one, or if they got a waiver.
4. I asked Mr. Daniel about his comment on the sight distance at the existing MD Route 118 bridge. He agreed with me that if the bridge is closed, then there is no problem. I asked him if the County had a condition report on the bridge. He said they did not, since it is SHA-owned. He does not know the condition of the bridge; however, he assumed that it would need to be reconstructed if it remains open. He agreed with me that it could be widened to eliminate a sight distance problem if it is reconstructed.

cc: Mr. Ernest Baisden

53

CONVERSATION RECORD

DATE: 6/8/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Greg Cook

CORPORATION: SHA - Access Permits

PHONE NO. (410)333-1250

RE: Access from New MD 118

NOTES:

I called Greg Cook to find out if he had received our plan showing access from the new MD Route 118. He said he had the plan, but SHA would need to look at the overall area to see where additional cross-overs may be. Any median break requires approval of the Chief Engineer. He also stated that in the past, that typically there was a letter from the County supporting the access point. Greg will be sending this to Ken McDonald in highway design for further review.

54

54

CONVERSATION RECORD

DATE: 6/19/94

TIME: 3:20 p.m.

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Denise Boswell

CORPORATION: MNCPPC

PHONE NO. (301) 495-4731

RE: Maximum 25% Lot Coverage Requirement

NOTES:

Denise said we would have to petition the Board of Appeals for a variance if we need a building coverage more than 25%. It does not matter whether it is publicly owned and operated--MTA (the parking garage).

The burden of proof is on us to prove hardship, that the existing zoning imposes hardship.

I told Denise we want to keep the use compatible.

We must still go through the mandatory referral process.

BY: Pat Ford

MEMO

STATE OF MARYLAND

Department of Housing and Community Development
Division of Historical and Cultural Programs
Maryland Historical Trust
100 Community Place
Crownsville, Maryland 21032-2023
(410) 514-7600
FAX # (410) 987-4071

TRANSMITTAL SLIP

TO: Kevin Zeck RE: Greenbank MTA to P
FROM: Beth Harrold FAX #: (201) 495-
DATE: 6/26/95
PAGES (including cover): _____ DUE: _____

<input type="checkbox"/> Please Approve & Return	<input type="checkbox"/> For Your Signature
<input checked="" type="checkbox"/> As Requested	<input type="checkbox"/> Please Note & File
<input type="checkbox"/> Please Comment & Return	<input type="checkbox"/> Please Note & Return
<input type="checkbox"/> Copies On	<input type="checkbox"/> Please See Me
<input checked="" type="checkbox"/> For Your Information	<input type="checkbox"/> Please Take Charge

REMARKS:

*Found the rest of the file.
I apologize. It looks as though, made the determination without
consulting or even copying you all! Don't know what
I was thinking
Does appear clear that only commented on Western site.
While they said don't believe district is eligible, could
reconsider if you all felt ~~the~~ incorrect & provided argument
Also, would address possibility of individually eligible
or smaller grouping of properties that might be affected by Eastern
Site.
Have called to MTA to ask about status.*



William Donald Schaefer
Governor

Jacqueline H. Rogers
Secretary, DHCD

December 13, 1994

Office of Preservation Services
Mr. Jonathan P. Wise
Project Planner
Office of Planning and Programming
Maryland Department of Transportation
Mass Transit Administration
300 West Lexington Street
Baltimore, Maryland 21201-3415

Re: MARC Germantown Station Surface
Parking Expansion
Montgomery County, Maryland

Dear Mr. Wise:

We understand that you wish to confirm a prior determination by this office that the above-referenced project would have no effect on historic properties. Your concern arose from a recent letter from this office to an engineering firm requesting historic properties information for a project located in Germantown. In that letter we noted that Germantown is a National Register eligible historic district. Furthermore, our records indicated that a contributing resource might be located within or adjacent to the project area.

Germantown was determined to be eligible for the National Register of Historic Places on October 17, 1979. This is a formal determination of eligibility and can be officially reversed only by the National Register office. However, it is the opinion of this office that in the years since the determination, Germantown has lost a substantial degree of integrity and would no longer be eligible for the National Register of Historic Places. On December 13, 1994, I made a site visit to Germantown. Based on the site visit I can reconfirm that, in our opinion, the district is not eligible and the proposed surface parking expansion will have no effect on historic properties.

Please remember that, in the Section 106 process, the federal agency (FTA) or its designee (MTA) consults with the State Historic Preservation Office (MHT) in making determinations of eligibility and effect. However, it is ultimately the federal agency's responsibility to make the determinations.



Division of Historical and Cultural Programs
100 Community Place • Crownsville, Maryland 21032 • (410) 514-7627/7628

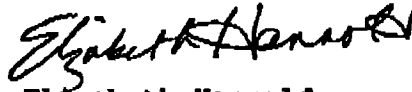
The Maryland Department of Housing and Community Development (DHCD) pledges to foster the letter and spirit of the law for achieving equal housing opportunity in Maryland.



Mr. Jonathan P. Wise
December 13, 1994
Page 2

If we can be of any further assistance, please contact me at
(410) 514-7636.

Sincerely,



Elizabeth Hannold
Preservation Officer
Project Review and Compliance

EAH

cc: Diane Ratcliff
John Newton

O. James Lighthizer, Secretary



John A. Agro, Jr., Administrator



MARYLAND DEPARTMENT OF TRANSPORTATION

MASS TRANSIT ADMINISTRATION -

300 West Lexington Street • Baltimore, Maryland 21201-3415

November 18, 1994

NOV 22 1994

Mr. Gary D. Shaffer
Preservation Officer
Division of Historical and Cultural Programs
Department of Housing and Community Development
100 Community Place
Crownsville, Maryland 21032

Re: MARC Germantown Station Surface Parking Expansion

Dear Mr. Shaffer:

Per your request, attached are copies of a location map, a site map and pictures of the proposed Germantown Station surface parking expansion project.

The station is located at Germantown Road (old route 118) and Mateney Hill Road. Parking expansion is proposed for part of parcels 58, 4, and 50. The northwest section of these parcels will be used for the relocation of route 118.

Please share these maps and pictures with Ms. Elizabeth Hannold of the Review and Compliance Section within the Historical and Cultural Programs.

Sincerely,

Jonathan P. Wise
Project Planner
Office of Planning and Programming

Attachments

CC: Diane Ratcliff - MTA
John Newton - MTA
Elizabeth Hannold

My phone number (410) _____

FAX number (410) _____

TDD for people who are hearing and/or speech impaired _____



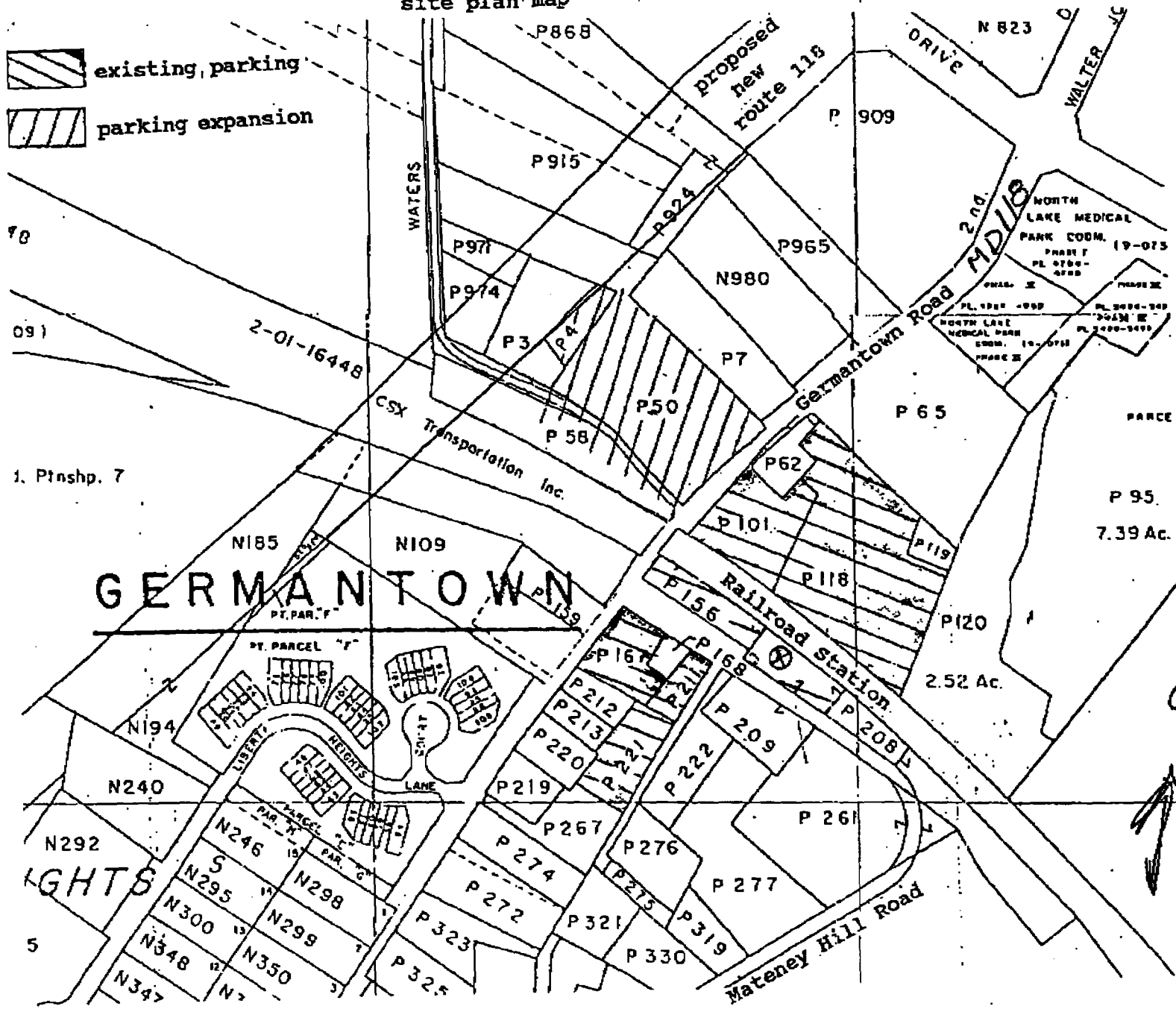
site plan map



existing parking



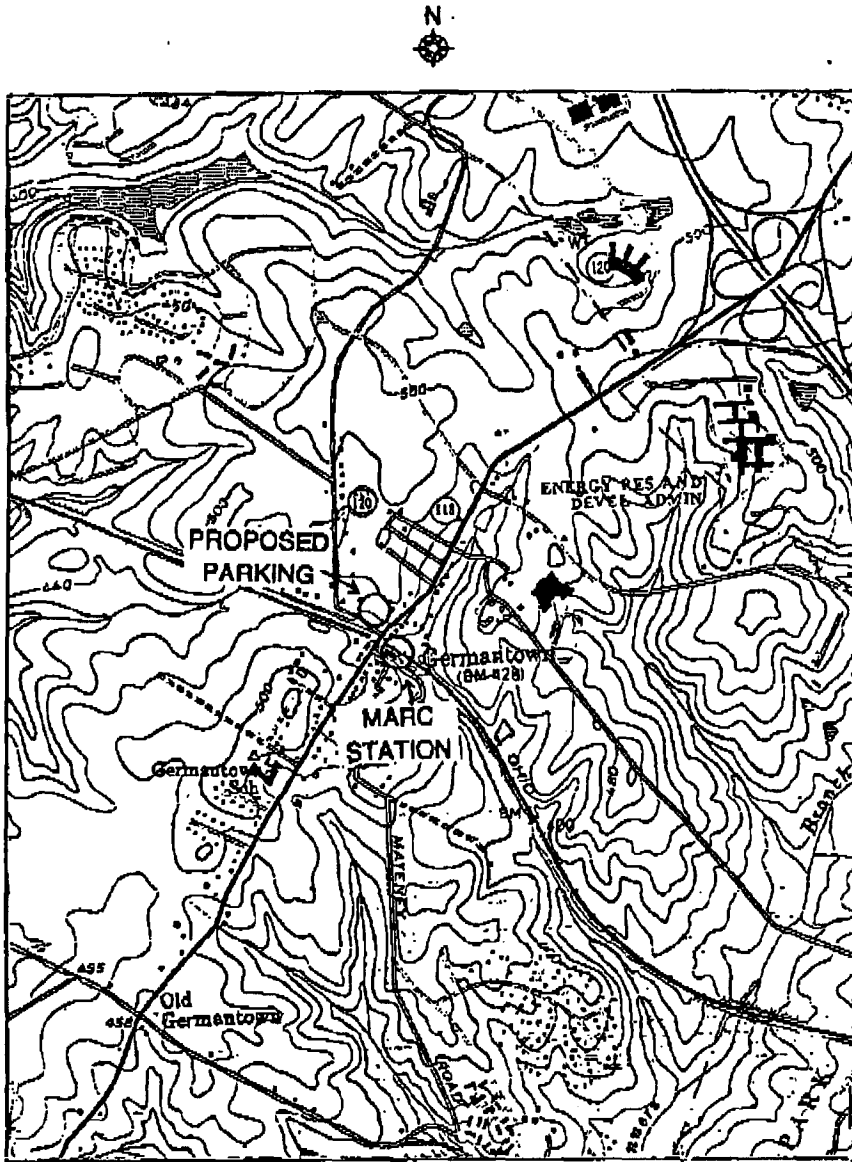
parking expansion



1. Ptnshp. 7

GERMANTOWN

LIGHTS



source: 7.5 Minute Series - Germantown, Maryland - Topographic Quadrangle Map

MTA
 MASS TRANSIT ADMINISTRATION
 300 West Lexington Street
 Baltimore, Maryland 21201-3415
 410-333-3375

TITLE:	Site Vicinity Map Germantown Parking Lot	
DATE:	September 1994	SCALE: 1:24,000
DRAWN:	USGS	



William Donald Schaefer
Governor

Jacqueline H. Rogers
Secretary, DHCD

September 27, 1994

Office of Preservation Services

Mr. David C. Powlen
Whitney, Bailey, Cox, Magnani
849 Fairmount Avenue, Suite 100
Baltimore, Maryland 21286

Re: MTA Parking Lot - Germantown
Reforestation Plan
Montgomery County, Maryland

Dear Mr. Powlen:

Thank you for your recent letter, dated 23 August 1994 and received by the Trust on 31 August 1994, requesting information on historic properties for your Reforestation Plan submittal.

We understand that the Reforestation Plan is being developed as part of the planning for a three-story parking deck on Germantown Road, at the intersection of Waters Road, directly across from an existing MTA parking lot. The parking deck would be built by the State of Maryland.

Our records indicate that on October 17, 1979 the Germantown Historic District was determined to be eligible for the National Register of Historic Places. Enclosed for your information is a copy of the Maryland Inventory of Historic Properties form for the district. The proposed parking facility would be located at the heart of the district. The historic district map shows contributing buildings adjacent to, if not within, the area where the proposed parking facility would be located. We are aware that considerable development that has occurred in the area since 1979 and that a number of historic buildings have been lost. Thus, we believe the eligibility of the district should be reevaluated.

The Inventory does not presently record any known archeological sites within or immediately adjacent to the project area.

Since the proposed project entails state agency involvement, it will be subject to review under Sections 5-617 & 5-618 of Article 83B of the Annotated Code of Maryland. State historic preservation law requires MTA to consider the effects of the proposed project on significant historic properties, including

Division of Historical and Cultural Programs

100 Community Place • Crownsville, Maryland 21032 • (410) 514-7627/7628

The Maryland Department of Housing and Community Development (DHCD) pledges to foster the letter and spirit of the law for achieving equal housing opportunity in Maryland.



EQUAL HOUSING
OPPORTUNITY

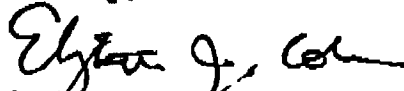


Mr. David C. Powlen
September 27, 1994
Page 2

architectural and archeological resources. Part of the review process involves consultation between the agency (or its designee) and our office to identify and evaluate historic properties that may be affected by the project and to develop measures to avoid, reduce or mitigate any adverse effects on significant historic properties.

If you have questions or require further assistance, please call Ms. Elizabeth Hannold (for structures) at (410) 514-7636 or me (for archeology) at (410) 514-7631.

Sincerely,



Elizabeth J. Cole
Administrator, Archeological Services

EJC/EAH
9402328
Enclosure

cc: Ms. Diane Ratcliff (MTA)
Hon. Gilbert Gude
Dr. Thomas King
Mr. Mike Seebold
Ms. Gwen Marcus
Ms. Marie-Regine Charles-Bowser

Alexander Whitney, Jr., P.E.
Paul E. Cox, P.E.
Richard Wm. Magnani, P.E.
Douglas F. Swess, P.E.
Wm. Preston Davis, P.E.
Philip Der, P.E.
David G. Mongan, P.E.

WHITNEY BAILEY



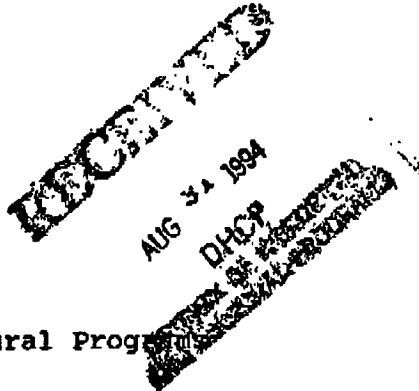
MTA
EJC/COX
COX MAGNANI

9402328

Associates:
Bilon D. Jones, P.E.
Burrus E. Holloway, P.E.
Leon J. Kriebel, P.E.
W. Howard Gorrett, AIA
John J. Knight, P.E.
John D. Wayne, P.L.S.
Albert M. Copp
Joseph J. Siemek, P.E.

849 Fairmount Avenue
Suite 100
Baltimore, Maryland 21286
410-512-4500
Fax 410-324-4100

August 23, 1994



Office of Preservation Services
Maryland Historical Trust
Division of Historical and Cultural Programs
100 Community Place
Crownsville, MD 21032

ATTN: Ms. JoEllen Freese

RE: MTA Parking Lot - Germantown, Maryland
Reforestation Plan
WBCM Job No. 93043.03.1

Ladies and Gentlemen:

The State of Maryland is planning to build a new three-story parking deck in Montgomery County on Germantown Road, at the intersection of Waters Road. The property is currently a wooded site located between Waters Road and a Bell Atlantic office building directly across from an existing MTA parking lot.

We would appreciate a determination regarding any historical sites located on the property so that we may proceed with our Reforestation Plan submittal.

Please notify us if we can provide any further information or if we can be of any assistance.

Sincerely,

ADC Msp 13 10/13

WHITNEY, BAILEY, COX & MAGNANI

David Powlen *Germantown*
David C. Powlen

DCP/slw
9304303L.002

Arch: 1A - BC 9/19/94

*Ipe
Germantown det. also
inherit of elig. HD. However,
was determined in 1979 and
Ron Andrews believes the
district has been blotted.
Would like photos to
confirm*

MEMO

STATE OF MARYLAND

Department of Housing and Community Development
Division of Historical and Cultural Programs
Maryland Historical Trust
100 Community Place
Crownsville, Maryland 21032-2023
(410) 514-7600
FAX # (410) 987-4071

TRANSMITTAL SLIP

TO: Kobin Zeck

RE: Greenwood MTHA lot

FROM: Beth Kanowitz

FAX #: (301) 495-

DATE: 6/26/95

PAGES (including cover): _____

DUE: _____

<input type="checkbox"/> Please Approve & Return	<input type="checkbox"/> For Your Signature
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<input checked="" type="checkbox"/> For Your Information	<input type="checkbox"/> Please Take Charge

REMARKS:

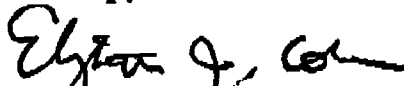
Found the rest of the file.
I apologize. It looks as though, mad. the determination without
consulting or even copying you all! Don't know what
I was thinking
Does appear clear that only commented on Western site.
While Beth said don't believe! district is eligible, could
reconsider if you all felt ~~that~~ incorrect & provided explanation
Also, would address possibility of individual or smaller
or smaller grouping of properties that might be affected by Eastern
site.
Have called to MTHA to ask about status

Mr. David C. Powlen
September 27, 1994
Page 2

architectural and archeological resources. Part of the review process involves consultation between the agency (or its designee) and our office to identify and evaluate historic properties that may be affected by the project and to develop measures to avoid, reduce or mitigate any adverse effects on significant historic properties.

If you have questions or require further assistance, please call Ms. Elizabeth Hannold (for structures) at (410) 514-7636 or me (for archeology) at (410) 514-7631.

Sincerely,



Elizabeth J. Cole
Administrator, Archeological Services

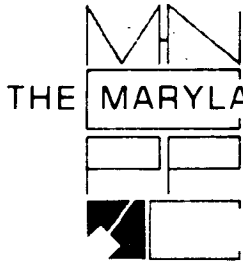
EJC/EAH
9402328
Enclosure

cc: Ms. Diane Ratcliff (MTA)
Hon. Gilbert Gude
Dr. Thomas King
Mr. Mike Seebold
Ms. Gwen Marcus
Ms. Marie-Regine Charles-Bowser

Sept. 24, 1994
HHT letter
to David Poller
of Whitney, Massey, Cox &
Masponi

Post-it™ Fax Note	7671	Date	6/26/95	# of pages ▶	7
To	Beth Hammed	From	Robin Zick		
Co./Dept.	MIT	Co.	M-NCPCC		
Phone #	617 514-7636	Phone #	495-4570		
Fax #	410 984-4071	Fax #	301-495-1317		

Robin the stuff
He's a
German
for I couldn't
that find
Gover



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
8787 Georgia Avenue • Silver Spring Maryland 20910-3760

May 30, 1995

*Comments to John
by 6/15/95*


MEMORANDUM

TO: Joe Anderson, Coordinator
Environmental Planning Division

Gwen Marcus, Coordinator
Design, Zoning and Preservation Division

Ki Kim, Planner
Transportation Planning Division

Brooke Farquhar, Planner
Development Review Division

FROM: John Matthias, Coordinator 
Transportation Planning Division

SUBJECT: Mandatory Referral on Germantown MARC Station Parking
Garage

Attached is a copy of the report prepared for the Maryland Mass Transit Administration which evaluates the two alternative sites. Also attached is a copy of a letter from Agro of MTA indicating that this project would be considered by the Planning Board. I have scheduled this review for June 29.

I have scheduled a meeting for 10:00 a.m. on Friday, June 2, in the Environmental Planning Division Conference Room. The purpose of this meeting is to identify the issues which should be addressed in the staff report and to decide what work, if any, needs to be done to prepare responses to those issues.

Based on previous discussions during the preparation of the staff report for the Germantown MARC Station parking lot, my impression is that the site for the new lot is preferred over the one closer to the platforms. This preference has to do with impacts on the historic resources in the area and the cost per additional space.

JM:kcw
Attachments

cc: Callum Murray

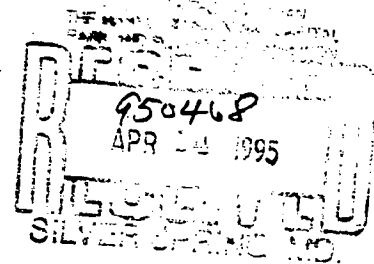


MARYLAND DEPARTMENT OF TRANSPORTATION

MASS TRANSIT ADMINISTRATION

300 West Lexington Street • Baltimore, Maryland 21201-3415

April 10, 1995



Mr. William H. Hussmann, Chairman
Maryland-National Capital Park and
Planning Commission
8787 Georgia Avenue
Silver Spring, MD 20910-3760

**RE: GERMANTOWN PARKING LOT EXPANSION
CONTRACT NO. MTA-4316-103**

Dear Mr. Hussmann:

The Secretary of Transportation, David Winstead and I would like to thank you for your letter dated March 6, 1995 concerning the above mentioned MARC Station's parking lot expansion and the commission's timely review of the project.

The Mass Transit Administration's (MTA) Office of Engineering has reviewed the recommendations and will:

- 1.) Incorporate within the plans a double row of trees along the site's frontage of Relocated MD. 118.
- 2.) Eliminate the fourteen (14) parking spaces adjacent to existing MD. 118 and add the area to the landscaped pedestrian area to be developed adjacent to the bridge over the railroad line.

The site selection for the Germantown parking garage and the platform improvements, which are two (2) independent projects, will be presented to the Planning Board at a future date.

The MTA will support and coordinate with the Montgomery County Department of Transportation as they develop their plans for the landscaped pedestrian/bicycle crossing over the railroad line at the existing MD. 118.

My phone number (410) 333-3885

FAX number (410) 333-3279


TDD for people who are hearing and/or speech impaired 539-3497



Mr. William H. Hussmann
April 10, 1995
Page Two

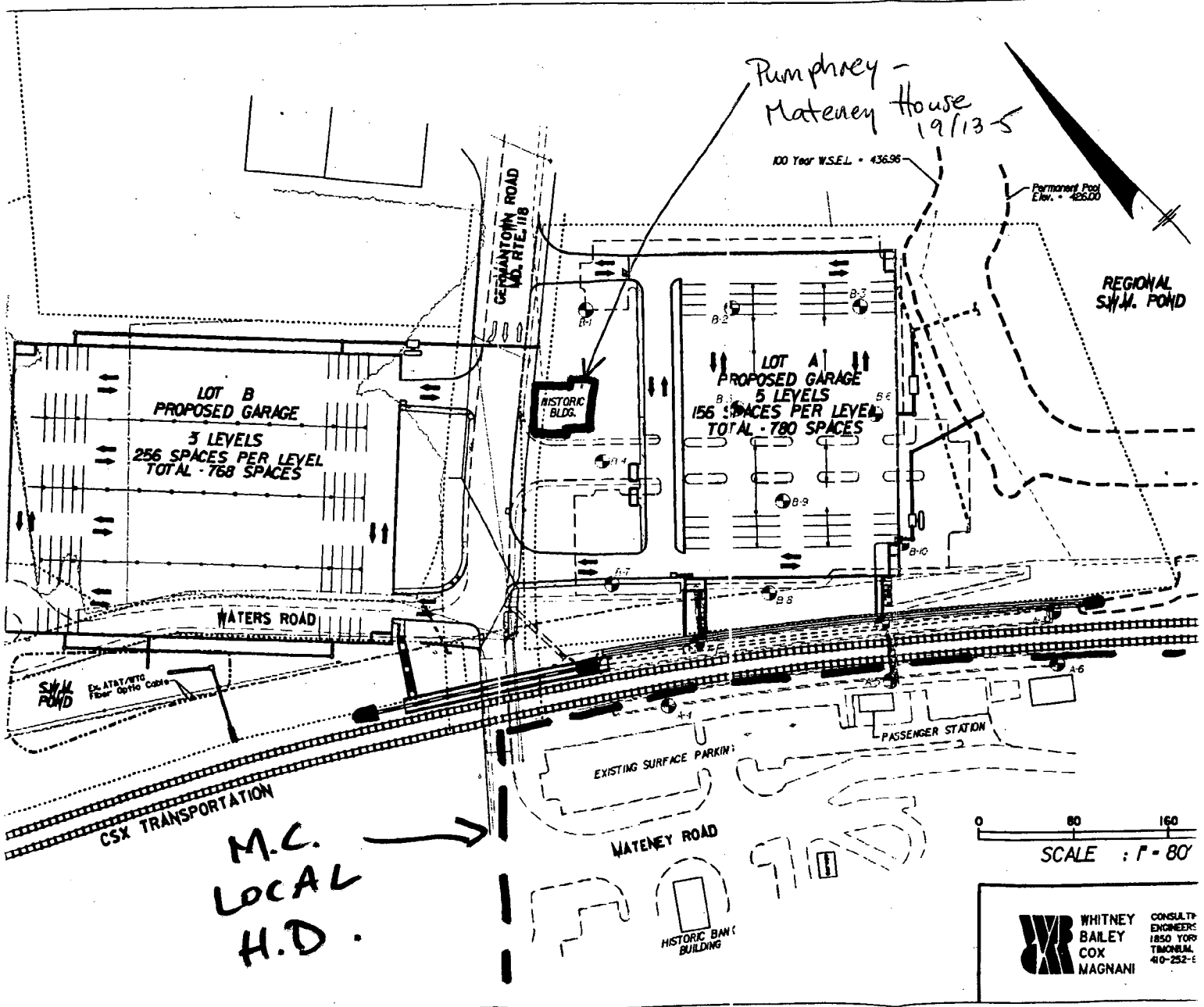
It has been a pleasure to work with the Planning Board and the MTA looks forward to working with the Planning Board on the future projects. If you have any questions, please do not hesitate to contact me.

Sincerely,


John A. Agro, Jr.
Administrator

JAA

cc: David L. Winstead
Secretary



Pumphrey -
Mateney House
19/13-5

100 Year W.S.E.L. • 436.96

Permanent Pool
Elev. • 426.00

REGIONAL
SWIM POND

LOT B
PROPOSED GARAGE
3 LEVELS
256 SPACES PER LEVEL
TOTAL - 768 SPACES

LOT A
PROPOSED GARAGE
5 LEVELS
156 SPACES PER LEVEL
TOTAL - 780 SPACES

HISTORIC
BLDG.

WATERS ROAD

SINK ROAD
Fiber Optic Cable

CSX TRANSPORTATION

M.C.
LOCAL
H.D.

EXISTING SURFACE PARKING

PASSENGER STATION

MATENEY ROAD

HISTORIC BANK
BUILDING

0 80 160
SCALE : 1" = 80'

WB WHITNEY
BAILEY
COX
MAGNANI
CONSULTING
ENGINEERS
1850 YORK
TIMONUM, MD.
410-252-1100

Germantown Parking Garage Alternative Site Analysis Report

Submitted to:
Maryland Department of Transportation
Mass Transit Administration

June 30, 1994

Prepared by:

Whitney, Bailey, Cox & Magnani
1850 York Road
Timonium, Maryland 21093

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ALTERNATIVE SITE ANALYSIS REPORT

GERMANTOWN PARKING GARAGE

A. INTRODUCTION

The purpose of this Alternative Site Analysis is to document the preliminary analysis conducted for the potential construction of a parking garage along existing MD Route 118 at the CSX Railroad crossing. This area is the site of an existing MARC train station (see Figure 1). Two potential sites were analyzed. The first site is in the northeast quadrant of the intersection between the CSX Railroad and MD Route 118 and the second site is in the northwest quadrant. The first site will hereafter be referred to as Lot 'A' and the second site as Lot 'B' (see Figure 2). The report addresses the following topics:

- Existing Site Conditions
- Zoning and Historic Requirements
- Site Options & Utility Impacts
- Structural Options
- Geotechnical Data
- Traffic Analysis
- Construction Cost

Through discussions with Mr. Edward Daniel, Special Assistant to the Director of the Montgomery County Department of Transportation, it is apparent that only two parking levels above ground will be acceptable on either lot due to historic considerations and community acceptance. Therefore, this study focused on maximizing the number of spaces that can be obtained based on this requirement. This report addresses the findings of that study.

B. EXISTING SITE CONDITIONS

Lot 'A' is the site of an existing park-n-ride facility. The lot slopes downward from MD Route 118 at approximately a 5.5% grade and is situated 15 to 20 feet higher than the railroad tracks. The park-n-ride is a 375-car facility with approximate dimensions of 220 feet by 290 feet. In the northwest corner of the lot is a historic house which is currently being used as a commercial office. When the lot for the park-n-ride facility was purchased by Montgomery County, this house was restored by the County and leased for 99 years. East of the park-n-ride facility is an existing regional stormwater management pond owned by Montgomery County.

Lot 'B' is a wooded tract of land approximately 230 feet by 260 feet. The land slopes at approximately 4% from north to south. Immediately to the south of the wooded lot, between the lot and the railroad is an existing road (Waters Road). This road is owned and maintained by the State Highway Administration. Between Waters Road and the railroad is a tract of land varying in width from 35 feet to 105 feet which is currently owned by the CSX railroad. Montgomery County is in the process of purchasing the wooded lot. The tract of land between Waters Road and the railroad is to be purchased by the MTA, and the two sites together with the Waters Road right-of-way will be the site of a 300-space MARC parking lot to be constructed by the MTA. MD Route 118 is proposed to be relocated west of the wooded lot. Since the MD Route 118 relocation will sever Waters Road, this section of roadway will not serve any properties. SHA District Engineer, Creston Mills confirmed that this portion of roadway can be abandoned. Projected advertisement date for MD Route 118 is May, 1995.

The Germantown Master Plan, prepared by the Maryland-National Capital Park and Planning Commission (MNCPPC), approved and adopted in July 1989, states, "When Relocated MD Route 118 is open to traffic, the existing bridge will be either closed to automobile traffic or replaced by a pedestrian bridge." Conversations with SHA indicated that, part of the overall plan is to transfer the Old MD Route 118 to the County when the relocation of MD Route 118 is complete. Therefore, it will be a County decision on closing the bridge.

Access to Lot 'B' from Relocated MD Route 118 is currently under review by SHA Access Permits Division. Approval for a median break in Relocated MD Route 118 must be approved by the Chief Engineer.

C. ZONING AND HISTORIC REQUIREMENTS

There is a locally-designated historic district in Germantown (County-designated) located southeast of the CSX Railroad and MD Route 118 crossing. It is the one area in Germantown with an intact ensemble of historic resources that recall an overall historic ambiance. It is the heart of the late 19th Century-early 20th Century Germantown, and the Germantown Master Plan recommends that the area be preserved.

The Germantown Master Plan also recommends that

"...any subdivision or site plan in the areas bordering on and adjacent to the historic district be given careful consideration in terms of its impact on the historic district. In addition, more detailed consideration of the buffering issue is needed, and the development of a "buffer" zoning

classification or overlay zone, to provide the necessary design guidelines and review, may be warranted in the future."

In addition to the Germantown Historic District, there are individually designated historic structures (resources) outside the historic district, yet still in the Germantown area. A number of these structures are located along the Maryland Route 118 corridor, and any future development adjacent to these historic houses should respect these individually-designated houses.

Both lots 'A' and 'B' are zoned R-200. Normally, a parking garage requires a special exception for the use on property zoned R-200. However, since the facility will be owned and operated by a public entity, no special exception is required. The use still must be reviewed by the appropriate departments via the **mandatory referral process**. There is no checklist per se for this review process. The requirement is that plans be submitted to the Chairman of the Montgomery County Planning Board, William Hussman. In sixty days or less, plans will be reviewed by the Planning Board at a public hearing. The review of plans will be supervised by the Transportation Division in the MNCPPC. This department will distribute the plans to other departments for comment, including the County's Historic Preservation Commission.

With respect to zoning restrictions, the minimum building setback from the street is forty feet. Setbacks from adjoining lots areas follows: twelve feet on one side (provided the sum of both sides is not less than 25 feet), and the rear is 30 feet (see Figure 3). Since the garage will only be two levels above grade, the minimum setback requirements stated above apply.

The maximum percentage of net lot area that may be covered by buildings, including accessory buildings, is 25 percent. Mr. Edward Daniel does not know if this criteria can be waived by the County; but if so, the County would waive the maximum 25-percent lot coverage, but would want to adhere to the setback criteria. In a telephone conversation with Ms. Denise Boswell of MNCPPC, she stated that MTA would need to petition the Board of Appeals for a variance from the maximum 25-percent lot coverage requirement. This applies even if the garage is publicly owned and operated. The MTA must prove that the existing zoning imposes a hardship.

D. SITE OPTIONS

1. Lot 'A'

To maximize the number of spaces based on the maximum two levels above ground restriction, the option of constructing two levels below ground was studied. The lower level would be at the same approximate elevation (Elev. 430) as the railroad station platform providing direct access to the station platform. This elevation is above the permanent water surface elevation (Elev. 426) of the existing regional stormwater management pond. This option would provide a five-level garage with two of the levels below ground. The maximum garage size that can be constructed on this lot based on minimum setback requirement is 182 feet by 272 feet (see Figure 4).

Construction of the parking garage on Lot 'A' would displace the entire existing park-n-ride facility. Handicap parking would need to be provided in the garage. A kiss-n-ride and bus stop area could be provided along the west side of the garage adjacent to the historic building. Access to the stormwater management facility would be provided by access around the north and east sides of the parking garage. Two fiber optic lines are located along the southwest side of the lot adjacent to the railroad. These lines would require relocation.

This garage would provide 156 spaces per level, or a total of 780 parking spaces.

2. Lot 'B'

Using minimum setback requirements, a garage size of 242 feet by 326 feet can be obtained using the wooded lot, Waters Road, and a portion of the lot that MTA is purchasing from the CSX railroad. Based on only two levels above ground and one at grade, a total of 768 parking spaces can be provided in this garage, 256 spaces per level (see Figure 5).

On this site, the train platforms may be extended to the west under the existing MD Route 118 bridge with new stairs from the garage site for more direct access. Without the platform extension, the walking distance to the existing platforms would be 450 feet.

The lot adjacent to the railroad is much wider on the west end (105 feet) versus 35 feet at the east end.

The option of using an additional portion of this lot at the west end was studied, however, was found not to be cost-effective because very few additional spaces would be provided.

The construction of a garage on Lot 'B' would require the relocation of several utility poles along MD Route 118. There are two also fiber optic cables parallel to the railroad. Although it appears that the garage itself would not impact the cables, construction of the stairs and stormwater management pond would affect the cables.

The top floor elevations for either garage (Lot 'A' or Lot 'B'), with only two levels above ground, would be lower than the height of the existing historic house (see Figure 6).

E. Stormwater Management/Environmental Requirements

Environmental design requirements, as outlined by the Montgomery County Department of Environmental Protection, generally apply to parking garage facilities constructed within Montgomery County. The proposed garage locations considered in this feasibility study will require that the following environmental systems be incorporated in the design of the parking garage facility, regardless of locations.

1. Top Parking Deck

The top deck of the parking garage is considered analogous to an on-grade parking lot, having the same surface area, subject to stormwater quality control. Therefore, all stormwater drainage associated with the uppermost or top deck must be conveyed to a sandfilter water quality structure (SFWQ) with outfall connections to the project's stormwater management facility. Design criteria for design of the SFWQ is based on a peak discharge for a 15-year storm event, and storage for the first flush runoff of 0.5 inches over the top deck surface.

2. Below Top Deck Parking Levels

Parking lot levels below the top deck are not subject to drainage from stormwater events. These parking levels will have their drainage systems piped from floor drains, with an outfall connection to the nearest available public sanitary sewer system preceded by an on-site oil interceptor with separate waste oil holding

tank. Vehicular access to the oil interceptor and the underground holding tank should be provided for routine maintenance services. Preliminary sizing of the oil interceptor is based on 35 gpm, which should adequate for either structure.

For Lot 'A', quantity stormwater management would not be required since this site drains to the existing regional stormwater management pond today.

The stormwater management requirements for Lot 'B' were studied. The existing runoff flows toward the railroad tracks and under the northeast platform via a storm drain, and does not enter the existing regional stormwater management pond. Using computations for the existing stormwater management pond obtained from Montgomery County, the proposed lot drainage was routed into the pond. Adding this additional drainage into the pond increased the release rate for the 2-year storm by 7.66 cfs, or a 36 percent increase. The 10-year storm release rate increased by 23.80 cfs, or 7 percent increase, and the 100-year storm release rate increased 21.12 cfs, or 3 percent. During a telephone conversation with Mr. Mike Geier of Montgomery County Environmental Protection, he indicated that the increase in the 2-year storm release rate was unacceptable. Modifications to the release structure, which could lower the 2-year release rate, should be examined during final design; however, Mr. Geier indicated that on-site stormwater management of Lot 'B' may be preferable. Analysis of this site indicated that a surface stormwater management pond could be constructed on the southwest corner of the lot; however, this location of the pond would require relocation of the fiber optic cables owned by AT&T and Wiltel Communications. In lieu of the surface lot, an underground stormwater management structure could be constructed to avoid relocation of the fiber optic cables.

F. STRUCTURAL OPTIONS

Structural systems that were considered include precast concrete and cast-in-place concrete. We recommend a precast concrete system based on the following advantages over cast-in-place concrete:

- Lower cost per square foot
- Minimize user inconvenience
- Better quality control of finished product
- More suitable to expansion or modification.

Even though neither site is within the Historic District, the garage will be subject to review by the Montgomery

County Planning Board, which will consider the impacts of the garage on the historic district. Ms. Nancy Witherall, of the Design Zoning and Preservation Division of the Maryland National Capital Parks and Planning Commission, has indicated that the garage may not need to be enclosed; however it should be attractive and appropriate. Therefore, the options of an open structure or enclosed structure were studied.

The open garage would consist of a structure with parapet/barriers designed as part of the spandrel beam system. The open structure will maximize the benefits of natural lighting and ventilation. The spandrel beams could be designed to accommodate a brick veneer system. Using a brick veneer on the spandrels and at the stair towers, in combination with sloped roofs over the stair towers, may be adequate to provide an attractive facade for the reviewing agencies. Should the reviewing agencies require one or two completely closed facades, the code will allow two closed sides and still consider the structure an open garage. Should the reviewing agencies require that all sides of the garage be enclosed by the facade, then the structure would be classified as a public garage. This classification has more stringent requirements for fire protection, ventilation, lighting and life safety.

Two-way and one-way traffic patterns were considered within the garages. We recommend a two-way pattern for the following reasons:

- Allows more options for the driver and quicker parking access.
- Does not require interconnections between ramps.
- Allows simpler, less costly structural system.

Parking stalls will be perpendicular since diagonal parking is a less effective parking configuration.

The structural system would consist of precast, prestressed concrete double tees spanning between precast concrete girders that are supported by precast concrete columns. The foundations and ground floor would be cast-in-place concrete. The typical framing bay would be 60 feet by 27 feet. This bay can accommodate three 8½-foot wide parking spaces at each end with a 20 foot wide lane in the middle. The garage sizes will be based upon multiples of this bay size. The depth of the structural system would be approximately 3 feet and the recommended clearance is 7 feet. Therefore, the floor-to-floor or story height used in this study is 10 feet. The at-grade floor on Lot 'A' would provide a 9-foot clearance to accommodate handicap vans.

Based upon the lot sizes and the set-back requirements, the garage footprint at Lot 'A' would be 182 feet wide and 272 feet long (see Figure 7). At Lot 'B', the garage footprint would be 242 feet wide by 326 feet long (see Figure 8).

The garage footprint at Lot 'A' will accommodate 156 parking spaces per floor. With five parking levels, 780 spaces would be provided. It was decided to place two levels below grade to take advantage of the grade differential adjacent to the railroad tracks. The remaining three levels would be at or above grade and the building height would be approximately 23 feet, except that the stair and elevator towers would project above the uppermost parking deck. The lowest parking level would be nearly the same elevation as the station platform level and gives the owner the opportunity to create a plaza area with landscaping, kiosks and seating adjacent to the tracks.

The garage footprint at Lot 'B' will accommodate 256 parking spaces per floor. With three parking levels, 768 spaces would be provided. The entire parking structure would be at or above grade and the building height would be approximately 23 feet, except that the stair towers would project above the uppermost parking deck.

The building code for open parking structures requires provision of a minimal amount of lighting. Fire protection requirements would consist of fire hose stations at certain intervals within the garage. The underground parking levels at Lot 'A' require sprinklers, ventilation and higher lighting levels.

Should the reviewing agencies require a completely enclosed structure, the garage would need complete sprinkler protection, higher lighting levels and be completely ventilated.

Life safety requirements in the building code for open garages prescribe a minimum of two exits with a maximum travel distance of 300 feet. The minimum of two stair towers will satisfy this criteria for both garages. Additionally, we are recommending that an elevator be provided for the garage on Lot 'A' due to the number of stories of the structure and to access the van handicap parking on the at-grade level.

Should an enclosed structure be required, the life safety requirements may be more stringent than that required for an open parking garage. These requirements would require a thorough code review during the final design, after the determination of the building classification is made.

The ADA requirements for parking structures of this size (501 to 1,000 parking spaces) mandates that 2 percent of the total number of spaces be handicap accessible. Based on the total number of spaces provided (780 or 768), 16 spaces would be required. The code does allow all of the spaces to be clustered in one area. The garage at Lot 'A' would accommodate the handicapped parking on the lowest parking level, which would permit easy access from the northbound platform without a major ramp system, except that the handicap vans would be restricted to the at-grade level. However, the handicap vans could be allowed through the lower parking levels if the story heights were increased by two feet. This change would require adjusting the grades around the building for the at-grade parking level. The costs to increase the story heights would be minimal. On Lot 'B', handicapped parking may not be required since an adequate number of spaces can be provided at the existing park-and-ride lot, which would be closer to the station platform. However, handicapped parking, except handicap vans, could easily be accommodated within the garage on Lot 'B'.

G. GEOTECHNICAL DATA

The sites are located in the Piedmont region of the State. This area is generally characterized by residual soils overlaying rock strata. The rock can generally be found 10 to 30 feet below the surface. The garages considered in this study will have column loads in the range of 800 to 1200 kips. The residual soils will generally have low (2 to 4 KSF) bearing capacities. The low capacities would require large spread footings should a shallow foundation system be selected. We recommend that a deep foundation system be utilized for the garages. This would consist of caissons or drilled piers that would bear on the rock strata. The rock would generally provide bearing capacities in the 10 to 12 KSF range.

Review of the shallow borings (5 to 10 feet) taken for the MD Route 118 extension indicate that the soils are generally sandy silt and silty clay. At one location, the rock strata was encountered at elevation 448.0 feet; however, this elevation is approximately 12 feet below the finished grade of the garage on Lot 'B'. This boring was approximately 600 feet northeast of Lot 'B'.

Examinations of the shallow borings (10 feet) taken for construction of the existing parking lot (Lot 'A') show that no rock was encountered. Rock excavation is not anticipated at the southwest end of the garage; however, there is a possibility for rock excavation at the northeast end of the

garage since the existing borings ended 10 feet above the proposed bottom floor.

Further geotechnical investigations will be required at the specific site locations during final design.

H. TRAFFIC ANALYSIS

A formal Traffic Impact Study was prepared by WBCM under separate cover for assessing traffic impacts associated with the various siting options of the proposed parking structure. Data presented herein represents a summary of the key findings.

Analyses of the siting options were based on non-site-generated traffic data developed by the Maryland National Capital Park and Planning Commission (MNCPPC). The projections reflect design-year 2003 conditions assuming MD 118 relocated complete and open to traffic as shown in Figure 9. The new roadway will attract through trips from existing MD 118 and reduce turning movements along Wisteria Drive. As a result, existing MD 118 north of Clopper Road would serve only local traffic. Included as components of the projected non-site traffic data developed by MNCPPC are regional traffic growth and site-specific local development activity. Trips destined for the proposed parking structure and the existing park-n-ride lots represent site-generated trips and were not included in the MNCPPC computations.

The year 2003 projections were reduced to year 2001 figures (the assumed design year of the parking structure) by WBCM for subsequent analyses.

Anticipated trip generation of the proposed parking structure was based on hourly traffic volume counts conducted at the driveway of existing Lot A. Based on current operations, local trip generation per parking space of the park-n-ride is as follows:

Trip Generation Rates

Germantown Park-n-Ride

PERIOD	ENTER	EXIT
AM Peak Hour	0.47	0.14
PM Peak Hour	0.16	0.58

Applying the trip generation factors to the proposed 800-space facility yields the following trip generation.

Trip Generation Summary
800-Space Parking Structure

PERIOD	TRIP GENERATION	
	ENTER	EXIT
A.M. Peak Hour	376	112
P.M. Peak Hour	128	464

Based on existing traffic dispersion, approximately 67 percent of the site-generated trips will be oriented to/from residential development south of the site. Trip attractions to the north along MD 118 account for 16 percent of the distribution; 11 percent is oriented to/from the west on Wisteria Drive and 6 percent to/from the east on Wisteria Drive. Design-year 2001 total traffic volumes were derived by adding the adjusted non-site traffic to the trips generated by the new facility and the existing park-n-ride lots.

Capacity analyses were conducted to determine impacts to the intersections of Wisteria Drive with relocated and existing MD 118 caused by the projected non-site traffic (traffic which would be on the roadway network even if the parking structure and other MTA lots were not present). Once potential capacity constraints were identified and measures determined to accommodate the non-site traffic, a second set of studies were conducted to assess the impacts of site-generated trips plus non-site traffic. Analyses were based on the design-year lane geometry with MD 118 relocated in place and open to traffic.

Non-site analyses indicate that the relocation of MD 118 and corresponding redistribution of through-traffic significantly reduces the turning movements along Wisteria Drive. As a result, projected increases in non-site through-traffic along Wisteria Drive can be accommodated by a reassignment of lane use within the existing four-lane section. However, improvements would be required along northbound, relocated MD 118 to provide a double left-turn lane at Wisteria Drive given anticipated queuing of non-site traffic in the evening peak hour. The additional turn lane

would be required even if the parking structure was not constructed.

Four access scenarios for the new parking structure were considered in assessing impacts with total traffic volumes (non-site plus MTA-generated trips). In each scenario, it was assumed that the new facility would be constructed to the west of existing MD 118 (Lot 'B'). A western siting offers the advantage of potential access to relocated MD 118. The additional access point would serve to alleviate potential congestion at local intersections by redistributing site-generated traffic directly to relocated MD 118. Access to relocated MD 118 could be provided with a Lot 'A' siting; however, it would be difficult to restrict the access from existing MD 118 to MTA patrons only. The surface lot would become a short-cut for non-site traffic accessing relocated MD 118--a major disadvantage.

The access scenarios considered in the analyses for Lot 'B' are noted below:

1. Existing MD 118 would remain open to vehicular traffic. Access to the new facility would be provided across from the existing northern park-n-ride lot on existing MD 118.
2. Existing MD 118 would be closed to vehicular traffic at the CSX railroad bridge between the existing park-n-ride lots. The existing CSX bridge would only serve pedestrian traffic. Access to the new facility would be provided across from the existing northern park-n-ride on existing MD 118.
3. Existing MD 118 would remain open to vehicular traffic. Site access would be provided on existing MD 118 across from northern park-n-ride lot and from relocated MD 118.
4. Existing MD 118 would be closed to vehicular traffic at the CSX bridge between the existing park-n-ride lots. Full access from relocated MD 118 and across from existing northern park-n-ride on existing MD 118 would be provided.

Results of the analyses for each scenario are noted below:

Scenario 1: Trips generated by the proposed facility can be accommodated by the design-year lane geometry assuming the northbound approach of relocated MD 118 at Wisteria Drive is improved to provide a double left-turn for non-site traffic as

noted above. The additional lane would be required even if the parking structure was not constructed. Widening of the narrow CSX bridge along existing MD 118, between the park-n-ride lots, and a widening of existing MD 118 would be required to provide a left-turn lane into the new facility, assuming a Lot 'B' siting, thus ensuring efficient traffic flow along northbound MD 118. Currently, the roadway widens north of the CSX bridge to provide a right-turn lane into the park-n-ride. The limited widening would not be sufficient to accommodate a left-turn lane; thus, the additional improvements would be required.

Scenario 2: As noted above, 67 percent of the site-generated trips are oriented to/from the south; therefore truncating existing MD 118 at the CSX bridge would cause redistribution of a major component of the site-generated trips. Turning movements would balloon to over 600 vehicles at the Wisteria Drive intersections necessitating the construction of a double left-turn lane westbound and an additional northbound right-turn lane to accommodate the potential queuing of site generated traffic. Also, an additional left-turn lane along northbound relocated MD 118 would be required at Wisteria Drive to accommodate projected non-site traffic. The additional lane would be required even if the parking structure was not constructed. An advantage of Scenario 2 is that the northbound left-turn lane and associated bridge repairs and roadway widening along existing MD 118 would not be required.

Scenario 3: Access to the proposed parking structure from relocated MD 118 would divert traffic from existing MD 118 and eliminate the need for constructing turn lanes along existing MD 118 at Wisteria Drive. All site generated traffic could be accommodated by the improved non-site roadway network. Widening along existing MD 118 for a left turn lane would be required to accommodate site traffic from the south; however, the length of widening would be reduced over that of Scenario 1. Also, the additional left-turn lane along northbound relocated MD 118 would be required at Wisteria Drive to accommodate projected non-site traffic. The additional lane would be required even if the proposed parking facility was not constructed.

Scenario 4: The proposed facility would be the primary trip generator in the MD 118 corridor;

therefore, access to relocated MD 118 would mitigate the majority of turning movement demands created by the discontinuity of existing MD 118. Widening of the CSX bridge and existing MD 118 would not be required. However, additional widening could be required along westbound Wisteria Drive to accommodate the right turn movement at relocated MD 118. Also, the additional left-turn lane along northbound relocated MD 118 would be required at Wisteria Drive to accommodate projected non-site traffic.

The preferred alternate is Scenario 4 given it provides for the dispersion of site-generated traffic, thus reducing impacts to individual intersections. Elimination of bridge and roadway widening along existing MD 118 to provide a left-turn lane at the site access is another advantage of this alternate. A summary of the traffic impacts of each access scenario are noted below.

Traffic Analysis Summary

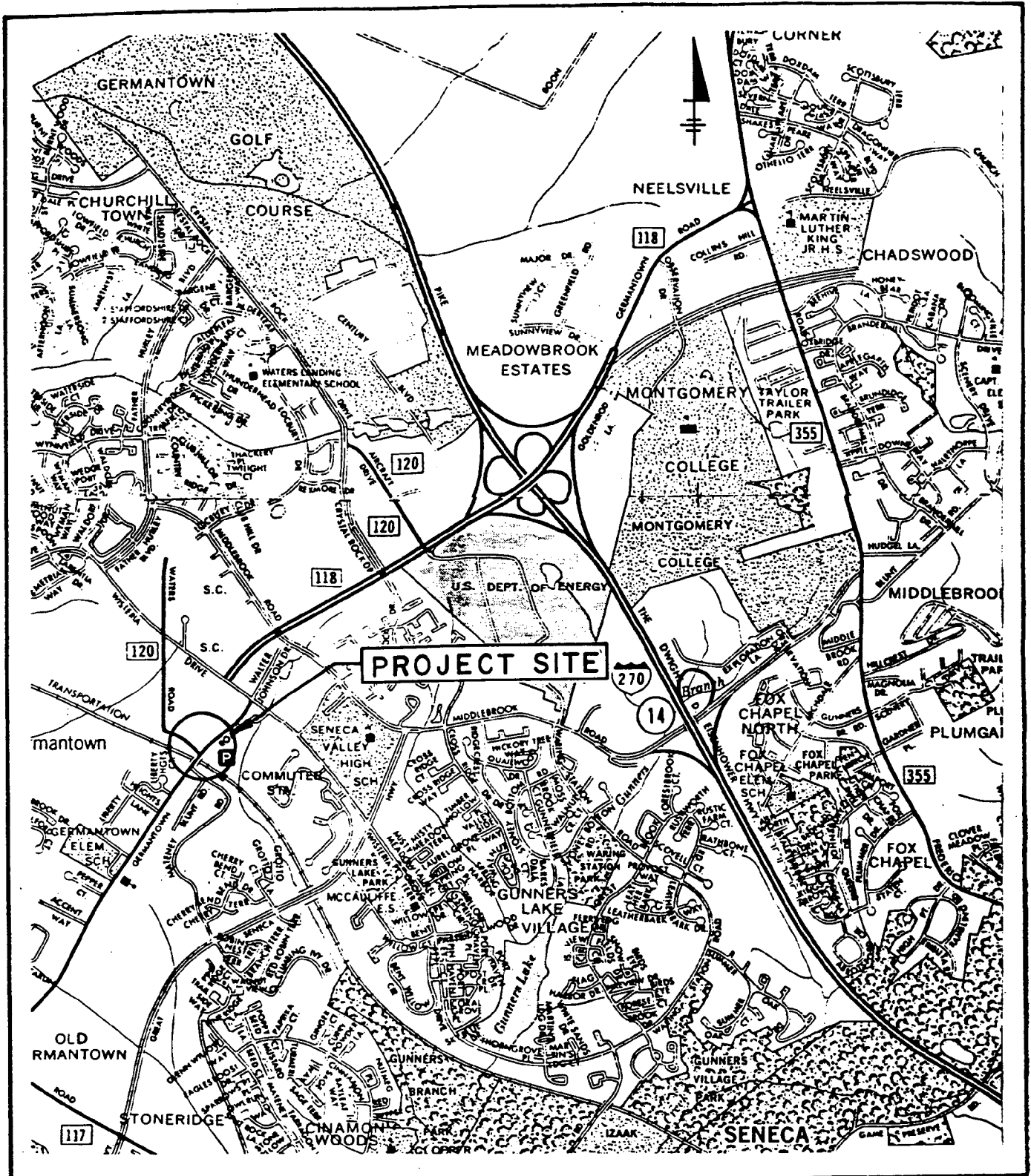
SCENARIO NO.	TRAFFIC IMPACTS
1. Existing MD 118 open. No access to relocated MD 118.	<ul style="list-style-type: none"> • Left-turn lane required on existing MD 118 at site access • No off-site improvements required for site trips
2. Existing MD 118 closed. No access to relocated MD 118.	<ul style="list-style-type: none"> • No improvement at site access • Off-site improvements required for site trips
3. Existing MD 118 open. Access to relocated MD 118.	<ul style="list-style-type: none"> • Site access improvements reduced • No off-site improvements required for site trips
4. Existing MD 118 closed. Access to relocated MD 118	<ul style="list-style-type: none"> • No improvements at site access • No off-site improvements required for site trips

I. CONSTRUCTION COST

The total estimated probable cost for Lot 'A' without a facade is \$16,472,000; and with a facade is \$17,599,000. For Lot 'B', the total estimated probable cost is \$14,062,000 without a facade; and \$15,858,000 with a facade. See Appendix 'A' for the cost breakdowns.

**ALTERNATIVE SITE ANALYSIS
GERMANTOWN PARKING GARAGE**

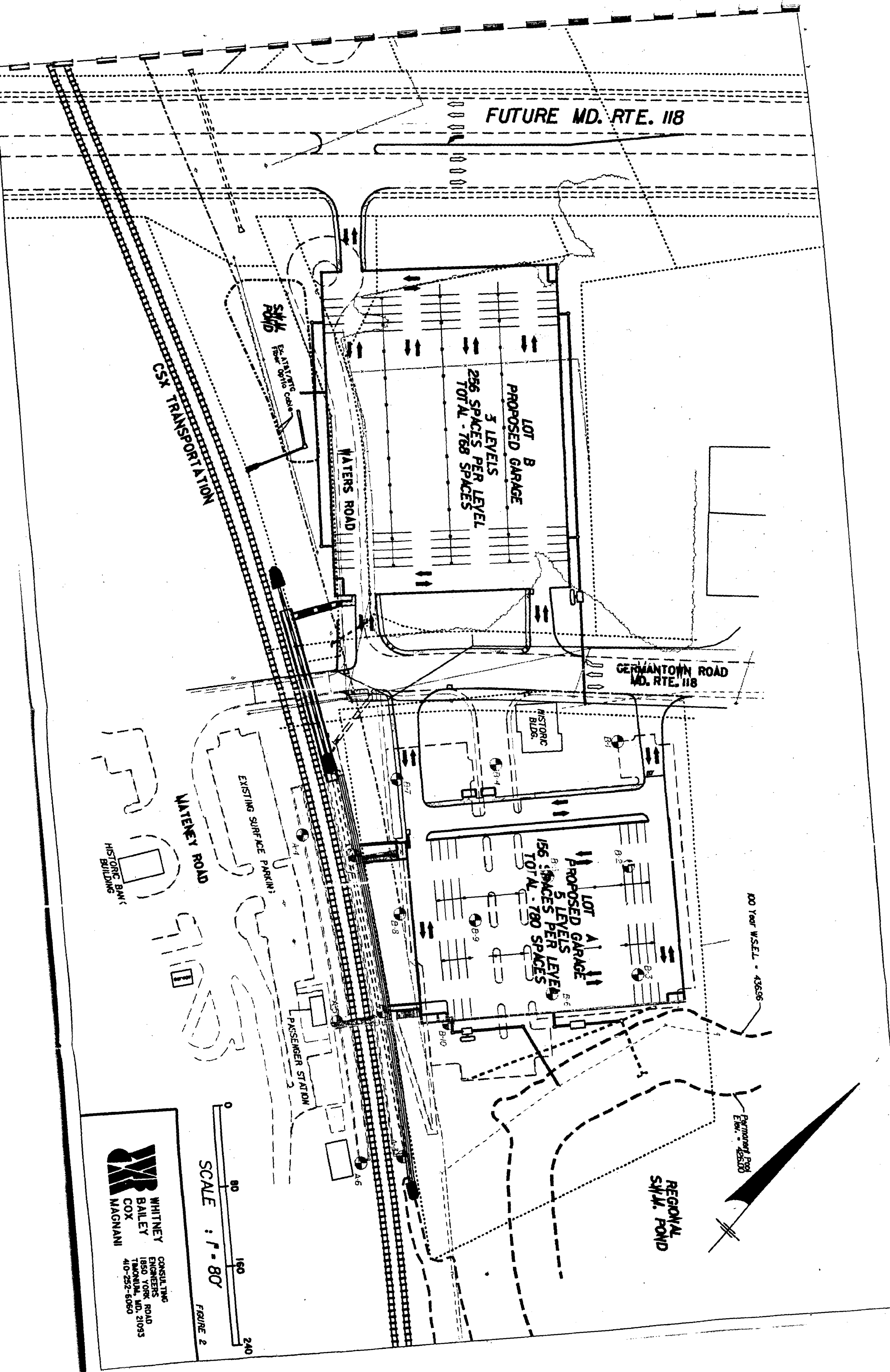
SITE FACTORS	LOT 'A'	LOT 'B'
Vehicular Access	From existing MD Rte 118	May be able to have access from relocated MD 118
Pedestrian Access to Station Platform	Direct access from bottom level or elevator	Access via stairs to extended station platform (450' walk distance to boarding area)
Historic Impacts	Proposed garage located directly across from the historic district. Also historic building on same site (80' from proposed garage structure)	Visual impact to historic district a concern.
Displacement of Existing Parking	All existing parking (375 spaces will be displaced)	Existing parking on proposed surface lot will be displaced (300 spaces)
Stormwater Management Quantity	Existing regional SWM pond (no quantity management)	Modifications to existing regional pond or new pond required for quantity management
Stormwater Management Quality	Sand filter water quality structure and oil interceptor	Sand filter water quality structure and oil interceptor
Cost Comparison w/o Facade	\$16,472,000	\$14,062,000
Cost Comparison w/ Facade	\$17,599,000	\$15,858,000



WHITNEY
 BAILEY
 COX
 MAGNANI

LOCATION MAP
 GERMANTOWN PARK-N-RIDE

FIGURE I



FUTURE MD. RTE. 118

CSX TRANSPORTATION

WATERS ROAD

GERMANTOWN ROAD MD. RTE. 118

MATENEY ROAD

HISTORIC BANK BUILDING

EXISTING SURFACE PARKING

PASSENGER STATION

LOT B
PROPOSED GARAGE
3 LEVELS
256 SPACES PER LEVEL
TOTAL - 768 SPACES

LOT A
PROPOSED GARAGE
5 LEVELS PER LEVEL
156 SPACES PER LEVEL
TOTAL - 780 SPACES

100 Year W.S.E.L. - 436.96

REGIONAL
S.W.M. POND

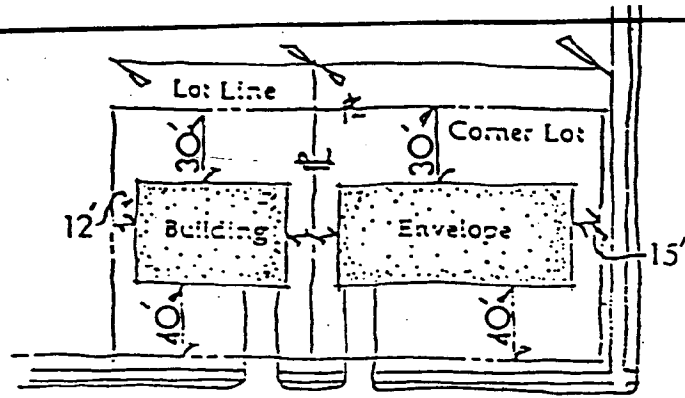
Permanent Pond
Elev. - 425.00



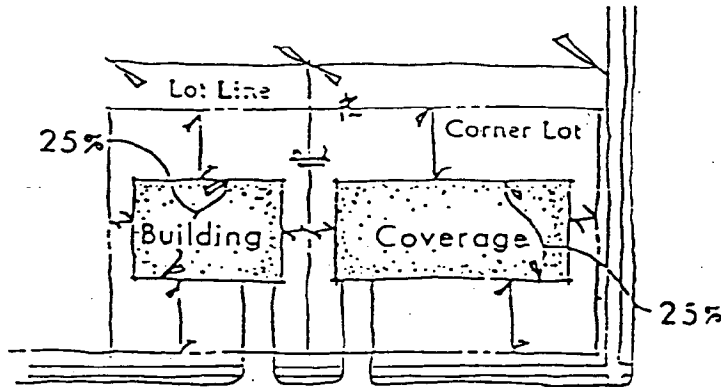
FIGURE 2

WMB
WHITNEY
BAILEY
COX
MAGNANI

CONSULTING
ENGINEERS
1850 YORK ROAD
TOWSON, MD. 21093
410-252-6060



Zoning Envelope



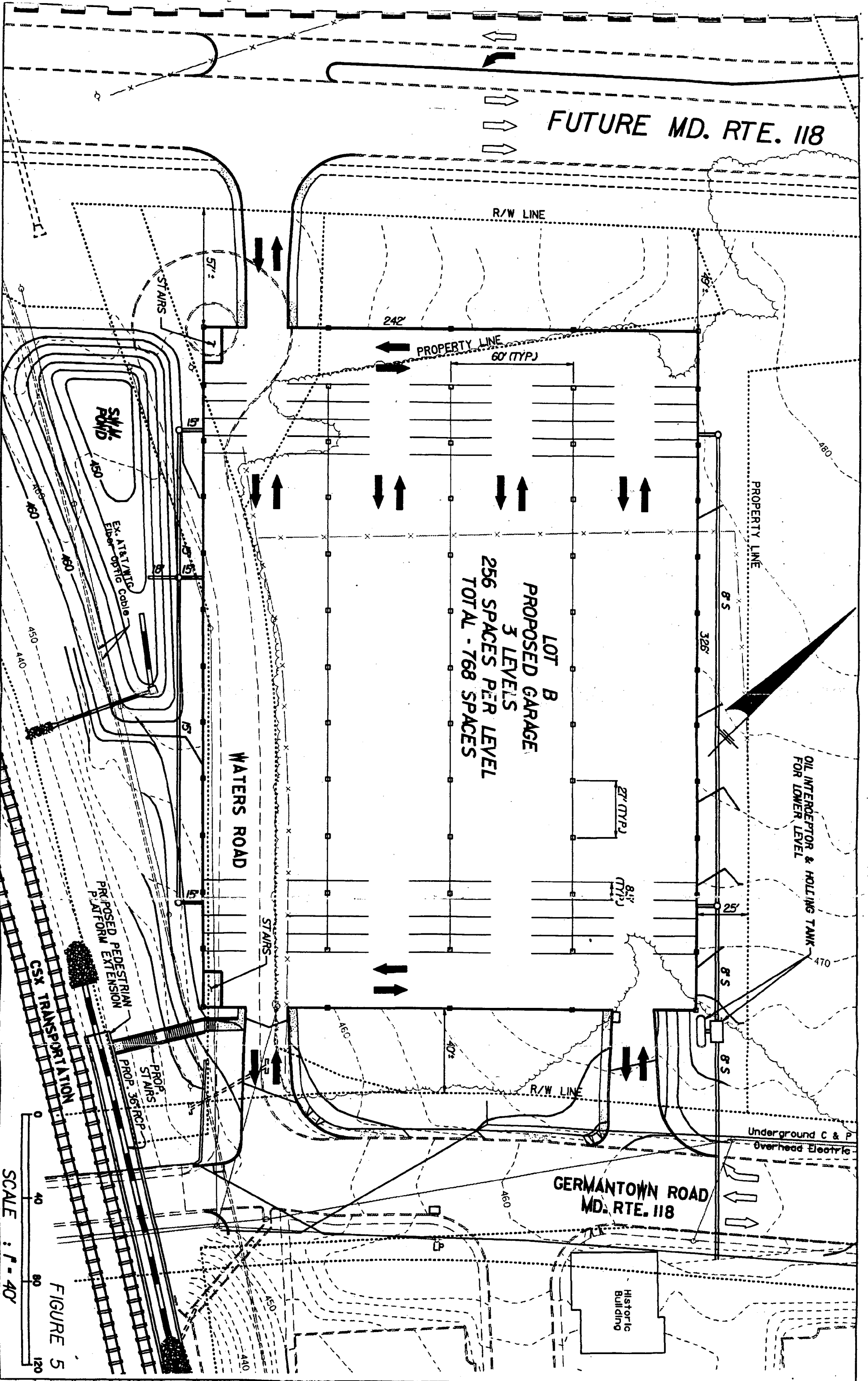
Zoning Envelope



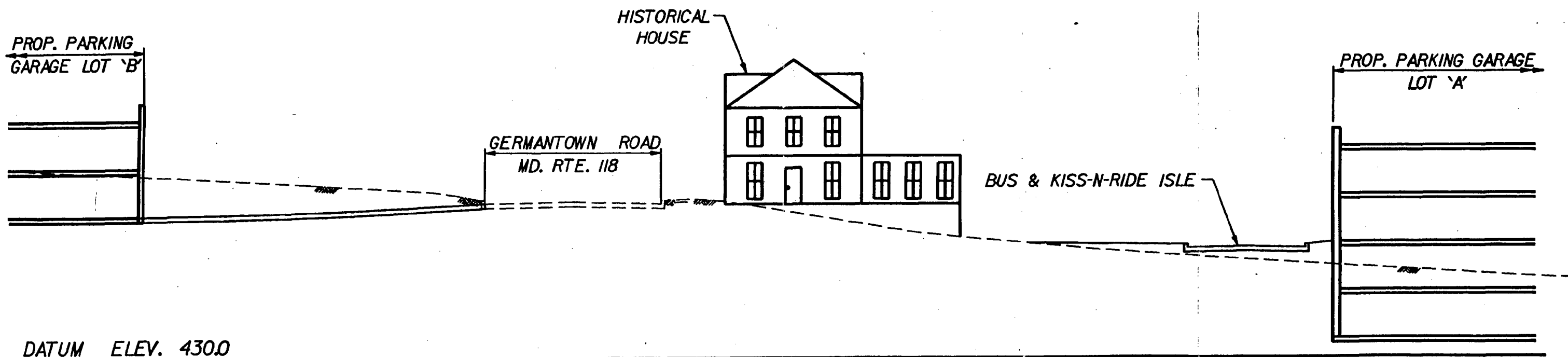
WHITNEY
BAILEY
COX
MAGNANI

ZONING REQUIREMENTS

FIGURE 3



SCALE : 1" = 40'
 FIGURE 5



CROSS SECTION THROUGH HISTORICAL HOUSE AND BOTH PARKING GARAGES

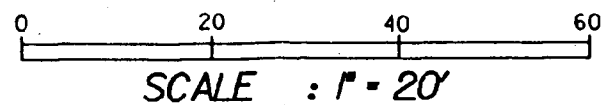
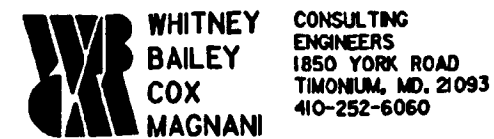


FIGURE 6



GERMANTOWN COMMUTER RAIL STATION IMPROVEMENTS
PARKING GARAGE CONCEPTUAL DESIGN PLAN
MONTGOMERY COUNTY, MARYLAND

REVISIONS	DATE	DESIGN	G.E.G.
		DRAFT	R.J.S.
		APPROVED	L.J.K.
		DATE	6-28-94
		SCALE	1" = 20'



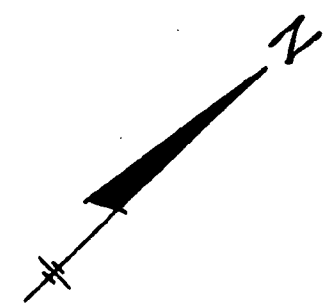
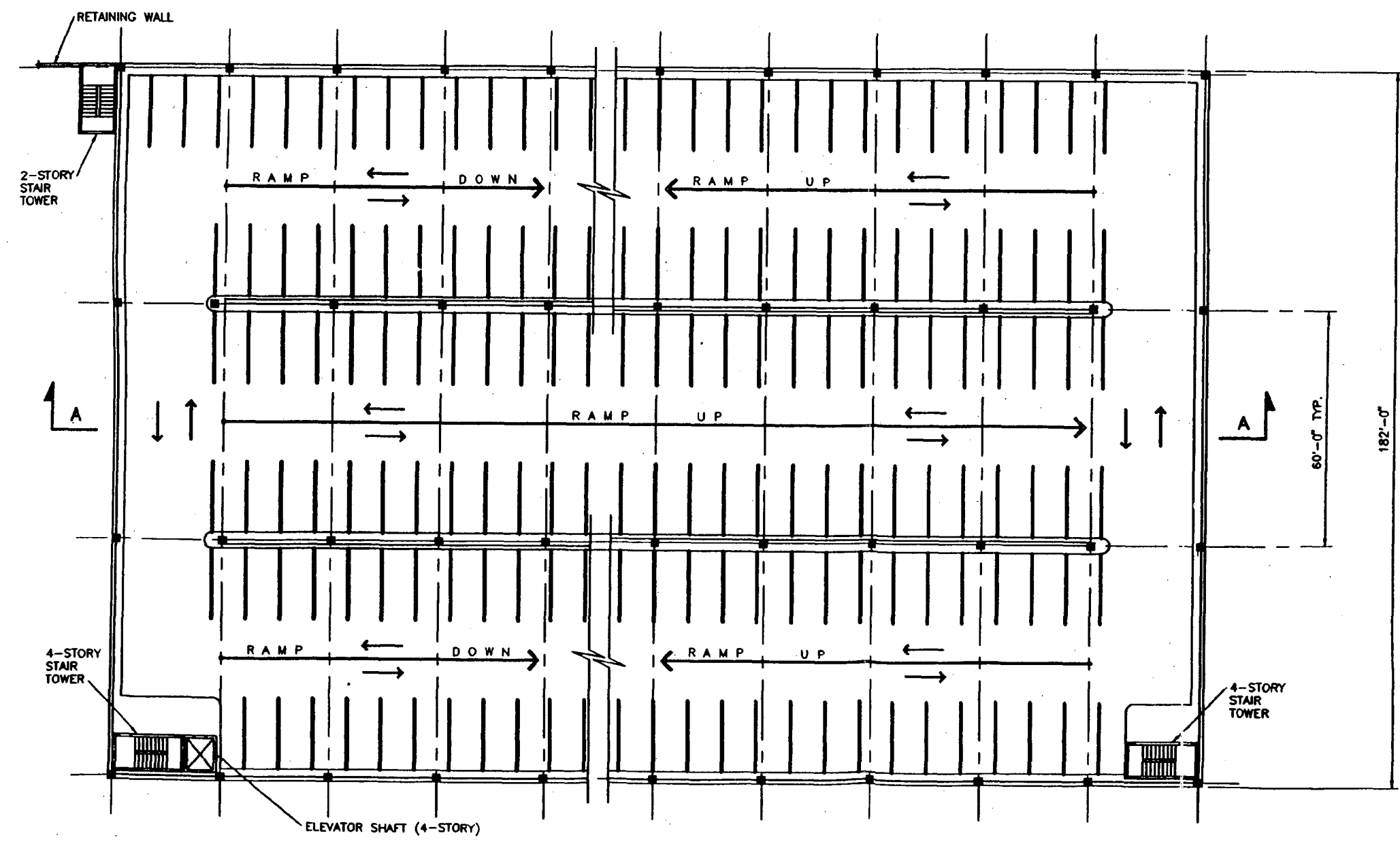
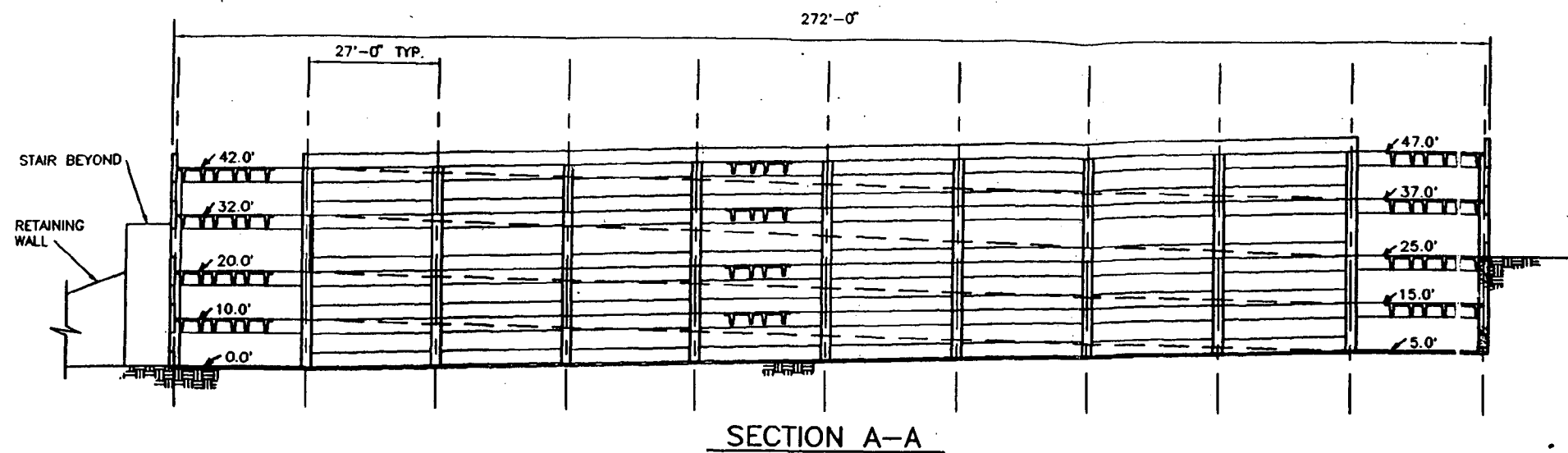


FIGURE 7
TYPICAL FLOOR PLAN - LOT A

WB WHITNEY CONSULTING
BAILEY ENGINEERS
COX 1850 YORK ROAD
MAGNANI TIMONIUM, MD. 21093
410-252-6060

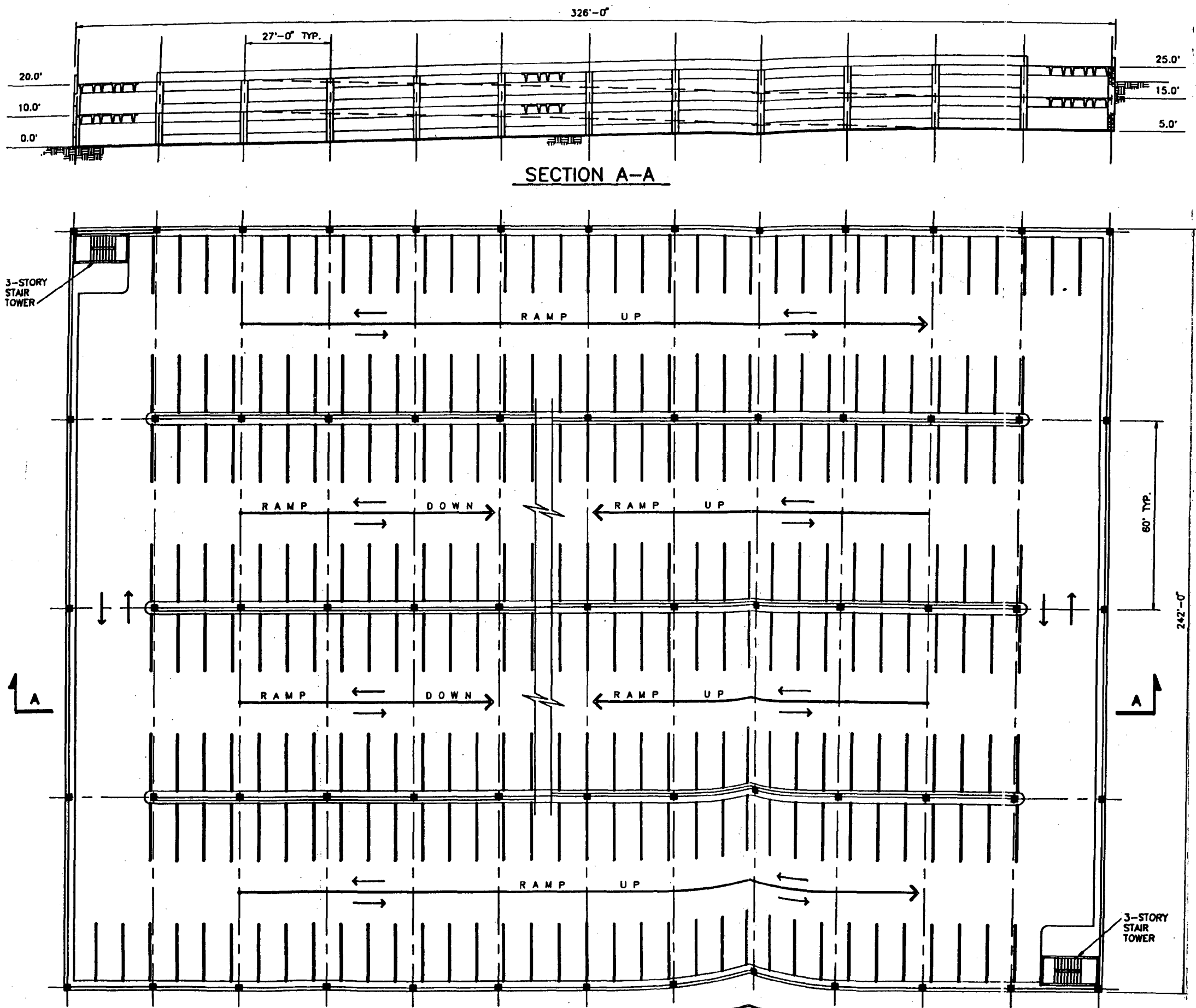
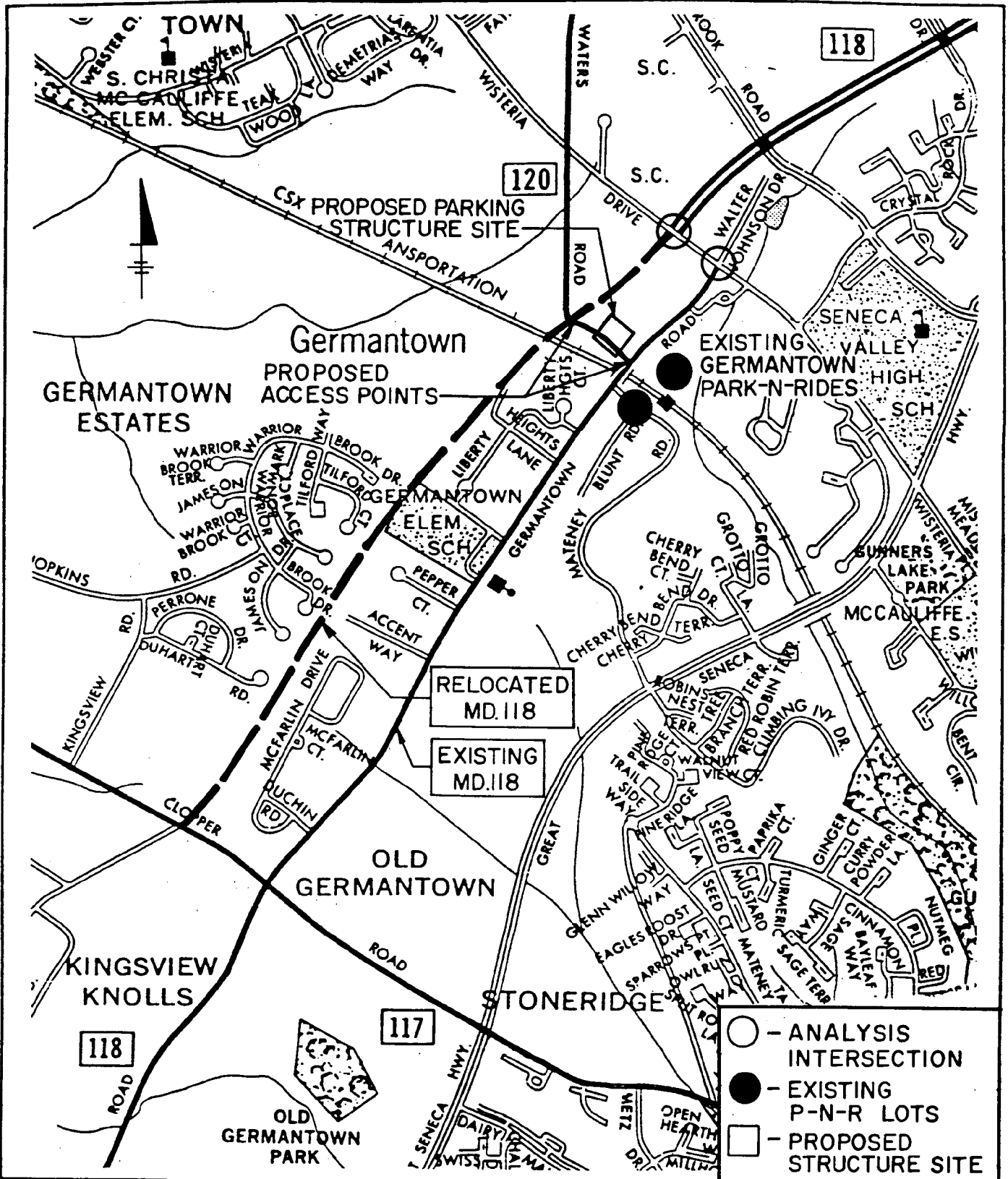


FIGURE 8
TYPICAL FLOOR PLAN - LOT B

WB WHITNEY
BAILEY
COX
MAGNANI

CONSULTING
ENGINEERS
1850 YORK ROAD
TIMONIUM, MD. 21093
410-252-6060



- - ANALYSIS INTERSECTION
- - EXISTING P-N-R LOTS
- - PROPOSED STRUCTURE SITE

WB WHITNEY
 BAILEY
 COX
 MAGNANI

**PROJECT AREA MAP
 GERMANTOWN PARK-N-RIDE**

FIGURE 9

APPENDIX 'A'

ESTIMATED CONSTRUCTION COST BREAKDOWN

COST ESTIMATE BREAKDOWN

LOT 'A'

<u>Description</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Cost</u>	<u>Total</u>
Preliminary				
Mobilization (5% of total)	1	LS	\$395,000	\$395,000
Construction Stakeout	1	LS	\$40,000	\$40,000
Main. of Traffic	1	LS	\$5,000	\$5,000
Clear & Grub	1	LS	\$5,000	\$5,000
Eng. Office	1	LS	\$30,000	\$30,000
Grading				
Excavation	32,500	CY	\$11	\$357,500
Sheeting & Shoring (20' High)	450	LF	\$600	\$270,400
Sawcut	225	LF	\$6	\$1,350
Drainage (Includes Inlets, Pipes, Etc.)				
	1	LS	\$10,000	\$10,000
Paving				
Hot Mix Asphalt (1½") Surface	125	TON	\$45	\$5,625
Hot Mix Asphalt (4½") Base	360	TON	\$35	\$12,600
6" Subbase	1,400	SY	\$10	\$14,000
Pavement Marking	1	LS	\$1,000	\$1,000
Shoulder				
Type 'A' Curb & Gutter	1,070	LF	\$11	\$11,770
5' Sidewalk	2,700	SF	\$5	\$13,500
Retaining Wall (Includes Stairwell to At-Grade Parking)				
	1	LS	\$28,000	\$28,000
Utilities				
Relocate Ex. U.G. Fiber Optics Cable	700	LF	\$50	\$35,000
Relocate O.H. Lines (C&P)	1	LS	\$50,000	\$50,000
Possible Reloc. 24" W	150	LF	\$100	\$15,000
Landscaping				
	1	LS	\$25,000	\$25,000

Parking Garage: 182' x 272' x 5 (Levels)

Structure (Includes Brick Veneer on Spandrels)	247,520	SF	\$23*	\$5,692,960
Fire Protection	247,520	SF	\$0.60*	\$148,512
Electric Service	247,520	SF	\$2.50	\$618,800
Mechanical	247,520	SF	\$0.20	\$49,504
Elevator	1	LS	\$120,000	\$120,000

(* Unit Prices are higher due to below-grade parking levels)

Upper Deck Drainage Sys. (Includes Drains, Pipes, Manholes, Etc.)	1	LS	\$91,000	\$91,000
Lower Decks Drainage Sys. (Includes Drains, Pipes, Manholes, San. Sewer, Oil Interceptor, Holding Tank, Etc.)	1	LS	\$93,000	\$93,000
Contingent Costs for Facade Above Ground				
Structure	148,510	SF	\$3.00	\$445,530
Fire Protection	148,510	SF	\$0.25	\$37,128
Mechanical	148,510	SF	\$0.50	\$74,255

TOTAL WITHOUT FACADE \$ 8,139,121
(\$10,435/space)

TOTAL WITH FACADE \$ 8,696,034
(\$11,148/space)

CONSTRUCTION COST ESTIMATE

LOT 'A'

	<u>Without Facade</u>	<u>With Facade</u>
A. Base Estimate	\$ 8,139,121	\$ 8,696,034
B. Contingency 25%	\$ 2,034,780	\$ 2,174,009
C. Escalation 5% (1994 to 2001)	\$ 3,560,865	\$ 3,804,515
D. Design Costs 8% (A & B)	\$ 813,912	\$ 869,603
E. Construction Mgmt/Insp. 8% (A+B+C)	\$ 1,098,781	\$ 1,173,965
F. Project Administration 6% (A+B+C)	\$ 824,086	\$ 880,473
	<hr/>	<hr/>
TOTAL COST	\$16,471,545	\$17,598,599

CONSTRUCTION ESTIMATE BREAKDOWN

LOT 'B'

<u>Description</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Cost</u>	<u>Total</u>
Preliminary				
Mobilization (5% of total)	1	LS	\$375,000	\$375,000
Construction Stakeout	1	LS	\$40,000	\$40,000
Main. of Traffic	1	LS	\$7,500	\$7,500
Clear & Grub	1	LS	\$30,000	\$30,000
Eng. Office	1	LS	\$30,000	\$30,000
Grading				
Excavation	20,900	CY	\$11	\$229,900
Sheeting & Shoring (10' High)	160	LF	\$210	\$33,600
Sawcut	130	LF	\$6	\$780
Drainage (Includes Inlets, Pipes, Etc.)				
	1	LS	\$10,000	\$10,000
Paving				
Hot Mix Asphalt (1½") Surface	115	TON	\$45	\$5,175
Hot Mix Asphalt (4½") Base	345	TON	\$35	\$12,075
6" Subbase	1,300	SY	\$10	\$13,000
Pavement Marking	1	LS	\$750	\$750
Shoulder				
Type 'A' Curb & Gutter	950	LF	\$11	\$10,450
5' Conc. Sidewalk	2,300	SF	\$5	\$11,500
Stairs (Includes Ret. Walls Around Stairs, Excavation, Stairs, Etc.)				
	1	LS	\$20,000	\$20,000
Platform Extension (Includes Pipe Extension Hdwall, Conc., Brick, Etc.)				
	1	LS	\$21,000	\$21,000
Utilities				
Relocate Ex. U.G. Fiber Optics Cable	950	LF	\$50	\$47,500
Relocate O.H. Lines	1	LS	\$75,000	\$75,000
Possible Reloc. 24" W	300	LF	\$100	\$30,000
Landscaping				
	1	LS	\$25,000	\$25,000

SWM Pond (Includes
 Class I Exc., Pipes,
 Riser Structure, Riprap,
 Fence, Gate, Etc.) 1 LS \$30,000 \$30,000

Parking Garage: 326' x 242' x 3 (Levels)

Structure (Includes Brick Veneer on Spandrels)	236,676	SF	\$21	\$4,970,196
Fire Protection	236,676	SF	\$0.50	\$118,338
Electric Service	236,676	SF	\$2.50	\$591,690
Upper Deck Drainage System (Includes Drains, Pipes, Manholes, Etc.)	1	LS	\$106,000*	\$106,000
Lower Decks Drainage System (Includes Drains, Pipes, Manholes, San. Sewer, Oil Interceptor, Holding Tank, Etc.)	1	LS	\$104,000†	\$104,000
Contingent Costs for Facade				
Structure	236,676	SF	\$3.00	\$710,028
Fire Protection	236,676	SF	\$0.25	\$59,169
Mechanical	236,676	SF	\$0.50	\$118,338

TOTAL WITHOUT FACADE \$ 6,948,454
 \$ 9,047/space)

TOTAL WITH FACADE \$ 7,835,989
 (\$10,203/space)

* Cost higher than Lot 'A' due to more trench drains.

† Cost higher than Lot 'A' due to more piping and longer connection to
 sanitary sewer.

CONSTRUCTION COST ESTIMATE

LOT 'B'

	<u>Without Facade</u>	<u>With Facade</u>
A. Base Estimate	\$ 6,948,454	\$ 7,835,989
B. Contingency 25%	\$ 1,737,114	\$ 1,958,997
C. Escalation 5% (1994 to 2001)	\$ 3,039,949	\$ 3,428,245
D. Design Costs 8% (A & B)	\$ 694,845	\$ 783,599
E. Construction Mgmt/Insp. 8% (A+B+C)	\$ 938,041	\$ 1,057,858
F. Project Administration 6% (A+B+C)	\$ 703,531	\$ 793,394
TOTAL COST	\$14,062,025	\$15,858,082

APPENDIX 'B'

**TELEPHONE CONVERSATION RECORDS
AND CORRESPONDENCE**

Baker

Michael Baker Jr., Inc.
c/o AT&T
11820 Leesburg Pike
Herndon, VA 22070

(703) 430-5086
FAX 430-5306

March 25, 1994

Whitney, Baily, Cox & Magnani
ATTN: Mr. Leon J. Kriebel, P.E.
1850 York Road
Timonium, MD 21093

RE: Parking Garage Study
MD Route 118 at CSX Railroad
Montgomery County, Maryland

Dear Mr. Kriebel:

Attached is a map showing AT&T cables in the vicinity of MD Route 118 and the CSX railroad.

The locations of these cables are as follows:

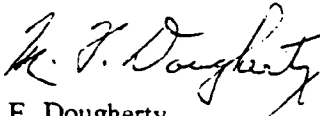
- The lightguide cable is located within the CSX railroad right-of-way on the north side of the tracks.
- The wire cable is located on the south side of Wisteria Drive on private right-of-way to Old Route 118, then, also in private right-of-way, north along Old Route 118 on the west side of the road.

Should you wish to have the cables marked in the field you may contact AT&T at Monrovia, Maryland on (301) 865-3803.

It there are any questions or additional information is needed, please call me on (703) 430-5088.

Sincerely,

MICHAEL BAKER JR., INC.



M. F. Dougherty
Project Engineer

MFD/skt

Enclosure

Whitney, James J. PE
Bailey, John PE
Cox, William A. Magnani PE
Magnani, James PE
David D. Magnani PE

WHITNEY BAILEY COX MAGNANI



Consulting Engineers

Associates
Brand Jones PE
Russell E. Holloway PE
Leon J. Kriebel PE
W. Howard Garret, AIA
John J. Knight, PE
John D. Wayne, PLS
Albert M. Copp
Joseph, Semek, PE

1950 York Road
Timonium, Maryland 21093
410-252-0000
Fax 410-250-1194

April 28, 1994

Mr. Ernest M. Baisden, P.E.
Mass Transit Administration
300 West Lexington Street
Baltimore, Maryland 21201-3415

Re: Contract No. MTA-23-91-518
Design of Suburban Transit Facilities
Germantown Parking Garage Study
WBCM Job No. 93-100-08

Dear Mr. Baisden:

On April 25, 1994, a meeting was held at MTA between WBCM and you, with the primary purpose of discussing the alternates that have been proposed to this point. The following is a list of questions and concerns compiled during the feasibility study of said project that require answers to finalize the feasibility study. Please address these issues so that WBCM can continue with the study.

- Since the State Highway Administration is proposing to relocate MD Route 118 northwest of the existing wooded lot, should it be assumed that access will be allowed from proposed MD 118 to the proposed garage on either lot?
- For the studies, can we make an assumption that the existing MD 118 bridge will be closed to vehicular traffic and used for pedestrian traffic? If not, will studies need to be done for both alternatives?
- Based on zoning requirements, the maximum percentage of net lot area that may be covered by buildings is 25%. Can the County waive this criteria?
- What are the landscaping requirements?
- How many levels in a garage would be acceptable in this area? To obtain approximately 1,000 spaces, a garage at the wooded lot would have 4 floors (240 spaces per floor). On the existing park-n-ride lot, 7 floors would be needed (144 spaces per floor). For a garage at the wooded lot, would 3 levels/720 spaces be sufficient? For the garage at the existing park-n-ride, would 5 or 6 levels/720 or 864 spaces be sufficient?

WHITNEY

BAILEY



COX

MAGNANI

Attn: Mr. Ernest M. Baisden

April 28, 1994

Page 2

- In the study of the wooded lot, it was assumed that Waters Street could be used to expand for usable building area. Since this is a major factor to our design layout, can we receive confirmation on this from the County?
- Are you aware of any requirement for a minimum distance to the historic building on the lot from a proposed sidewalk or garage?
- Based on our research, both sites are out of the historic district. Therefore, will the County require any special exterior building treatment?

Sincerely,

WHITNEY, BAILEY, COX & MAGNANI

Leon J. Kriebel, P.E.
Associate

LJK:GEG:tjs
9310008L.002

Whitney Bailey
 Consulting Engineers
 357 Oak Ridge
 Columbia, Maryland 21046
 410-291-0200
 FAX 410-504-1131

WHITNEY

BAILEY



Consulting Engineers

COX

MAGNANI

Whitney Bailey
 Consulting Engineers
 357 Oak Ridge
 Columbia, Maryland 21046
 410-291-0200
 FAX 410-504-1131

May 17, 1994

Mr. Mike Geier
 Permit and Plan Review
 Environmental Protection
 250 Hungerford Drive
 Rockville, Maryland 208

Reference: Germantown Parking Garage Study
 MD Route 118 and CSX Railroad
 WBCM Job No. 93-100-08

Dear Mr. Geier:

As we discussed by phone, attached is a summary of the results of our storm water management analysis for the above referenced project. We assumed that the entire proposed site would be impervious and would be conveyed to the existing regional storm water management pond. The existing site does not drain to this facility today. Re-running the TR-20 program supplied to us with the additional drainage area resulted in the following:

SUMMARY OF ADDITIONAL D.A. TO EXISTING REGIONAL POND

	EX. POND	POND W/ ADDIT. AREA	INCREASE
Q2 WSEL Q2 Discharge	435.04 21.21 cfs	435.11 28.87 cfs	+0.07' +7.66 cfs
Q10 WSEL Q10 Discharge	436.28 332.65 cfs	436.33 356.45 cfs	+0.05' +23.80 cfs
Q100 WSEL Q100 Discharge	436.96 623.75 cfs	437.00 644.87 cfs	+0.04' +21.12 cfs
SPLWY HYDRO WSEL SPLWY Q Discharge	437.37 884.98 cfs	437.41 910.66 cfs	+0.04' +25.68 cfs
PROB. MAX. WSEL PROB. MAX. Q Discharge	439.64 2228.73 cfs	439.78 2283.72 cfs	+0.14' +54.99 cfs



Attn: Mr. Mike Geier
May 17, 1994
Page 2

A quick observation of the riser layout indicates that reducing the 7" orifice at the gate valve for the 8" ductile iron pipe would decrease the discharges during extended detention thereby raising the extended detention water surface elevation. As a result, the 2-year storm water surface elevation would also be raised, increasing the 2-year storm discharge. (The riser crest elevation for this pond was set at an elevation slightly below the 2-year storm water surface elevation.) Conversely, opening the gate valve to its maximum orifice size of 8" would have the opposite effect - lowering the extended detention water surface elevation, increasing the extended detention discharges, lowering the 2-year storm water surface elevation and decreasing the 2-year discharge. Increasing the extended detention discharges (thereby reducing the effectiveness of extended detention) would not be advised unless the pond had been over-designed for extended detention. Until the original extended detention computations are obtained and analyzed, further study of this option (i.e., opening the valve to its maximum orifice size) is not warranted.

As you are aware, we are only in a study phase for the proposed garages; therefore, we are requesting an opinion on whether the higher water surface elevations and increased release rates would be permitted. If not, would modification of the existing pond release structures be permitted or would a separate storm water management facility be required?

We thank you for your time. If you have any questions or need additional information, please contact me.

Sincerely,

WHITNEY, BAILEY, COX & MAGNANI

Leon J. Kriebel, P.E.
Associate

LJK:tjs
9310008L.003

cc: Mr. Ernest Baisden

WHITNEY

BAILEY



COX

MAGNANI

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May 27, 1994

Mr. David Ramsey
Acting Chief
Access Permits Division
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21202

Attn: Mr. Greg Cook

Re: Germantown Parking Garage Study
Montgomery County
WBCM Job No. 9310008

Gentlemen:

Reference is made to our telephone conversation of May 17, 1994, regarding access from Relocated MD Route 118 to a potential parking garage facility. We are currently conducting a study for the Maryland Mass Transit Administration for potential sites for a parking garage to serve the MARC train station.

Attached is a print of a concept site plan for the garage on a site located adjacent to the CSX Railroad and Relocated MD Route 118. We have marked in red on the print a potential access point from MD Route 118 to the garage. Please note that this is only a preliminary layout and turning lanes are not based on traffic numbers. We are requesting a determination on whether access would be permitted from the Relocated MD Route 118. As an additional note, please be aware that the MTA plans to construct a surface lot on this site prior to constructing any parking garage.

We would appreciate a reply at your earliest convenience. If you have any questions, please do not hesitate to call. I can be reached at 561-8388.

Sincerely,

WHITNEY, BAILEY, COX & MAGNANI

Leon J. Kriebel, P.E.
Associate

LJK:mml
9310008L.004

cc: Mr. Ernest Baisden, MTA



Member ACEC



**Maryland Department of Transportation
State Highway Administration**

O. James Lighthizer
Secretary
Hal Kassoff
Administrator

June 10, 1994

Mr. Leon J. Kriebel
Whitney, Bailey, Cox & Magnani
1850 York Road
Timonium, Maryland 21093

Re: Montgomery County
MD 118 Relocated
Germantown Parking
Garage Study

Dear Mr. Kriebel: .

Thank you for your transmittal of May 27th, which we received on May 31st concerning the above referenced project.

We are presently circulating this information to the Design Division within the State Highway Administration for their review and comment. We will advise you of our findings and recommendations within the next few weeks.

In the interim, if you have any questions, or need additional information, please contact Greg Cooke of this office at (410) 333-1350.

Very truly yours,

A handwritten signature in cursive script that reads "David Ramsey".

David Ramsey, Acting Chief
Engineering Access Permits
Division

GC/maw

A large, bold, block-letter stamp that says "RECEIVED".

JUN 14 1994

A bold, block-letter stamp that says "WBCM".

My telephone number is 410-333-1350 (Fax# 333-1041)

Maryland Relay Service for Impaired Hearing or Speech
1-800-735-2258 Statewide Toll Free

Mailing Address: P.O. Box 717 • Baltimore, MD 21203-0717
Street Address: 707 North Calvert Street • Baltimore, Maryland 21202

CONVERSATION RECORD

DATE: 4/4/94

TIME: 12:15 p.m.

REFERENCE: GERMANTOWN PARKING GARAGE STUDY

INDIVIDUAL: Ed Daniel

CORPORATION: Montgomery County

PHONE NO. (301)217-2976

RE: Questions Sent to Mr. Daniel; Refer to April 28, 1994, letter to Mr. Baisden

NOTES:

1. Montgomery County has always assumed that the future parking lot and/or garage will have access from the new MD 118. This access point has not been approved yet by SHA.
2. Montgomery County has not entered into discussion with the SHA about closing the bridge, but this has been in the County Master Planning Plan for a long time. Mr. Daniel is not aware of any community opposition to closing the bridge since there will be an arterial connection between the new MD 118 and old MD 118, 1000' west of the railroad.
3. Mr. Daniel was not aware of the 25% criteria and does not know if it can be waived by the County. If so, the County would certainly waive it. He seems to think that the zoning has been changed in the most recent zoning plan. He also said that the County would not want to waive any of the building set-back criteria.
4. Mr. Daniel does not know what the landscaping criteria is. However, the County would want a lot of landscaping to blend the garage into the area as much as possible.
5. In the County's conceptual work, they had 3 levels, one at grade and two above ground. Their criteria was for 800 vehicles; however, there has never been a demand study. Their feelings have been that a garage was never feasible on the existing park-n-ride lot due to the smaller space available. Since a major portion of the historic district is directly across the tracks from the existing park-n-ride, Mr. Daniel feels that a 7-level garage would never be accepted by the community.

6. Waters Street is a SHA highway. The County has always assumed that the MTA will work out closure of the road with SHA.
7. Mr. Daniel thinks the sites are within the historic district. He is not aware of a minimum distance that the garage must be from the existing historic house; however, he would not want it too close. The County owns the historic house and has leased it out for 99 years.
8. Even if the garage is not in the historic district, the County would want the garage compatible or old-looking to match the community. Something like a brick facing would be preferred on all four sides.

CONVERSATION RECORD

DATE: 4/13/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Creston Mills

CORPORATION: SHA - District #3

PHONE NO. (301)513-7311

RE: study

NOTES:

I talked to Creston Mills about the following items:

1. Waters Road is an SHA highway. SHA has no problem with abandonment of the roadway when it is not longer needed.
2. Closing of the bridge on Old MD Route 118--it has been part of the overall plan to transfer the existing MD Route 118 to the County when the new MD Route 118 is complete. Therefore, the County can do whatever they want to.
3. Access to the parking lot or garage from the new MD Route 118 - SHA has not looked at this access issue. It has never been discussed with Mr. Mills. He would not have the final say on breaking limited access. This would have to go through Access Permits. He would be asked to comment on it. Mr. Mills suggested that I talk to Mr. Greg Cook in Access Permits about this issue.

CONVERSATION RECORD

DATE: 4/17/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Greg Cook

CORPORATION: SHA - Access Permits

PHONE NO. (410)333-1350

RE: Access from Relocated MD Route 118

NOTES:

I talked to Greg Cook about access from the new MD Route 118 to the future parking garage site. He said they have received several requests for access points for that project. He suggested that we send a concept plan and request comment on access from the new MD Route 118.

CONVERSATION RECORD

DATE: 5/26/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE STUDY

INDIVIDUAL: Nancy Witherall

CORPORATION: MNCPP

PHONE NO. (301)495-4570

RE: Architectural Considerations.

NOTES:

Ms. Witherall is with the Design Zoning & Preservation Division of MNCPPC. The purpose of the call was to discuss requirements for the parking garage with respect to impacts on the historic district. I asked her if the garage would need a facade or if it would need to be completely enclosed. She said that she would be mostly concerned about the location and height of the structure and what visual impacts that the garage would have on the historic district. She said that it may not need to be enclosed; however, it should be attractive and appropriate.

cc: Mr. Ernest Baisden

CONVERSATION RECORD

DATE: 6/2/94

TIME:

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Mike Geier

CORPORATION: Montgomery County Dept. of Environment

PHONE NO. (301)217-6324

RE: Existing Stormwater Management Pond

NOTES:

Mr. Geier called regarding our May 17, 1994, letter on our analysis of the existing stormwater management pond if the additional runoff from Lot 'B' was to be conveyed to the pond. Mr. Geier stated that he did not like the 2-year discharges based on our analysis. He said we may be able to modify the discharges by opening the gate valve further; however, this would affect the extended detention and he did not know the designed extended detention duration. The way the discharges are based on the numbers that we submitted to him, he is not willing to entertain a waiver.

He raised the possibility of raising the weir crest; however, he would really prefer to keep the drainage flowing the same direction that it does today.

cc: Mr. Ernest Baisden

CONVERSATION RECORD

DATE: 6/8/94

TIME: 10:00 A.M.

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Mr. Ed Daniel

CORPORATION: Montgomery County

PHONE NO. (301)217-2976

RE: Questions on Parking Garage

NOTES:

1. I asked Mr. Daniel if the County had any plans on the historic house. He checked and was not able to locate any.
2. I asked Mr. Daniel if he was aware of any previous history on this project (i.e., any previous study for a parking garage). He was not aware of any previous studies.
3. I asked Mr. Daniel about the need for an elevator with a 2-story garage. He said that they did not put one in the Shady Grove garage, which was only two stories; however, he does not know if the code did not require one, or if they got a waiver.
4. I asked Mr. Daniel about his comment on the sight distance at the existing MD Route 118 bridge. He agreed with me that if the bridge is closed, then there is no problem. I asked him if the County had a condition report on the bridge. He said they did not, since it is SHA-owned. He does not know the condition of the bridge; however, he assumed that it would need to be reconstructed if it remains open. He agreed with me that it could be widened to eliminate a sight distance problem if it is reconstructed.

cc: Mr. Ernest Baisden

CONVERSATION RECORD

DATE: 6/8/94
TIME:
REFERENCE: GERMANTOWN PARKING GARAGE
INDIVIDUAL: Greg Cook
CORPORATION: SHA - Access Permits
PHONE NO. (410)333-1250
RE: Access from New MD 118

NOTES:

I called Greg Cook to find out if he had received our plan showing access from the new MD Route 118. He said he had the plan, but SHA would need to look at the overall area to see where additional cross-overs may be. Any median break requires approval of the Chief Engineer. He also stated that in the past, that typically there was a letter from the County supporting the access point. Greg will be sending this to Ken McDonald in highway design for further review.

CONVERSATION RECORD

DATE: 6/19/94

TIME: 3:20 p.m.

REFERENCE: GERMANTOWN PARKING GARAGE

INDIVIDUAL: Denise Boswell

CORPORATION: MNCPPC

PHONE NO. (301) 495-4731

RE: Maximum 25% Lot Coverage Requirement

NOTES:

Denise said we would have to petition the Board of Appeals for a variance if we need a building coverage more than 25%. It does not matter whether it is publicly owned and operated--MTA (the parking garage).

The burden of proof is on us to prove hardship, that the existing zoning imposes hardship.

I told Denise we want to keep the use compatible.

We must still go through the mandatory referral process.

BY: Pat Ford